

MATANUSKA-SUSITNA BOROUGH PLANNING COMMISSION AGENDA

Edna DeVries, Mayor

PLANNING COMMISSION
Doug Glenn, District 1
Richard Allen, District 2 – Vice-Chair
C. J. Koan, District 3 - Chair
Vacant, District 4
Linn McCabe, District 5
Wilfred Fernandez, District 6
Curt Scoggin, District 7



Michael Brown, Borough Manager

PLANNING & LAND USE
DEPARTMENT
Alex Strawn, Planning & Land Use Director
Kim Sollien, Planning Services Manager
Jason Ortiz, Development Services Manager
Fred Wagner, Platting Officer
Karol Riese, Planning Clerk
Corinne Lindfors, Planning Clerk Assistant

*Assembly Chambers of the
Dorothy Swanda Jones Building
350 E. Dahlia Avenue, Palmer*

August 21, 2023
REGULAR MEETING
6:00 p.m.

Ways to participate in the meeting:

IN PERSON: You will have 3 minutes to state your oral comment.

IN WRITING: You can submit written comments to the Planning Commission Clerk at msb.planning.commission@matsugov.us.

Written comments are due at noon on the Friday prior to the meeting.

TELEPHONIC TESTIMONY:

- Dial 1-855-290-3803; you will hear “joining conference” when you are admitted to the meeting.
- You will be automatically muted and able to listen to the meeting.
- When the Chair announces audience participation or a public hearing you would like to speak to, press *3; you will hear, “Your hand has been raised.”
- When it is your turn to testify, you will hear, “Your line has been unmuted.”
- State your name for the record, spell your last name, and provide your testimony.

OBSERVE: observe the meeting via the live stream video at:

- <https://www.facebook.com/MatSuBorough>
- Matanuska-Susitna Borough - YouTube

I. CALL TO ORDER, ROLL CALL, AND DETERMINATION OF QUORUM

II. APPROVAL OF AGENDA

III. PLEDGE OF ALLEGIANCE

IV. CONSENT AGENDA

A. MINUTES

Regular Meeting Minutes: August 7, 2023

B. INTRODUCTION FOR PUBLIC HEARING: QUASI-JUDICIAL MATTERS

Resolution 23-31 A Conditional Use Permit in accordance with MSB 17.60 – Conditional Uses; for the operation of a Marijuana Retail Facility known as Talkeetna Connection, located at 39209 South Talkeetna Spur Road, Tax ID #24N04W29D002; within Township 24 North, Range 4 West, Section 29, Seward Meridian; Public Hearing: September 18, 2023; (Applicant: Talkeetna Connection; Staff: Rick Benedict, Planner II)

C. INTRODUCTION FOR PUBLIC HEARING: LEGISLATIVE MATTERS

V. COMMITTEE REPORTS

VI. AGENCY/STAFF REPORTS

VII. LAND USE CLASSIFICATIONS

VIII. AUDIENCE PARTICIPATION (*three minutes per person, for items not scheduled for public hearing*)

IX. PUBLIC HEARING: QUASI-JUDICIAL MATTERS

(Commission members may not receive or engage in ex-parte contact with the applicant, other parties interested in the application, or members of the public concerning the application or issues presented in the application).

Resolution 23-33 A Conditional Use Permit in Accordance with MSB 17.17 – Denali State Park Special Land Use District to Operate a Commercial Use (Horse & Wagon Rides). Located at 10578 East Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33, Seward Meridian; (Applicant: Steven Van Troba dba D & S Trail Rides; Staff: Peggy Horton, Planner II)

X. PUBLIC HEARING: LEGISLATIVE MATTERS

Resolution 23-20 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending the Matanuska-Susitna Borough Assembly adoption of the 2023 Coordinated Human Services Transportation Plan Update; (Staff: Maija DiSalvo, Planner II)

Resolution 23-30 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending the Matanuska-Susitna Borough Assembly Approval of the Matanuska-Susitna Borough's Bicycle and Pedestrian Plan; (Staff: Kelsey Anderson, Planner III)

Resolution 23-04 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending Approval of an Ordinance Amending MSB 17.05 – Essential Service Utilities by Providing Additional Process and Standards for Approval; (Staff: Alex Strawn, Planning and Land Use Director)

XI. CORRESPONDENCE & INFORMATION

XII. UNFINISHED BUSINESS

XIII. NEW BUSINESS

XIV. COMMISSION BUSINESS:

A. Upcoming Planning Commission Agenda Items

XV. DIRECTOR AND COMMISSIONER COMMENTS

XVI. ADJOURNMENT (*Mandatory Midnight*)

Disabled persons needing reasonable accommodation in order to participate at a Planning Commission Meeting should contact the Borough ADA Coordinator at 861-8432 at least one week in advance of the meeting.

**MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION MINUTES**

**REGULAR MEETING
August 7, 2023**

The regular meeting of the Matanuska-Susitna Borough Planning Commission was held on August 7, 2023, at the Matanuska-Susitna Borough Assembly Chambers, 350 E. Dahlia Avenue, Palmer, Alaska. The meeting was called to order at 6:00 p.m. by Chair Koan.

I. CALL TO ORDER, ROLL CALL, AND DETERMINATION OF QUORUM

Planning Commission members present and establishing a quorum:

Mr. Doug Glenn, Assembly District #1 *
Ms. C. J. Koan, Assembly District #3
Mr. Michael Rubeo, Assembly District #4
Ms. Linn McCabe, Assembly District #5
Mr. Wilfred Fernandez, Assembly District #6
Mr. Curt Scoggin, Assembly District #7

Planning Commission members absent and excused were:

Mr. Richard Allen, Assembly District #2

Staff in attendance:

Ms. Peggy Horton, Planner II
Mr. Jason Ortiz, Development Services Manager
Mr. Alex Strawn, Planning and Land Use Director
Ms. Shannon Bodolay, Assistant Attorney
Ms. Julie Spackman, Planning and Land Use Administrative Assistant
Ms. Karol Riese, Planning Commission Clerk *

*Indicates that the individual attended telephonically.

II. APPROVAL OF AGENDA

Chair Koan inquired if there were any changes to the agenda.

GENERAL CONSENT: The agenda was approved with one amendment without objection.

Received a request for continuance from DOWL on behalf of Durham Bus Barn until September 18, 2023.

MOTION: Commissioner Scoggin made a motion to postpone Resolution 23-24 to September 18, 2023. Motion was seconded by Commissioner Rubeo.

VOTE: Motion passed without objection.

III. PLEDGE OF ALLEGIANCE

The pledge of allegiance was led by Commissioner Fernandez.

IV. CONSENT AGENDA

A. Minutes Regular Meeting Minutes: June 19, 2023

**MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION MINUTES**

**REGULAR MEETING
August 7, 2023**

B. INTRODUCTION FOR PUBLIC HEARING: QUASI-JUDICIAL MATTERS

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C. INTRODUCTION FOR PUBLIC HEARING: LEGISLATIVE MATTERS

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Resolution 23-30 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending the Approval of an Ordinance Adopting the Bicycle and Pedestrian Plan. Public Hearing: August 21, 2023 (Staff: Kelsey Anderson, Planner III)

Resolution 23-04 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending Approval of an Ordinance Amending MSB 17.05 Essential Service Utilities Providing Additional Process and Standards for Approval; Public Hearing: August 21, 2023 (Staff: Alex Strawn, Planning and Land Use Director)

GENERAL CONSENT: The consent agenda was approved without objection.

V. COMMITTEE REPORTS - *(There were no committee reports.)*

VI. AGENCY/STAFF REPORTS - *(There were no Agency/Staff Reports.)*

VII. LAND USE CLASSIFICATIONS - *(There were no land use classifications.)*

VIII. AUDIENCE PARTICIPATION (Three minutes per person.)

The following people spoke against Resolution 23-04, Essential Services Utilities: Bill Kendig and Jon Sinclair.

There being no one else to be heard, Chair Koan closed Audience Participation.

**MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION MINUTES**

**REGULAR MEETING
August 7, 2023**

IX. PUBLIC HEARING: QUASI-JUDICIAL MATTERS

Resolution 23-24 A Conditional Use Permit in Accordance with MSB 17.61 – Core Area, to Operate a School Bus Facility (Bus Barn) within the Core Area Planning Boundary. The Site is Located at 1600 & 1672 South Tanya Court, Tax ID #7964000L002 & 8193000L009A, within Township 17 North, Range 1 East, Section 16, Seward Meridian (Applicant: DOWL Engineering; Staff: Peggy Horton, Planner II)

Resolution 23-24 was moved to be heard at the September 18, 2023 Planning Commission meeting during Approval of the Agenda.

Resolution 23-25 A Conditional Use Permit in Accordance with MSB 17.70 – Regulation of Alcoholic Beverage Uses for an Alcoholic Beverage Dispensary (Bar) Known as, Meier Lake Event Center, Located at 6059 Wasilla-Fishhook Road, Tax ID #18N01E16B020, within Township 18 North, Range 1 East, Section 16, Seward Meridian (Applicant: Marissa Senna for Meier Lake Event Center; Staff: Peggy Horton, Planner II)

Chair Koan read the resolution title into the record.

Ms. Horton provided a staff report.

Commissioners questioned staff regarding:

Chair Koan asked the applicant if they had anything to add: Marissa Senna for Meier Lake Event Center said they were pleased with what Peggy Horton presented.

Chair Koan opened the public hearing.

There being no one else to be heard, Chair Koan closed the public hearing and discussion moved to the Planning Commission.

Ms. Horton stated that they had nothing further to add.

MOTION: Commissioner Scoggin moved to approve Planning Commission Resolution 23-25. The motion was seconded. by Commissioner McCabe.

Discussion ensued

VOTE: The main motion passed without objection.

X. PUBLIC HEARING LEGISLATIVE MATTERS

Resolution 23-26 A Resolution of the Matanuska-Susitna Borough Planning Commission Recommending the Matanuska-Susitna Borough Assembly Adopt Ordinance No. 23-065 Amending the Beverly Lake, Lake Management Plan; (Staff: Alex Strawn, Planning & Land Use Director)

**MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION MINUTES**

**REGULAR MEETING
August 7, 2023**

Chair Koan read the resolution title into the record.

Mr. Strawn provided a staff report.

Commissioners questioned staff regarding:

Chair Koan opened the public hearing.

The following persons spoke in regarding Planning Commission Resolution 23-26: Zachary Reid questioned whether engines specified 2-stroke or 4-stroke.

Chair Koan invited staff to respond to questions and statements from the audience.

Mr. Strawn responded to questions and statements from members of the audience; the ordinance does not specify.

Mr. Strawn stated that they had nothing further to add.

There being no one else to be heard, Chair Koan closed the public hearing and discussion moved to the Planning Commission.

MOTION: Commissioner McCabe moved to approve Planning Commission Resolution 23-26. The motion was seconded. by Commissioner Scoggin.

Discussion ensued

VOTE: The main motion passed without objection.

XI. CORRESPONDENCE AND INFORMATION
(There was no correspondence and information.)

XII. UNFINISHED BUSINESS - *(There was no unfinished business.)*

XIII. NEW BUSINESS

XIV. COMMISSION BUSINESS

- A. Upcoming Planning Commission Agenda Items *(Staff: Jason Ortiz, Development Services Manager)*

(Commission Business was presented, and no comments were noted.)

XV. DIRECTOR AND COMMISSIONER COMMENTS

Alex Strawn: Commissioner Rubeo's last meeting; appreciated your services.

**MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION MINUTES**

**REGULAR MEETING
August 7, 2023**

Jason Ortiz: Thank you Commissioner Rubeo; September 18 Work Session – items you would like to discuss at this work session; welcomed Julie Spackman, Administrative Assistant.

Commissioner Glenn: Sorry to see Mr. Rubeo leaving us; appreciate his time.

Commissioner Fernandez: Gratitude to the entire staff.

Commissioner Rubeo: Appreciate the experience of being on this board.

Commissioner Koan: You all have done a great job keeping us on track; Karol and Corinne, we miss you.

Shannon Bodolay: Work Session date and time – might want to think about what will be on the Agenda for the meeting.

XVI. ADJOURNMENT

The regular meeting adjourned at 6:35 p.m.

C J KOAN
Planning Commission Chair

ATTEST:

CORINNE LINDFORS
Planning Commission Clerk

Minutes approved: _____

INTRODUCTION FOR PUBLIC HEARING - QUASI-JUDICIAL

Resolution No. 23-31

Talkeetna Connection

(Pages 11 - 56)

INTRODUCTION FOR PUBLIC HEARING



MATANUSKA-SUSITNA BOROUGH

Matanuska-Susitna Borough
Development Services

Planning and Land Use Department Development Services Division

350 East Dahlia Avenue • Palmer, AK 99645

Phone (907) 861-7822 • Fax (907) 861-8158

Email: permitcenter@matsugov.us

JUL 28 2021

Received

CONDITIONAL USE PERMIT FOR MARIJUANA RELATED FACILITIES – MSB 17.60

Carefully read instructions and applicable borough code. Fill out forms completely. Attach information as needed. Incomplete applications will not be processed.

Application fee must be attached:

- ☒ \$1,000 for Marijuana Retail Facility
☐ \$1,000 for Marijuana Cultivation Facility

Prior to the public hearing, the applicant must also pay the mailing and advertising fees associated with the application. Applicants will be provided with a statement of advertising and mailing charges. Payment must be made prior to the application presentation before the Borough Planning Commission.

Required Attachments for a Marijuana Cultivation Facility:

- ☐ Wastewater and Waste Material Disposal Plan – 17.60.160 (A)
☐ Odor Mitigation and Ventilation Plan – 17.60.160 (B)
☐ Hazardous Chemicals Information – 17.60.160 (C)
☐ Security plan – 17.60.160 (D)

Required Attachments for Both Retail and Cultivation Facilities:

- ☐ Documentation demonstrating full compliance with applicable fire code – 17.60.150 (D) (2)

Subject Property: Township: 24N, Range: 4W, Section: 29, Meridian: Seward
MSB Tax ID# 24N04W29D002
SUBDIVISION: N/A BLOCK(S): N/A, LOT(S): D2
STREET ADDRESS: 39209 S. Talkeetna Spur Rd Talkeetna, AK 99676
FACILITY / BUSINESS NAME: The Talkeetna Cannabis Connection, Inc
PBA: Talkeetna Connection

Ownership: A written authorization by the owner must be attached for an agent or contact person, if the owner is using one for the application. Is authorization attached? ☐ Yes ☐ No ☐ N/A

Name of Property Owner

Gerhard & Romy Hafele

Mailing: Bavergasse 9A

Thaur 6085 Austria 99999

Phone: Hm N/A Fax N/A

Wk N/A Cell 907-203-8867

E-mail alaskageri@outlook.com

Name of Agent / Contact for application

Alysa Tunnell Eastwood

Mailing: Po Box 1103

Talkeetna, AK 99676

Phone: Hm N/A Fax N/A



Wk N/A Cell 907-355-3497

E-mail alysatunnell79@gmail.com

Site Plan – Attach a detailed, to scale, site plan clearly showing the following information:	Attached
Proposed and existing structure(s) on the site. Indicate which structure(s) will be used for the proposed use. Dimensions and locations of all existing and proposed structures on the site in relationship to all property lines.	✓
Signage – Existing and Proposed.	✓
Location and dimensions for all access points to and from the site to public rights-of-way or public access easements.	✓
Buffering – Fences, vegetation, topography, berms, and any landscaping	✓
Drainage	✓
Vehicular and pedestrian circulation patterns.	✓
Exterior site lighting.	✓
Location and dimensions of parking areas to be provided	✓
Scale and north arrow using standard engineering intervals such as 1" = 30', 1" = 50' or similar as required by project size.	✓

Map – Attach a detailed, to scale, vicinity map clearly showing the following information:	Attached
Identify all existing land uses within 1,000 feet.	✓
Scale and north arrow using standard engineering intervals such as 1" = 30', 1" = 50' or similar as required by project size.	✓

In order to grant a conditional use permit under MSB 17.60, the Planning Commission must find that each of the following requirements have been met. Explain the following in detail:	Attached
Is the conditional use compatible with and will it preserve or not materially detract from the value, character and integrity of the surrounding area?	✓
Will the granting of the conditional use permit be harmful to the public health, safety, convenience and welfare?	✓
Are sufficient setbacks, lot area, buffers and other safeguards being provided?	✓
Does the conditional use fulfill all other requirements of MSB Chapter 17.60 pertaining to the conditional use in this section?	✓
Describe measures taken to prevent any potential negative effect upon other properties in the area due to such factors as noise and odor.	✓
Describe measures taken to reduce negative effects upon adjacent properties by: <ul style="list-style-type: none"> • Increased property line and right-of-way buffers • Planted berms and landscaping • Site and building design features which contribute to the character of the surrounding area 	✓
Describe how this use is compatible with the character of the surrounding area.	✓
Current status of State License application process – 17.60.150 (D) (1)	✓

17.60.170 Standards for Marijuana Retail Facilities:	Attached
<p>Describe how the subject parcel is appropriate for the proposed conditional use. Include information detailing:</p> <ul style="list-style-type: none"> • The proximity of the proposed use to existing businesses; • The proximity of parcels developed with residential uses; • Whether the roads associated with the proposed use have been, or will be, appropriate for commercial use; and • Proposed hours of operations. 	
<p>Provide information showing minimum parking standards have been met as required by MSB 17.60.170 (B) and (C).</p>	

Floor Plan for Marijuana Retail Facilities – Attach a detailed, to scale, floor plan clearly showing the following information:	Attached
Dimensions of all structures.	✓
Interior floor plans (specific location of the use or uses to be made of the development).	✓
Net floor area square footage calculations.	✓

OWNER'S STATEMENT: I am owner of the following property:

MSB Tax account #(s) 24N04W290002 and, I hereby apply for approval of a conditional use permit to operate a marijuana related facility on the property as described in this application.

I understand all activity must be conducted in compliance with all applicable standards of MSB 17.60 and with all other applicable borough and state.

I understand that other rules such as local, state and federal regulations, covenants, plat notes, and deed restrictions may be applicable and other permits or authorization may be required. I understand that the borough may also impose conditions and safeguards designed to protect the public's health, safety and welfare and ensure the compatibility of the use with other adjacent uses.

I understand that it is my responsibility to identify and comply with all applicable rules and conditions, covenants, plat notes, and deed restrictions, including changes that may occur in such requirements.

I grant permission for borough staff members to enter onto the property as needed to process this application and monitor compliance. Such access will at a minimum, be allowed when the activity is occurring and, with prior notice, and at other times necessary to monitor compliance.

The information submitted in this application is accurate and complete to the best of my knowledge.

Signature: Property Owner

GERHARD HATELE
(AND AS ATTORNEY IN FACT FOR ROMEDIA HATELE) 7/26/2

Printed Name _____

Date _____

Signature Agent

Alysa Tunnell Eastwood 7/22/2

Printed Name _____

Date:

Revised 4/4/2017

Permit#:

Page 3 of 3

PERMIT CENTER – FEE RECEIPT FORM

August 21, 2023 Planning Commission Packet
16 of 450

Property Location: _____

Applicant: Tal Keetna Connection

USE PERMITS {100.000.000.341.300}	Fee
8.35 Public Display of Fireworks	\$25.00
8.40.010 Liquor License - Alcohol & Marijuana Control Office (AMCO) Referrals for Matanuska Susitna Borough Review of Issuance, renewal or transfer (location, owner)	\$100.00
X 8.41.010 Marijuana License - Alcohol & Marijuana Control Office (AMCO) Referrals for Matanuska Susitna Borough Review of Issuance, renewal or transfer (location, owner)	\$100.00
8.52 Temporary Noise Permit	\$1000.00
8.55 Special Events Permit 500 – 1000 Attendees 1000+ Attendees	\$500.00 \$1,000.00
8.55 Special Events Permit Site Monitor Fee / Per Day	\$300.00

Matanuska Susitna Borough
Payment Date Monday, August 30, 2021
Deposit Number 43943
Operator lach0415
Real 2021 (Total) \$0.00
MCR (Planning/Platting) \$100.00
Misc Rec
Click Map # 2MISC
Total Paid \$100.00
Check \$100.00
Change \$0.00
Receipt Number MSB91608866
/2021 7:28:10 AM
By TUNNELALYSA /CK 293
Printer Id. LACH0415

17.25 Tal Keetna Conditional Use Permit – Variance	\$100.00
17.27 Sutton Special Land Use District CUP	\$1500.00
17.29 Flood Damage Prevention Development Permit	\$100.00
17.29 Flood Damage Prevention Development Permit –Variance	\$500.00
17.30.040 Earth Materials Extraction Admin. Permit	\$1000.00
17.30.050 Earth Materials Extraction CUP	\$1500.00
17.36 Residential Planned Unit Development Application – Concept Plan – up to 50 Lots Additional Lots or tracts being created – Per Lot	\$500.00 \$100.00
17.48 Mobile Home Park Permit Application	\$500.00
17.52 Residential Land Use District App (Rezone)	\$1,000.00
17.52 Conditional Use Permit Application CUP	\$1,500.00
17.55 Shoreline Setback Exception Application	\$300.00
17.60 Conditional Use Permit Application	\$1500.00
17.61 Commercial/Industrial Core Area Conditional Use Permit	\$1500.00

17.62	Coal Bed Methane	\$1500.00
17.63	Conditional Use Permit for Racetracks	\$1500.00
17.65	Variance	\$1500.00
17.67	Tall Structures -	
	Network Improvement Permit	\$100.00
	Nonconforming Use	\$200.00
	Administrative Permit	\$500.00
	Conditional Use Permit	\$1500.00
17.68	Outdoor Shooting Range	\$1500.00
17.70	Regulation of Alcoholic Beverage Use Permit Application	\$1500.00
17.73	Multi-Family Land Use Permit – add \$25.00 for each additional unit beyond 5 units.	\$500.00
17.75	Single-Family Residential Land Use District CUP	\$1000.00
17.76	Large Lot Single-Family Residential Land Use District Conditional Use Permit Application	\$1000.00
17.80	Nonconforming Structures (Amnesty) Pre-Existing Legal Nonconforming (Grandfather)	\$300.00
17.90	Regulation of Adult Businesses – Conditional Use Permit	\$1500.00
28.60	Timber Transport Permit	\$300.00

	RIGHT-OF-WAY FEES:	
	Driveway	\$50.00
<input type="checkbox"/>	Driveway Deposit {100.226.100}	\$150.00
	Construction	\$200.00
	Utility (Application Fee = \$100 ~ Distance Fee \$0.25/per lineal foot)	
	Encroachment	\$150.00
	Construction Bond {100.227.000}	

	PLATTING PRE-APPLICATION CONFERENCE:	
	Pre-Application Fee	\$50.00

	FEES:	
	Flood Plain Development Survey CD	\$10.00
	CD/DVD/DVD-R	\$7.50
	Construction Manual/Title 43	\$5.00
	Plat Map/Tax Map Copies/Mylar	\$5.00
	Color Maps	\$12.00
	Xerox Copies (B/W = \$0.25 ~ Color \$1.00/page 11X17 Color \$1.75/page)	
	Advertising Fees	
	Cultural Resources Books or Maps	
<input type="checkbox"/>	Citation Payment (If sent to collections – use total due from Courtview)	
	Thumb Drive 2GB = \$5, 4GB = \$8, 8GB = \$10; 16GB = \$15; 32GB = \$20	

\$ 100 Amount Paid Date: 8/20 Receipt # _____ By: GL

Conditional Use Permit Talkeetna Connection

Is the conditional use compatible with and will it preserve or not materially detract from the value, character and integrity of the surrounding area?

Talkeetna Connection has high standards for creating and maintaining a professional and esthetically pleasing business model. The building and surrounding grounds shall be kept clean and appealing to not only our customers but our neighbors and community as well. The building itself is of brand new construction and designed by a professional engineering team. It is constructed entirely of steel, including the exterior and interior siding. The exterior color of the building is dark gray, and the inside is a mixture of zinc with dark forest green accents. It is our goal to create more of a “classy” look, unlike some marijuana retail stores that promote more of a bright artistic vibe.

The grounds surrounding our store consist of nicely graded gravel with adequate drainage to prevent any standing water. During the winter months, snow will be removed in a timely manner for easier customer access. Additionally, a handicap ramp for people w/ disabilities will be constructed before the store is open for business. The ramp will lead up to an 8 x 16 deck attached to the store front.

Talkeetna Connection will in absolutely no way materially detract from the value, character and integrity of the surrounding area; On the contrary, it is my belief that it will improve the value, character, and integrity of the surrounding area. In addition to the above details, the outside grounds and deck will be decorated with bright beautiful flowers which will most certainly add to the charm and character throughout the summer season.

Will the granting of the conditional use permit be harmful to the public health, safety, convenience and welfare?

The public's health is the utmost important requirement to fulfill while providing our services to our community and it will not be overlooked in any aspect. As a marijuana retail facility, we are required by law to conform to regulations that have been put in place in order to protect the public's health and safety. Following are details explaining these regulations.

The building in which we will be conducting our business out of had to be approved by the state fire marshal office. The fire marshal ensures that the structure is designed and erected to meet

safety standards that are particular to our local Upper Susitna area and will be able to withstand the extreme snow loads that are common in this region. Our structure has architectural drawings showing that it satisfies the fire marshal's code regulations. The fire marshal also confirms that the heating system and exhaust conforms to code regulations. There are two approved fire extinguishers placed in our place of business in the unfortunate event of structure fire. The deck that the public will use to gain entry into the store will have safety railing around the deck entirely to prevent people from falling off.

Talkeetna Connection will have a hand washing station in the back employee "restricted access" room. There will be a porta potty for employees located behind the store which will be maintained on a regular basis and always be stocked with hand sanitizer. Employees will be required to use hand sanitizer as well as wash their hands after using the toilet facilities. Employees will also be required to wash thoroughly before starting their shift. This will ensure that no product will be contaminated and passed on to the public. Employee personal hygiene will be a top priority in protecting public health. Additionally to personal hygiene, our employees will be held to high standards in keeping the facility clean and appealing to the public.

The marijuana that we provide to our customers will go through a state certified marijuana testing facility before it is added to our inventory. The State of Alaska requires the testing labs to do microbial testing in order to protect the public from harmful bacterias and they also provide a potency test in order to better inform customers how much THC and terpenes are present in each strain.

Are sufficient setbacks, lot area, buffers and other safeguards being provided?

Talkeetna Connection will be located off of the Talkeetna Spur Rd. The total ROW in this area is 125 ft, which includes the additional 25 ft setback required by the Matanuska Susitna Borough. In order to conform to these setbacks, we hired a certified surveyor to guide us in ensuring that we erected the building outside of the Talkeetna Spur Rd ROW. On the back side of the retail store there is an MEA powerline in which we also had to keep clear of their easement. Craig Hansen has prepared a survey showing our placement of the retail building in between the Talkeetna Spur Rd ROW and the MEA easement. We have gone to great lengths to ensure that we are not encroaching on these easements.

Does the conditional use fulfill all other requirements of MSB Chapter 17.60 pertaining to the conditional use in this section:

17.60.150:

A:) Describe measures taken to reduce negative effects upon adjacent properties by:

1.) Describe any measures taken to prevent any potential negative effects upon other properties in the area due to such factors as noise and odor.

- We do not expect any negative effects upon other properties from noise and/or odor. Being that we are not a cultivation facility, odor will not be an issue. Noise will not be an issue either. We will not be conducting any business that creates excessive noise.

2.) Describe measures taken to reduce negative effects upon adjacent properties by:

- **Increased property line and ROW buffers:** Great measures have been taken to ensure that the licensed premises is located outside of all ROW's and easements. Also, we will ensure that extra buffers are placed in order to delineate where the driveway approach is and where we want traffic to flow in order to best suit not only Talkeetna Connection, but neighboring businesses as well. Our lease entitles us to 1 acre of land which is part of an 11 acre parcel. Other than the Talkeetna Spur Rd ROW, property lines are not an issue as they are a great distance away to the North, East, and South directions. The closest business located next door, "My Mom's Thai" is 200 ft away from our structure. We are very confident that we will not be a nuisance to any of our neighboring businesses, instead, it is our belief that we will be a welcoming asset and will only benefit others around us.
- **Planted berms and landscaping:** The grounds surrounding Talkeetna Connection consist of nicely graded gravel with adequate drainage away from the structure and parking areas. Outside of the gravel pad consists of virgin grounds with a mixture of birch, cottonwood, and spruce trees, other than the commercial sites that lie along the Talkeetna Spur Rd ROW. Talkeetna Connection will also be placing a planted berm along the Spur Rd, in order to delineate the approved driveway approach. This will add a nice touch of green landscaping throughout the summer season, and a place to plow snow during the winter months.
- **Site and building design features which contribute to the character of the surrounding area:** The site surrounding Talkeetna Connection has been developed as an attractive commercial lot with enough room for vehicles to park and maneuver effectively. The building design is of brand new construction and designed by professional engineers. The exterior steel siding is dark grey and the bottom skirting surrounding the foundation is black corrugated steel siding which offsets the grey nicely. The design of the building and site will in no way harm the character of the surrounding area.

B:) At the time of their establishment marijuana related conditional uses shall meet the following requirements and not be located within 1000 ft of school grounds.

-Talkeetna Connection is located and placed more than 1000 ft from the closest point on a school property line.

Current Status of State License application process:

Our State license has been approved but is currently placed in delegation awaiting an approved CUP from the Matanuska Susitna Borough.

Describe how the subject parcel is appropriate for the proposed conditional use. Include information detailing:

-The proximity of the proposed use to existing businesses:

- The licensed premises is located 200 ft from the closest adjacent business, "My Mom's Thai." It is our assumption that the addition of our business will only improve the overall business traffic and increase revenue for neighboring businesses within the general area.

-The proximity of parcels developed with residential uses:

- The closest residential lot line is greater than 500 ft away from the proposed licensed premises. We do not foresee any issues that may arise from any residential lot owners.

-Whether the roads associated with the proposed use have been, or will be, appropriate for commercial use:

-The roads associated with the proposed licensed premises consist of the Talkeetna Spur Rd. Traffic will turn off of the Talkeetna Spur Rd directly onto our approved driveway approach entering our nicely graded parking area. Snow removal will be done as needed throughout the winter months.

Proposed hours of operation:

-Talkeetna Connection will be open for business from Sunday to Saturday. Hours of operation will be from 12:00 pm to 10:00 pm.

Provide information showing minimum parking standards have been met as required by MSB 17.60.170 B and C;

- The licensed premises will be 24 ft x 24 ft with a total square footage of 576 ft squared. There will be at least 2 designated parking spaces on the south side of the building which is the storefront and entrance. One of the parking spots will be at least 20 ft long, 10 ft wide, and there are no overhead obstructions present. The other parking spot will comply with the Americans with Disabilities Act to provide people with disabilities an unobstructed entrance to the premises. The ADA parking spot shall be a minimum of 96 inches wide with an extra 60 inches designated as an access aisle. It shall have no more than a 2.8 % slope in all directions with a surface that is firm, stable, and slip-resistant. A sign with the international symbol of accessibility shall be mounted at

least 60 inches above the ground, and directly in front of the ADA parking spot. This should satisfy the requirements set forth in section 17.60.170 of the MSB regulations.

Additional Information

Loitering Prevention, ID policy, and Lighting

The owners of Talkeetna Connection, Clint Tunnell and Alysa Tunnell Eastwood have strict intentions of sustaining a safe and attractive business front. Factors that can affect the safety and overall attractiveness that businesses must take into consideration are loitering prevention, ID policy, and exterior lighting. Loitering and soliciting are strictly prohibited on our premises, which will be posted on the exterior of our main entrance area. There will be one sign displaying "No Loitering No Soliciting Violators Will Be Prosecuted by Law." If a person or person's do not adhere to our policy, local law enforcement will be called. Our ID policy will be taken extremely seriously as this will be a requirement for our employees to perform every day during all business hrs as patrons enter the premises. As a marijuana retail store, anyone who enters the premises must be 21 years of age and must have a legal, unaltered state ID or passport to prove their age. There will be a sign posted reading "No one under 21 years of age allowed." Upon entering the main entrance an employee of Talkeetna Connection shall immediately ask for identification. If the customer is unable to provide adequate documentation of their age, they will be asked to leave the premises until they are able to provide a legal form of identification. The exterior lighting shall be installed around the entire exterior of the building. The building is 24 ft long x 24 ft deep x 8 ft tall. Since the building consists of four 24 ft walls (diameter), there will be two exterior flood lights mounted just below the eave on each wall, totalling eight lights around the entire building. The height of the lights will be approximately 7 ft high, just below the eave where the light is able to shine and illuminate the maximum space. With adequate exterior lighting, it is our intention to deter unwanted guests at all hours, but it also illuminates the outdoors, at 180 degrees, enough for our camera systems to detect and recognize possible intruders at all hours of the day and night.

Signage Design

Talkeetna Connection plans on having only one sign, which will be attached to the front of the building. As a retail marijuana store, the sign shall be no larger than 4800 square inches. When complete, the sign shall say "Talkeetna Connection" and be constructed of metallic materials. If the sign is not complete by the time of our grand opening, we will order a temporary vinyl banner to hang on the front of the store which will only be used until our permanent sign is complete. Our sign will be accompanied by one light below which will help illuminate it in the darker hours.

General Security

As per Alaska regulations, our security plan has been developed to high standards. There will be surveillance cameras placed throughout the premises, inside as well as outside, which will have sufficient lighting to be able to see discrepancies at all hours. These cameras will have extra focus on the entrances and exits of the building, as well as the cashier area. All exterior doors shall be equipped with commercial grade door handles and locks. Along with the security cameras and lighting, there will also be an alarm system installed on all exterior doors and windows which will alert staff of any unauthorized entrances. Talkeetna Connection will have policies and procedures in order to prevent any diversion of marijuana or any marijuana product. If owners or employees recognize any diversion of retail products, the local law enforcement shall be notified immediately. Law enforcement shall also be called upon for persons who continue to loiter and refuse to leave the premises. Additionally, in the unforeseen event that law enforcement is called, all disturbances shall be reported to AMCO within 24 hrs. All employees of Talkeetna Connection shall be thoroughly trained on the security plan and be competent in implementing a response as needed.

Marijuana Waste

Any expired marijuana product will be thoroughly destroyed and made unusable by mixing it with another material such as coffee grounds and then taken to the local landfill station.

Department of Commerce, Community, and Economic Development
ALCOHOL & MARIJUANA CONTROL OFFICE

[State of Alaska](#) / [Commerce](#) / [Marijuana](#) / [License Search](#) / License #27317

LICENSE NUMBER 27317

[Return to Search Results](#)

License Number: 27317

License Status: Delegated

License Type: Retail Marijuana Store

Doing Business As: Talkeetna Connection

Business License Number: [2115576](#)

Issue Date:

Effective Date:

Expiration Date:

Email Address: alysatunnell79@gmail.com

Physical Address: 39209 S Talkeetna Spur Rd
Talkeetna, AK 99676
UNITED STATES

Licensees: The Talkeetna Canna Connection, Inc. [10144597](#)

Entity Officials: Alysa Tunnell Eastwood
Clint Tunnell

Onsite Consumption Endorsement

Status:

Approval Date:

Issue Date:

Effective Date:

Alaska Business License # **2115576****Alaska Department of Commerce, Community, and Economic Development**

Division of Corporations, Business, and Professional Licensing

PO Box 110806, Juneau, AK 99811-0806

This is to certify that

Talkeetna Connection

P.O Box 1109, Talkeetna, AK 99676

owned by

The Talkeetna Canna Connection, Inc.

is licensed by the department to conduct business for the period

February 17, 2023 to December 31, 2024
for the following line(s) of business:

44-45 - Retail Trade



This license shall not be taken as permission to do business in the state without having complied with the other requirements of the laws of the State or of the United States.

This license must be posted in a conspicuous place at the business location.
It is not transferable or assignable.

Julie Sande
Commissioner

Alaska Business License # **2115576****Alaska Department of Commerce, Community, and Economic Development**

Division of Corporations, Business, and Professional Licensing

PO Box 110806, Juneau, AK 99811-0806

This is to certify that

Talkeetna Connection

P.O Box 1109, Talkeetna, AK 99676

owned by

The Talkeetna Canna Connection, Inc.

ENDORSEMENT: 2115576 - 1

Effective February 17, 2023 through December 31, 2024

This business license has an endorsement for the physical address shown below:

39209 S Talkeetna Spur Rd, Talkeetna, AK 99676



This license shall not be taken as permission to do business in the state without having complied with the other requirements of the laws of the State or of the United States.

This license must be posted in a conspicuous place at the business location.
It is not transferable or assignable.

Julie Sande
Commissioner



State of Alaska
Department of Transportation and Public Facilities

Commercial Driveway
Approval to Construct No. 31196

Contact Name: Gerhard Hafele

Phone Number: (907) 203-5518

Owner: Gerhard Hafele

Mailing Address: P.O. Box 488
Talkeetna, Alaska 99676

Driveway Location: 39209, 39301, 39325 Talkeetna Spur Hwy, that portion of SE1/4 SE1/4 of Sec 29, T24N, R4W lying east of Parks Hwy and southeasterly of Talkeetna highway. MSB #42688

Approval to Construct Expires: 09/30/2023

This Approval to Construct (ATC) pertains only to State rights of way.

Driveway Provisions:

A copy of this ATC must be on site during construction of the driveway.

The State reserves the right to require the removal of all activities from the area if any conditions of this ATC are violated.

This ATC is not a property right but a temporary authorization revocable by the State upon violation of any provision, special conditions, or other reasons. All reasonable attorney's fees and costs associated with legal or enforcement actions related to the terms and conditions of this ATC will be borne by the Owner.

A driveway constructed within the State rights of way is the property of the State, but all cost and liability arising from the construction, operation, or maintenance of a driveway is at the sole expense of those lands served. The Department is not obligated to change its maintenance practices to accommodate a driveway or to incur any additional expense removing snow berms or other obstructions from a driveway within the rights of way resulting from the Department's activities, or activities under a permit issued under 17 AAC 15 (Utility and Railroad Permits).

Any survey monument or monument accessory which will be disturbed or destroyed during construction of the driveway, will be referenced prior to the disturbance and restored or replaced by a Land Surveyor licensed in the State of Alaska. The Land Surveyor must file a Monument Record in accordance with AS 34.65.040. All Monument Records will be reviewed by the Department prior to filing with the District Recorder.

The Owner is responsible for adjusting, relocating or removing the access, without cost or liability to the Department, if the use or safety of the highway requires.

The Owner will obtain all necessary Federal, State, and Municipal permits and licenses required by law. Note: for relocation or adjustment of any utility within State rights of way a Department of Transportation Utility Permit (or amendment as appropriate) is required.

The Owner will pay all taxes and special assessments lawfully imposed upon the permitted area and pay other fees and charges assessed under applicable law.

Placement of fill material in waters of the U.S., including wetlands and streams, requires prior authorization from the U.S. Army Corps of Engineers (USACE). It is the responsibility of the Owner to contact the USACE before filling activities take place.

The Owner will construct and maintain the driveway in such a manner that the highway, and all of the highway's appurtenances or facilities, are not impaired or endangered in any way. These include drainage facilities, pipes, culverts, ditches, traffic control devices, street lights, pathways, and sidewalks.

If, as a result of this project, any improvements within the State owned rights of way are damaged, the Owner will be responsible for returning them to their previous condition.

No person will place, leave, or deposit upon any street, avenue, alley, sidewalk, or other public rights of way any snow or ice which has been removed from a private driveway, private parking area, or the adjacent property. The Owner is responsible for their snow removal contractor's actions concerning placement of snow from Owner's property.

The Owner will indemnify, defend and hold harmless the State, and its officers, employees, and contractors, from any and all claims or actions resulting from injury, death, loss, or damage sustained by any person or personal property resulting directly or indirectly from Owner's use of or activities in the area.

No equipment or materials are permitted on the shoulder during non-working hours.

All litter and debris generated as a result of this project must be removed and properly disposed of by the contractor. The Owner will be responsible for any and all costs incurred by the State associated with clean up or restoration of the State rights of way.

If a culvert is required by this ATC, the following applies:

- Culvert length and ditch depth, as stipulated in the Design Criteria, are generally based on information supplied by the applicant and must be verified on site during construction,
- Culvert and culvert ends must be installed at time of the driveway construction,
- Ensure a minimum ground cover of 12-inches is over the culvert prior to paving,
- Maintain existing drainage patterns.

The Owner will pave from the edge of pavement to the radius return or the distance stipulated by the Design Criteria "Landing length." Prior to paving, saw-cut 12-inches from the edge of pavement and tack coat.

A permit for this driveway will be issued only after construction is complete and accepted by the Department of Transportation and Public Facilities.

Design Criteria:

A.	Driveway width	24	Feet
B.	Left edge clearance	600	Feet
C.	Right edge clearance	800	Feet
D.	Left return radius	40	Feet
E.	Right return radius	40	Feet
F.	Shoulder width	6	Feet
G.	Approach angle	90	Degrees
H.	Curb type	N/A	
I.	Curb to sidewalk distance	N/A	
J.	Left driveway foreslope	2:1	Or match existing
K.	Right driveway foreslope	2:1	Or match existing
L.	Culvert length	N/A	Feet (field fit)
M.	Landing grade	2	Percent
N.	Landing length	30	Feet
O.	Culvert size	N/A	Inches
P.	Culvert type	N/A	
Q.	Ditch depth	N/A	Feet
R.	Shoulder type	Gravel	
S.	Road surface type	Paved	
T.	Driveway surface type	Asphalt	

Traffic Control for Driveway Construction:

The Owner or their contractor shall obtain a Lane Closure Permit for any work that closes traffic lanes, roadway shoulder, or pathway/sidewalk.

Short term or short duration work within the right of way does not typically require a Lane Closure Permit provided it conforms to the table below:

ATC No. 31196

	WORK ON THE SHOULDER	WORK BEYOND SHOULDER (ROADSIDE)
DURATION	Less than 1 hour	Less than one day
TIME OF DAY	Daylight conditions only	
BASIC TRAFFIC SAFETY REQUIREMENTS	Use high intensity rotating, flashing, oscillating or strobe light on top of all vehicles, visible for 700 feet or more in all directions. Wear orange work vests labeled as meeting ANSI/ISEA 107-2004 Class 2, with Level 2 retroreflective material	
WORK EXAMPLES	Offloading equipment to the roadside. Matching paved driveway to edge of roadway pavement. Sweeping the roadway shoulder.	

Do not park equipment within 30 feet of the edge of traveled way or within 10 feet of a curb when not working.

Special Conditions:

- Driveway paving limits shall be constrained to the design criteria listed here-in

Incorporated as part of this ATC:

- DOT&PF approved driveway site plan

I, Gerhard Hafele, the Owner, acknowledge and accept that I will comply with all the provisions and conditions of this ATC.

Owner Signature

Date

DOT&PF Signature

Date

Site Plan





THE STATE
of **ALASKA**
GOVERNOR MIKE DUNLEAVY

Department of Public Safety

DIVISION OF FIRE AND LIFE SAFETY
Plan Review Bureau - Anchorage

5700 East Tudor Road
Anchorage, Alaska 99507-1225
Main: 907.269.2004
Fax: 907.269.0098

August 19, 2022

Alysa Tunnell Eastwood
Talkeetna Connection
PO Box 1103
Talkeetna, AK 99676

SUBJECT: Talkeetna Connection (39209 S. Talkeetna Spur
Rd.) - Full
CITY: Talkeetna
PLAN REVIEW: 2021Anch1476
OCCUPANCY: M
2012 INTERNATIONAL BUILDING AND FIRE CODE

Dear Alysa Tunnell Eastwood:

Plans for the subject facility have been reviewed by this office for conformity with the State Fire Safety Regulations and are hereby approved as submitted. Enclosed is a certificate of approval that must be posted on the premises until completion of the above facility. You are prohibited to occupy this building until construction is completed as approved. Any changes to the approved plans must be submitted to this office for review and approval.

Approval of submitted plans is not approval of omissions or oversights by this office or noncompliance with any applicable regulations of the Municipal Government. The plans have not been reviewed for compliance with the federal Americans with Disabilities Act or structural requirements.

It must be understood that the inclusion of and compliance with State Fire Safety Regulations does not preclude the necessity of compliance with the requirements of local codes and ordinances.

If we can be of further assistance in this matter, please feel free to contact us at the address above.

Sincerely,

A handwritten signature in black ink, appearing to read "Greg Medina".

Greg Medina
Plans Examiner 1

Enclosure: Approval Certificate

State of Alaska
Office of the State Fire Marshal
Plan Review

This is to certify that the plans for this building were reviewed by the *State Fire Marshal* on August 19, 2022 for conformance with AS 18.70.010 -- 100; 13 AAC 50.027.

This certificate shall be posted in a conspicuous place on the premises named Talkeetna Connection (39209 S. Talkeetna Spur Rd.) and shall remain posted until construction is completed.

NOTICE: Any changes or modifications to the approved plans **must** be resubmitted for review by the *State Fire Marshal*.

Plan Review #: **2021Anch1476**

By: 

Authority: AS 18.70.080
Form: 12-741
(6/01)

Full

Greg Medina
Plans Examiner 1



HANSON
LAND SOLUTIONS
305 E. FIREWEED AVE.
PALMER, ALASKA 99645
(907) 746-7738



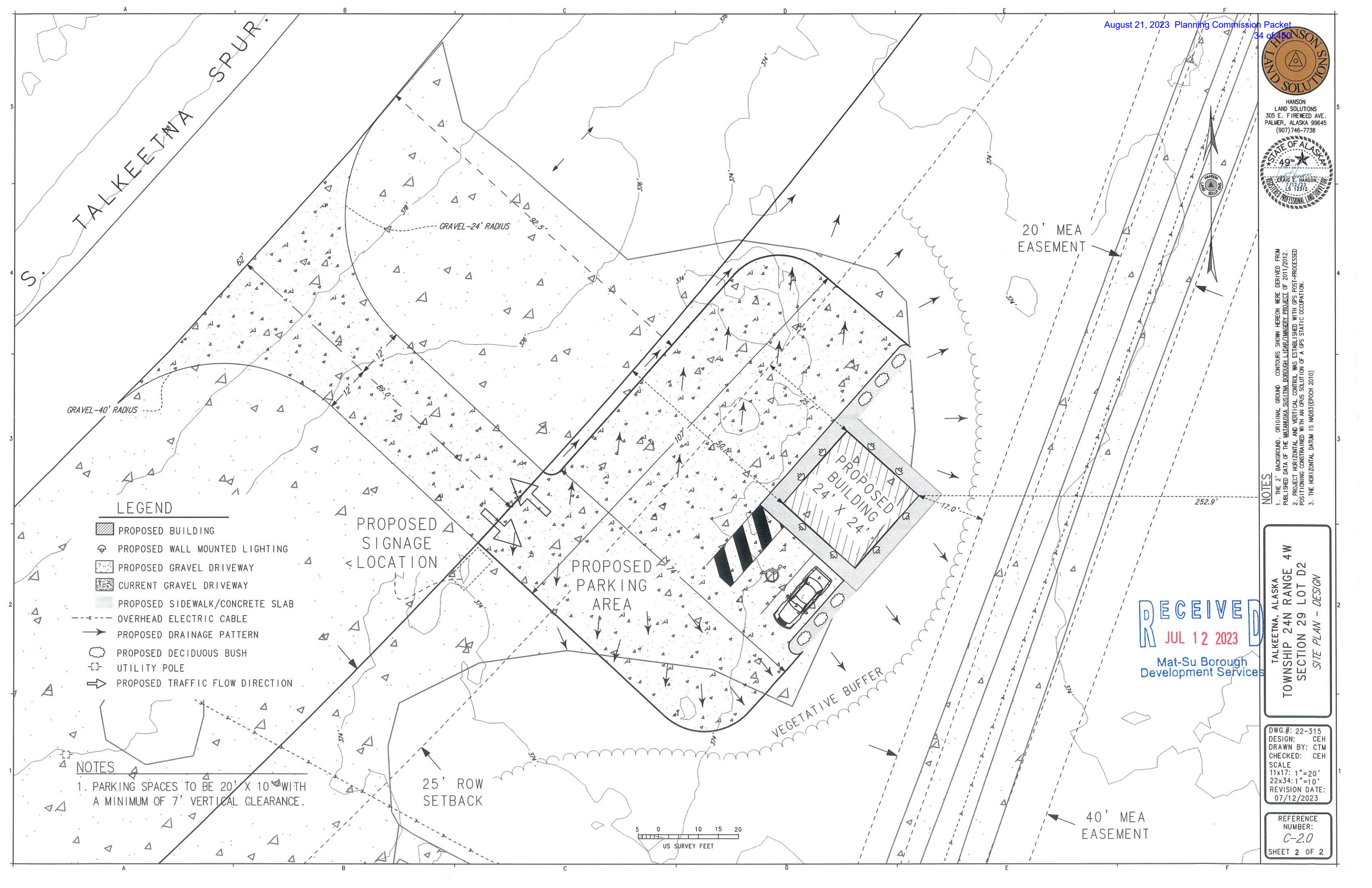
NOTES
1. THE 2' BACKGROUND, ORIGINAL GROUND CONTOURS SHOWN HEREON WERE DERIVED FROM PUBLISHED DATA OF THE WATANAIKSA SUSITNA BOROUGH LLADAR/IMAGERY PROJECT OF 2011/2012.
2. PROJECT HORIZONTAL AND VERTICAL CONTROL WAS ESTABLISHED WITH GPS POST-PROCESSED POSITIONING CONSTRAINED WITH AN GNSS SOLUTION OF A GPS STATIC OCCUPATION.
3. THE HORIZONTAL DATUM IS NAD83(EPOCH 2010)

TALKEETNA, ALASKA
TOWNSHIP 24N RANGE 4W
SECTION 29 LOT D2
SITE PLAN - DESIGN

DWG.#: 22-315
DESIGN: CEH
DRAWN BY: CTM
CHECKED: CEH
SCALE
11x17: 1"=20'
22x34: 1"=10'
REVISION DATE:
07/12/2023

REFERENCE
NUMBER:
C-2.0
SHEET 2 OF 2

RECEIVED
JUL 12 2023
Mat-Su Borough
Development Services





HANSON
LAND SOLUTIONS
305 E. FIREWEED AVE.
PALMER, ALASKA 99645
(907) 746-7738



RECEIVED
JUL 12 2023
Mat-Su Borough
Development Services



TRACT A

MONTANA
CREEK
ADDITION
NO. 3
(70-5T)

LEGEND

- ⊕ RECOVERED 2" ALUMINUM CAP
- ⊙ RECOVERED PLASTIC CAP
- RECOVERED REBAR
- ⊙ SET PLASTIC CAP
- ANCHOR GUY
- ⊙ CLEANOUT
- ⊙ UTILITY POLE
- ⊙ WATER WELL
- ⊙ VALVE BOX (WATER)
- ⊙ METER (ELECTRIC)
- ⊙ TANK (ABOVE GROUND)
- ⊙ LIGHT POLE
- ⊙ SIGN (PRIVATE)
- ⊙ SIGN (PUBLIC)
- ⊙ PEDESTAL (ELECTRIC)
- ⊙ PEDESTAL (TELEPHONE)
- ⊙ PLANTER
- ⊙ UTILITY POLE
- ⊙ MANHOLE (TELEPHONE)
- ▨ BUILDING
- ▨ DECK
- ▨ CURRENT GRAVEL DRIVEWAY/ TRAIL
- ▨ PROPOSED GRAVEL DRIVEWAY
- ▨ CURRENT ASPHALT ROAD
- OVERHEAD ELECTRIC CABLE

NOTES

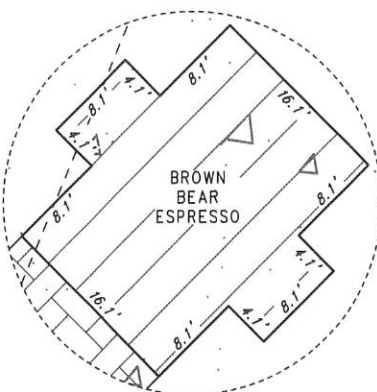
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2. PROJECT HORIZONTAL AND VERTICAL CONTROL WAS ESTABLISHED WITH GPS POST-PROCESSED POSITIONING CONSTRAINED WITH AN OPUS SOLUTION OF A GPS STATIC OCCUPATION.
3. THE HORIZONTAL DATUM IS NAD83(EPOCH 2010)

TALKEETNA, ALASKA
TOWNSHIP 24N RANGE 4W
SECTION 29 LOT D2
SITE PLAN - DESIGN

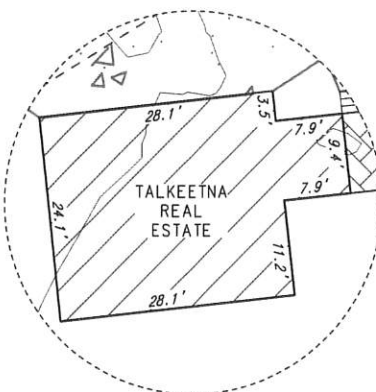
DWG.#: 22-315
DESIGN: CEH
DRAWN BY: CTM
CHECKED: CEH
SCALE
11x17: 1"=120'
22x34: 1"=60'
REVISION DATE:
07/12/2023

REFERENCE
NUMBER:
C-2.0
SHEET 1 OF 2

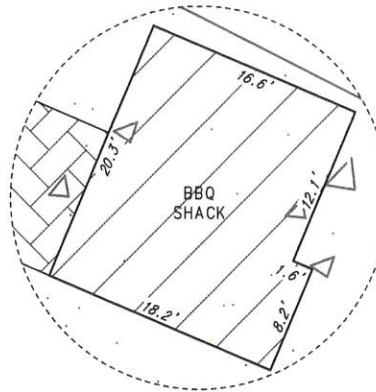
MT.
MCKINLEY
VIEW PLAZA
(84-25)



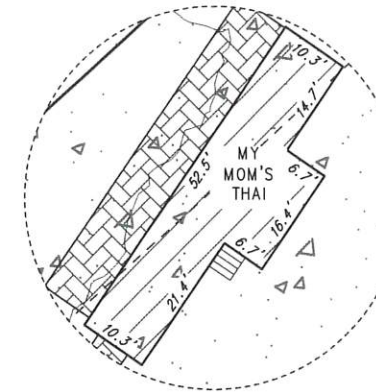
DETAIL 1
(N.T.S.)



DETAIL 2
(N.T.S.)



DETAIL 3
(N.T.S.)

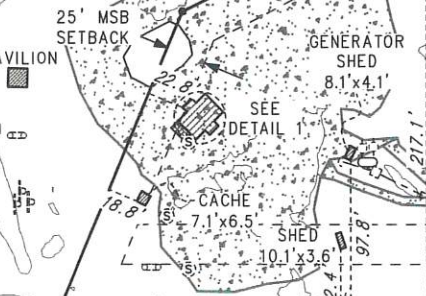


DETAIL 4
(N.T.S.)

FERGUSON
(85-34)
TRACT A

MT. MCKINLEY
VIEW PLAZA
(84-25)

PARKS
HIGHWAY



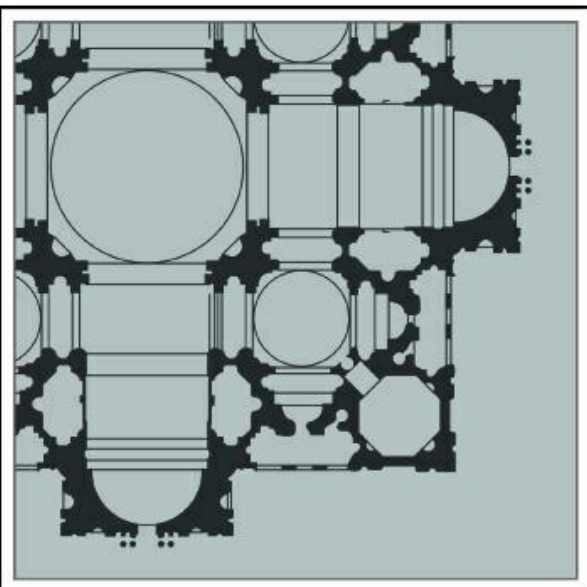
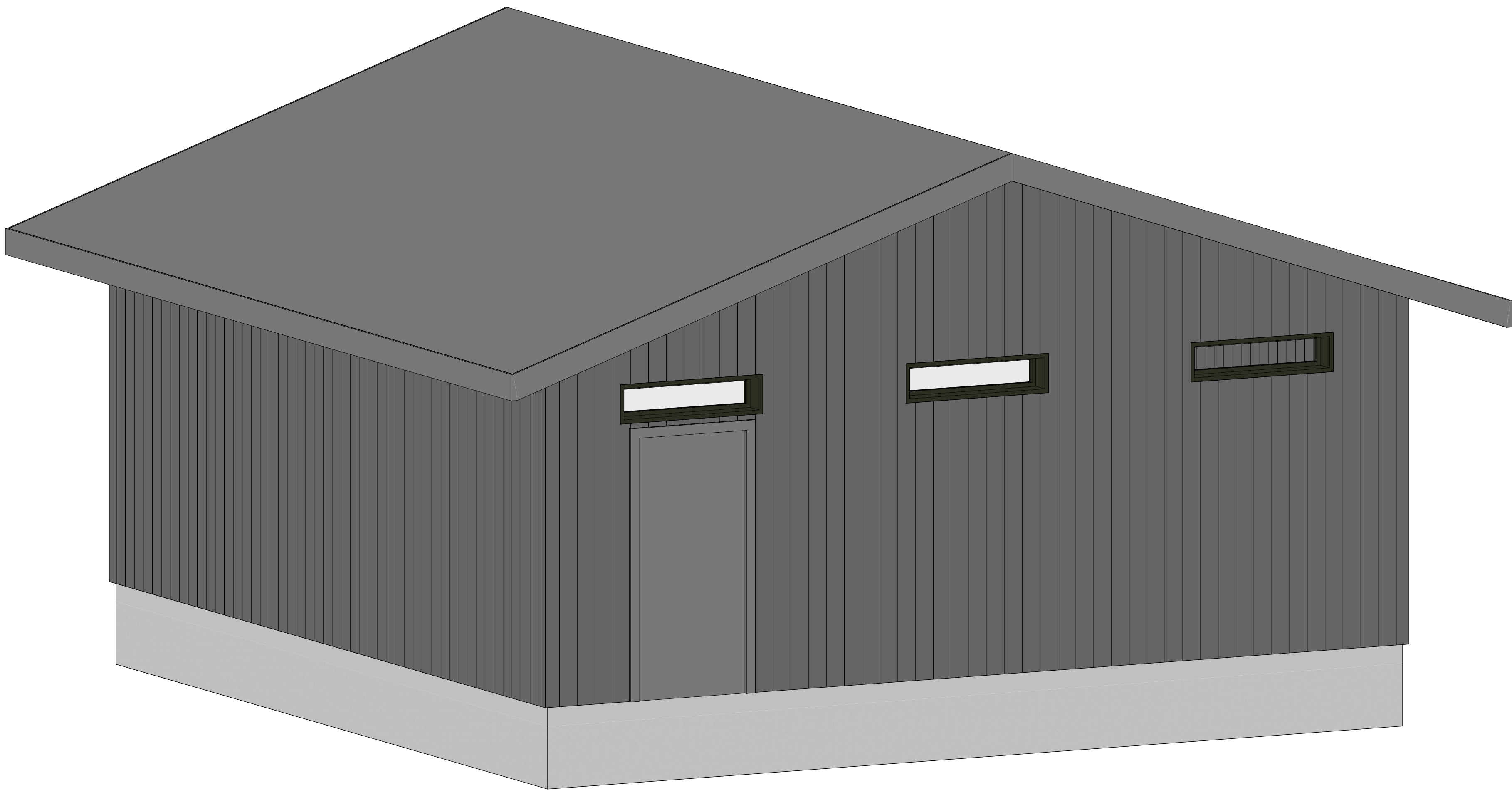
TALKEETNA
SEE SHEET 2

SPUR

25' MSB
SETBACK
20' MEA
EASEMENT
40' MEA
EASEMENT

25' MSB
SETBACK
33'
SECTION
LINE
EASEMENT
25' MSB
SETBACK
33'
SECTION
LINE
EASEMENT

ALL STEEL
24 x 24,
FAIRBANKS, AK



ALVIN REINHARD FRITZ
ARCHITECT
INC.

KEYPLAN:

LEGAL DESCRIPTION:

CLIENT INFORMATION:

#	Description	Date
Revision Schedule		

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE



Norland Coach House
#10 90001 Range Road 212,
Lethbridge County, Alberta
T1J 5N9
alvinfrizarchitect.com

ph. (403) 320-8100
Fax (403) 327-3373
general@alvinfrizarchitect.com

PROFESSIONAL SEAL:

PROJECT :
All Steel 24 x 24

LOCATION :
Fairbanks , AK

This design is and shall at all times remain the exclusive property of **ALVIN REINHARD FRITZ ARCHITECT INC.** and shall not be reproduced or used without the architect's written permission.

Do not scale this drawing. All dimensions, data and levels, shall be verified prior to construction and all errors or omissions shall be reported to the architect immediately.

DRAWING TITLE:
Cover Sheet

DRAWN BY : TO	CHECKED BY : DK
SCALE : 12" = 1'-0"	
PROJECT : 22236 NASHD	
ISSUE FOR: Issue for Review	
ISSUE DATE: 2022 06 06	
REV. NO.	SHEET NO. A0.0

PROJECT DESCRIPTION :

ALL STEEL
24 x 24

LEGAL ADDRESS:

ALL STEEL 24 x 24
FAIRBANKS, ALASKA

MUNICIPAL ADDRESS:

OWNER:
ALL STEEL INC.
JUSTIN SHEPARD
PHONE: 907.479.6002 /
907.373.6002

PLANNING DEPARTMENT:
N/A

BUILDING SAFETY DEPARTMENT:
N/A

ARCHITECT:
ALVIN REINHARD FRITZ ARCHITECT INC.
DAN KAIN
#10 90001 RR212
LETHBRIDGE COUNTY, ALBERTA T1J 5N9
E-MAIL: dkain@alvinfritzarchitect.com
PHONE: (403) 320-8100
FAX: (403) 327-3373

CIVIL ENGINEER:
N/A

STRUCTURAL ENGINEER:
N/A

MECHANICAL ENGINEER:
N/A

ELECTRICAL ENGINEER:
N/A

GENERAL NOTES :

1. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS & INSPECTIONS REQUIRED FOR CERTIFICATE OF OCCUPANCY. ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE **NBC-2019-AE**, LAWS, RULES, & REGULATIONS OF ALL LEGALLY CONSTITUTED PUBLIC AUTHORITIES HAVING JURISDICTION. IN CASE OF CONFLICT BETWEEN REQUIREMENTS, THE MOST RESTRICTIVE SHALL APPLY.

2. THE CONTRACTOR IS RESPONSIBLE TO PROVIDE TEMPORARY SHORING AND BRACING FOR ALL STRUCTURAL ELEMENTS AS REQUIRED UNTIL NEW STRUCTURAL MEMBERS ARE PERMANENTLY INSTALLED. WHETHER INDICATED ON THE DRAWINGS OR NOT, IF THE CONTRACTOR IS UNSURE WHETHER OR NOT TO PROVIDE TEMPORARY SHORING AND BRACING, THE CONTRACTOR SHALL REQUEST INFORMATION FROM THE ARCHITECT OR STRUCTURAL ENGINEER. IN WRITING, PRIOR TO COMMENCEMENT OF WORK.

3. CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONDITIONS AND CRITICAL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK. CONTRACTOR SHALL NOTIFY OWNER & ARCHITECT OF ANY DISCREPANCIES PRIOR TO PROCEEDING WITH WORK. THIS VERIFICATION SHALL INCLUDE BUT IS NOT LIMITED TO LOCATION OF NEW FRAMING MEMBERS, LINES OF SUPPORT, LOCATIONS OF ANCHOR BOLTS, HOLD DOWNS, EXISTING SITE CONDITIONS, AND UTILITIES PRIOR TO ORDERING MATERIALS.

4. CONTRACTOR TO VERIFY FIT & FINISH REQUIREMENTS FOR ALL PROJECT COMPONENTS, WITH OWNER, PRIOR TO ORDERING MATERIALS. REPORT CONFLICTING INFORMATION TO OWNER PRIOR TO PROCEEDING WITH WORK.

5. THE GENERAL CONTRACTOR SHALL DISPOSE OF ALL DEMOLITION & CONSTRUCTION DEBRIS AS REQUIRED BY PROVINCIAL & MUNICIPAL REQUIREMENTS. NO CONSTRUCTION RELATED WASTE PRODUCT SHALL BE ALLOWED TO ENTER THE SANITARY SEWER OR THE STORM SEWER. GROUTS AND CEMENTITIOUS MATERIALS MUST BE DISPOSED IN LANDFILL.

6. THE GENERAL CONTRACTOR SHALL ENSURE THAT ALL MATERIALS, AS REQUIRED BY CODE, ARE TESTED BY INDEPENDENT LABORATORIES AND THAT RESULTS ARE FURNISHED TO LOCAL BUILDING AUTHORITIES, OWNER, AND THE PROJECT CONSULTANTS IF REQUESTED.

7. CONTRACTOR SHALL STENCIL/LABEL ON ALL RATED WALLS IN CONCEALED AREAS THE FOLLOWING: "FIRE AND SMOKE BARRIER - PROTECT ALL OPENINGS"

8. BY THE USE OF THE DRAWINGS FOR CONSTRUCTION OF THE PROJECT, THE OWNER REPRESENTS THAT THEY HAVE REVIEWED AND APPROVED THE DRAWINGS, AND THAT THE CONSTRUCTION DOCUMENT PHASE OF THE PROJECT IS COMPLETE.

9. CONTRACTOR SHALL COORDINATE THEIR WORK WITH ALL OTHER CONTRACTORS FURNISHING THE LABOR, MATERIALS, AND ALL WORK, SO THAT THE WORK AS A WHOLE SHALL BE EXECUTED AND COMPLETED WITHOUT CONFLICT OR DELAY. THE OWNER AND THE CONSULTANTS SHALL NOT BE RESPONSIBLE FOR HOW THE WORK IS PERFORMED, SAFETY AND NEGLIGENT ACTS OR OMISSIONS BY THE GENERAL CONTRACTOR OR THE SUBCONTRACTORS ON THE JOB.

10. CONTRACTOR SHALL COORDINATE THE REQUIREMENTS OF ANY AND ALL DRAWINGS INCLUDING, BUT NOT LIMITED TO, ARCHITECTURAL, STRUCTURAL, MECHANICAL, ELECTRICAL, AND CIVIL. ANY CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER & ARCHITECT PRIOR TO ANY WORK.

11. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ACQUAINT THEMSELVES WITH THE DIMENSIONS OF ALL EQUIPMENT INCLUDED IN THIS PROJECT SO THAT PREPARATIONS CAN BE MADE TO PROVIDE ENTRY INTO THE FACILITY WITH SUFFICIENT CLEARANCE, AND TO ENSURE THAT ADEQUATE FLOOR SPACE IS AVAILABLE.

12. CONTRACTOR SHALL NEVER SCALE DRAWINGS. LOCATIONS FOR ALL PARTITIONS, WALLS, CEILINGS, ETC. WILL BE DETERMINED BY DIMENSIONS ON THE DRAWINGS. ANY SUCH DIMENSIONS MISSING FROM THE PLANS MUST BE BROUGHT TO THE ATTENTION OF THE OWNER & ARCHITECT IMMEDIATELY.

13. THE CONTRACTOR SHALL ADHERE TO THE DRAWINGS AND SPECIFICATIONS. SHOULD ANY ERROR OR INCONSISTENCY APPEAR REGARDING THE TRUE MEANING AND/OR INTENT OF THE DRAWINGS OR SPECIFICATIONS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ARCHITECT WHO WILL MAKE ANY NECESSARY CLARIFICATION, INTERPRETATION, OR REVISION AS REQUIRED. IF THE CONTRACTOR DISCOVERS AN ERROR OR INCONSISTENCY AND PROCEEDS WITH WORK WITHOUT NOTIFYING THE OWNER & ARCHITECT OF ANY SUCH DISCREPANCIES, THE CONTRACTOR SHALL ASSUME ALL CHARGES AND MAKE ANY CHANGES TO HIS WORK MADE NECESSARY BY THE CONTRACTORS FAILURE TO OBSERVE AND/OR REPORT THE CONDITION.

14. IF THE INTENT OF THE DRAWINGS & SPECIFICATIONS ARE UNCLEAR, THE CONTRACTOR SHALL ASK THE ARCHITECT FOR CLARIFICATION. PRIOR TO PROCEEDING WITH WORK, IN THE FORM OF A WRITTEN R.F.I. (REQUEST FOR INFORMATION), THE ARCHITECT SHALL THEN RESPOND IN WRITING TO ALL APPROPRIATE PARTIES.

15. CONTRACTOR SHALL PROVIDE ADEQUATE PROTECTION OF WORK, MATERIALS, FIXTURES, ETC. IN LEASED SPACE FROM LOSS, DAMAGE, FIRE, THEFT, ETC.

16. WHEREVER THE TERM "OR EQUAL" IS USED, IT SHALL MEAN EQUAL PRODUCT AS APPROVED IN WRITING BY ARCHITECT.

17. IF THE CONTRACTOR PROPOSES A MATERIAL OR EQUIPMENT SUBSTITUTION THE CONTRACTOR SHALL PROVIDE ALL APPROPRIATE DOCUMENTATION AND INFORMATION REQUIRED FOR THE ARCHITECT TO DETERMINE WHETHER OR NOT THE SUBSTITUTION IS EQUAL TO THE SPECIFICATION. ANY CHANGES TO THE DESIGN, AFTER ISSUANCE OF A BUILDING PERMIT, SHALL BE SUBMITTED TO THE PRESIDING BUILDING AGENCY FOR APPROVAL BY THE GENERAL CONTRACTOR.

18. CONTRACTOR SHALL PROVIDE AND INSTALL ALL NECESSARY IN-WALL FRAMING/BLOCKING REQUIRED TO CARRY SHELF, HANGING, AND VALANCE LOADS, RAILINGS, ETC. AS PER PLANS.

19. PROVIDE SILICONE SEALANT AT ALL JOINTS AND INTERFACES OF ALL COUNTERTOPS, EQUIPMENT AND WALLS.

20. PROJECT SHALL BE LEFT CLEANED, POLISHED, AND MOVE-IN READY AS PER EACH PRODUCT SPECIFICATION/RECOMMENDATIONS AFTER COMPLETION OF WORK.

21. THE CONTRACTOR SHALL VERIFY LOCATIONS OF ALL FOOD SERVICE EQUIPMENT AND COORDINATE LOCATION OF FLOOR SINKS, FLOOR DRAINS, SLOPES/SLAB DEPRESSIONS AND RAISED CURBS, ELECTRICAL AND PLUMBING STUB OUTS, AND ALL OTHER WORK UNDER THIS SCOPE OF RESPONSIBILITY RELATED TO THIS EQUIPMENT. REFER TO OWNERS FOOD SERVICE EQUIPMENT SUPPLIER FOR SPECIFIC REQUIREMENTS & REFERENCES. EQUIPMENT DRAWINGS ARE INCLUDED FOR REFERENCE ONLY. ACTUAL SHOP DRAWINGS FOR THE SPECIFIC PROJECT MAY VARY. IT IS THE CONTRACTORS RESPONSIBILITY TO COORDINATE ALL WORK WITH THE REQUIREMENTS OF THE SUPPLIERS FOR THE MATERIALS REPRESENTED BY SHOP DRAWINGS.

22. CONTRACTOR IS RESPONSIBLE FOR RECEIVING, UNLOADING, UNCRATING, INSTALLATION AND HOOK-UP OF ALL FOOD SERVICE EQUIPMENT AND OTHER OWNER FURNISHED ITEMS UNLESS OTHERWISE NOTED.

23. CONTRACTOR SHALL REFER TO THESE DOCUMENTS, AS WELL AS SPECIFICATIONS, FOR IDENTIFICATION OF ALL OWNER SUPPLIED ITEMS. CONTRACTOR SHALL VERIFY WITH OWNER, PRIOR TO ORDERING, WHICH ITEMS THE OWNER SHALL SUPPLY. ALL ITEMS NOT MARKED AS "OWNER SUPPLIED" ARE TO BE SUPPLIED BY THE CONTRACTOR. UNLESS NOTED OTHERWISE ALL ITEMS ARE TO BE INSTALLED BY GENERAL CONTRACTOR.

24. MINIMUM FLAME SPREAD CLASSIFICATION OF INTERIOR FINISHES SHALL CONFORM TO THE **NBC-2019-AE** AND LOCAL GOVERNING BUILDING CODES/ORDINANCES.

25. CONTRACTOR SHALL CONTACT THE LOCAL FIRE MARSHALL, AND PROVIDE AND INSTALL FIRE EXTINGUISHERS PER THE FIRE MARSHAL'S DIRECTION, INCLUDING TYPE, QUANTITY, AND LOCATIONS. AS A MINIMUM, CONTRACTOR SHALL PROVIDE FIRE EXTINGUISHERS HAVING A RATING OF 2-A10-BC FOR EVERY 3,000 S.F. OF FLOOR AREA AND TRAVEL DISTANCE TO AN EXTINGUISHER SHALL NOT EXCEED 75 FEET AS PER MECHANICAL REQUIREMENTS.

26. FOR CONSTRUCTION DETAILS NOT SHOWN, USE THE MANUFACTURER'S STANDARD DETAILS OR APPROVED SHOP DRAWINGS/DATA SHEETS IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS.

27. CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES PRIOR TO THE COMMENCEMENT OF WORK TO VERIFY LOCATIONS OF ALL UNDERGROUND UTILITIES. NOTIFY ARCHITECT IMMEDIATELY IF CONFLICTS EXIST BETWEEN EXISTING UTILITIES AND NEW CONSTRUCTION. PATCH, REPAIR, AND/OR REPEAL ADVERSELY AFFECTED FINISHES AND/OR SURFACES AS REQD. UPON COMPLETION OF CONSTRUCTION, ALL PARKING AREA PAVEMENT AND NEW CONCRETE PADS SHALL TRANSITION SMOOTHLY.

28. DOCUMENTS MARKED "BID DOCUMENTS" SHALL NOT BE USED FOR CONSTRUCTION.

29. POST OCCUPANT LOAD SIGN LISTED IN SPECIFICATIONS PER AUTHORITIES HAVING JURISDICTION.

30. INSTALL ADDRESS NUMBERS PER SPECIFICATIONS OR LOCAL GOVERNING AGENCY DIRECTIONS.

31. SUBMITTAL DOCUMENTS FOR DEFERRED SUBMITTAL ITEMS SHALL BE SUBMITTED TO THE ARCHITECT WHO SHALL REVIEW THEM AND PROVIDE A NOTATION INDICATING THAT THEY ARE DEFERRED.

32. SUBMITTAL DOCUMENTS HAVE BEEN REVIEWED AND FOUND TO BE IN GENERAL CONFORMANCE WITH THE DESIGN OF THE BUILDING. THESE SUBMITTAL ITEMS MUST THEN BE SUBMITTED TO THE BUILDING OFFICIAL. THE DEFERRED SUBMITTAL ITEMS SHALL NOT BE INSTALLED UNTIL THEIR DESIGN AND SUBMITTAL DOCUMENTS HAVE BEEN APPROVED BY THE BUILDING OFFICIAL. DEFERRED SUBMITTALS ARE AS FOLLOWS:
A. TRUSS DRAWINGS AND CALCULATIONS
B. EXHAUST HOOD AUTOMATIC FIRE EXTINGUISHING SYSTEM (AS REQUIRED)
C. ALARM MONITORING SYSTEM (AS REQUIRED)

33. SUBMIT SAMPLES IN TRIPLICATE FOR ALL FINISHES. LABEL SAMPLES WITH ORIGIN & INTENDED USE. A WRITTEN ENDORSEMENT WILL BE SUBMITTED BACK TO THE GENERAL CONTRACTOR WITH ACCEPTANCE OF THE SAMPLE. COORDINATE WITH ARCHITECTURAL SPECIFICATIONS SECTION 01 53 00 "SUBMITTAL PROCEDURES" POINT #1.5.

34. ALL WALLS THAT HAVE AN STC RATING OF 50 OR MORE REQUIRE ACOUSTICAL CAULKING AROUND ELECTRICAL BOXES AND OTHER OPENINGS, AS WELL AS AT THE JUNCTION OF INTERSECTING WOOD FRAMING AND STEEL STUD WALL AND FLOORS.

KEYPLAN:

LEGAL DESCRIPTION:

CLIENT INFORMATION:

Revision Schedule

CONSULTANT LOGO:

PROFESSIONAL SEAL:

PROJECT :
All Steel 24 x 24

LOCATION :
Fairbanks , AK

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DRAWING TITLE:
Project Information

DRAWN BY :
TO

CHECKED BY :
DK

SCALE : 12" = 1'-0"

PROJECT : 22236 NASHD

ISSUE FOR:

Issue for Review

ISSUE DATE:
2022 06 06

REV. NO.

SHEET NO.
A0.1

ARCHITECTURAL ABBREVIATIONS :

[illegible]

ARCHITECTURAL MATERIALS & GRAPHICS LEGEND :

MATERIAL INDICATIONS

ASPHALT ROOF

BRICK MASONRY

BRICK & STONE MASONRY - SECTION

CONCRETE MASONRY UNIT

CONCRETE MASONRY UNIT - SECTION

CONCRETE

STONE

MIRROR GLASS

GRANULAR BASE, CRUSHED ROCK

PLYWOOD

RIGID INSULATION

SAND, FILL SOIL

GYPSUM BOARD, FARGING

SPRAY FOAM

SHAKE ROOF

STEEL

WOOD FINISH

DETAIL INDICATIONS

WOOD FRAMING THROUGH MEMBER

WOOD FRAMING INTERRUPTED MEMBER

GLU-LAM BEAM

CEMENTITIOUS BACKER BD

GLASS BLOCK

OR GLASS

CARPET

CERAMIC TILE, QUARRY TILE, OR RESIL FLR (SHOW PROFILE ONLY)

LINE INDICATIONS

CENTER LINE

HIDDEN LINE

OVERHEAD LINE

SETBACK LINE

PROPERTY LINE

PEEL & STICK MEMBRANE LINE

BUILDING PAPER LINE

POLY A.V.B. LINE

ROOFING MEMBRANE

METAL LATHE

FLASHINGS

FOOTING FLASHINGS

GRAVEL FABRIC

EXTERIOR MEMBRANE (FLAT ROOF)

PEDESTRIAN DECK MEMBRANE

DAMP-PROOFING

BATT INSULATION LINE

REFERENCE INDICATIONS

COLUMN/ GRID LINE

LETTERS IN ONE DIRECTION, NUMBERS IN THE OTHER.

SURVEY LINES

FOR SURVEYOR'S GRID.

SECTIONS

MODULE IDENTIFICATION

SECTION DRAWING LOCATION ON SHEET

DETAIL CALLOUTS

MODULE IDENTIFICATION

DETAIL DRAWING LOCATION ON SHEET

DRAWING TITLES

MODULE IDENTIFICATION

DRAWING NAME

DRAWING SCALE

ORIGIN DRAWING LOCATION ON SHEET

REVISIONS

REVISION NO.

CLOUD AROUND REVISION

ELEVATIONS

ELEVATION MODULE IDENTIFICATION

ELEVATION LOCATION ON SHEET

ELEVATIONS UNFOLD CLOCKWISE

NO NUMBER INDICATES NO ELEV.

ROOM IDENTIFICATION - TYPE 1

ROOM NAME

ROOM NUMBER

ROOM MATERIAL CODE

1st (number) - floor/ceiling

2nd (letter) - walls/wainscot

3rd (number) - ceiling

EQUIPMENT TYPE

EQUIPMENT GROUP

EQUIPMENT ITEM

(LETTERS I AND O OMITTED IN ALL)

RED TEXT & BOXES REFLECT BUILDING CODE REQUIREMENTS

GREEN TEXT & BOXES REFLECT ENERGY CODE REQUIREMENTS

BLUE TEXT & BOXES REFLECT SPECIALIZED NOTES

FLOOR PLAN INDICATIONS

NEW FINISH GRADE DRAWN * 45 DEG.

EXISTING GRADE DRAWN * 45 DEG.

MAJOR PROPOSED OR FINISHED CONTOURS

MINOR PROPOSED OR FINISHED CONTOURS

EXISTING CONTOURS

WORK POINT, CONTROL POINT, OR DATUM POINT

TOP OF WALL

TOP OF CURB

TOP OF PAVEMENT

RECESSED FIXTURE

SEMI-RECESSED FIXTURE

SURFACE MOUNTED FIXTURE

* SEE ABBREVIATIONS FOR FIXTURE TYPE

CEILING TRANSITION NUMBER INDICATES CEILING HEIGHTS

DIMENSION TO CENTER LINE - TYPICAL

DIMENSION TO FACE OF STUD OF EXTERIOR WALLS TYPICAL UNLESS OTHERWISE NOTED

DIMENSION TO CENTER LINE OF STUDS OF INTERIOR PARTITIONS TYPICAL UNLESS OTHERWISE NOTED

FIXTURE OR SPACE DESIGNED FOR HANDICAPPED ACCESS

WALL & OPENING INDICATIONS

DOOR

WALL OR PARTITION WHERE OCCURS 4" TO DOOR SIDE OF JAMB, U.O.N.

DOOR NUMBER -SEE DOOR SCHEDULE

WINDOW / BORROWED LITE

PLAN INDICATION

WINDOW MARK -SEE SCHEDULE

BORROWED LITE MARK -SEE SCHEDULE

LOUVER

PLAN INDICATION

LOUVER MARK - SEE SCHEDULE

FURRING TYPES -SEE EXTERIOR WALL & PARTITION CONSTRUCTION SCHEDULE

INTERIOR PARTITION TYPES & EXTERIOR WALL TYPES -SEE EXTERIOR WALL & PARTITION CONSTRUCTION SCHEDULE

REFLECTED CEILING PLAN INDICATIONS

LIGHT FIXTURE - RECESSED

LIGHT FIXTURE - SURFACE MOUNTED

SURFACE OR SUSPENDED FLUORESCENT STRIPLITE (1 LAMP)

FLUORESCENT LAY-IN FIXTURES

FLUORESCENT SURFACE MOUNTED FIXTURE

FIRE ALARM RATE OF RISE DETECTOR

FIRE ALARM FIXED TEMPERATURE DETECTOR

FIRE ALARM IONIZATION SMOKE DETECTOR

SUPPLY REGISTER

RETURN REGISTER

ACCESS PANEL

SOUND SYSTEM SPEAKER LOCATION

FIRE SPRINKLER

EXIT LIGHT

PROJECTION SCREEN - RECESSED

PROJECTION SCREEN - SURFACE MOUNTED

CLIENT INFORMATION:

Description Date

Revision Schedule

CONSULTANT LOGO:

NO. BY DESCRIPTION DATE

ALVIN REINHARD FRITZ ARCHITECT INC.

PROFESSIONAL SEAL:

PROJECT: All Steel 24 x 24

LOCATION: Fairbanks, AK

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DRAWING TITLE:

Architectural Abbreviations & Symbols

DRAWN BY: TO

CHECKED BY: DK

SCALE: 1/4" = 1'-0"

PROJECT: 22236 NASHD

ISSUE FOR:

Issue for Review

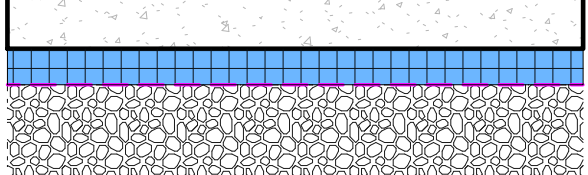
ISSUE DATE: 2022 06 06

REV. NO. SHEET NO. A0.2

ROOF TYPES :

FLOOR TYPE F1

LEVEL



6" CONCRETE SLAB
REFER TO STRUCT.
2" RIGID INSULATION
10 mil. POLY VAPOUR BARRIER
6" COMPACTED WASHED GRAVEL U.O.N. BY
STRUCT.

ROOFING PANEL
1 1/2" ROOF HATCHANNELS
R- 60 BLOWN IN INSUALTION
PRE-ENGINEERED ROOF TRUSSES -REFER TO STRUCT.

S1 PRE-FINISHED PERFORATED METAL SOFFIT

1/2 GYPSUM BOARD
3 1/2" DOUBLE STEEL STUDS @ 48" o.c.
R-30 BATT INSULATION
1/2 GYPSUM BOARD

Window Schedule								
Window No.	Frame Dimensions		Material	Head Height	Glass Pane	Operation	Comments	Count
	Width	Height						
W1	4' - 0"	1' - 0"		8' - 0 1/2"		P		3

LEGAL DESCRIPTION

CLIENT INFORMATION

Revision Schedule

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE
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ALVIN REINHARD FRITZ
ARCHITECT
INC.

PROFESSIONAL SEAL:

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DRAWING TITLE:

Construction Types

DRAWN BY :	CHECKED BY :
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TO	DK
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PROJECT: 22236 NASHD

ISSUE FOR:

ISSUE DATE:

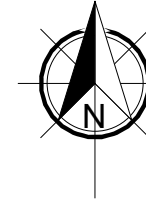
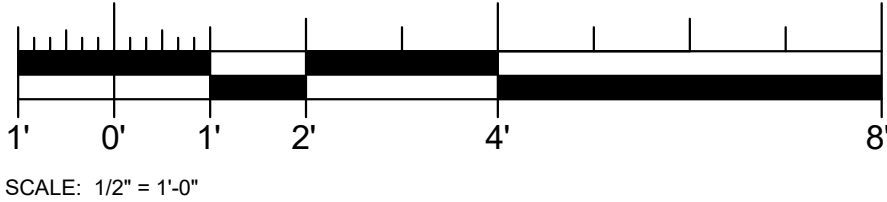
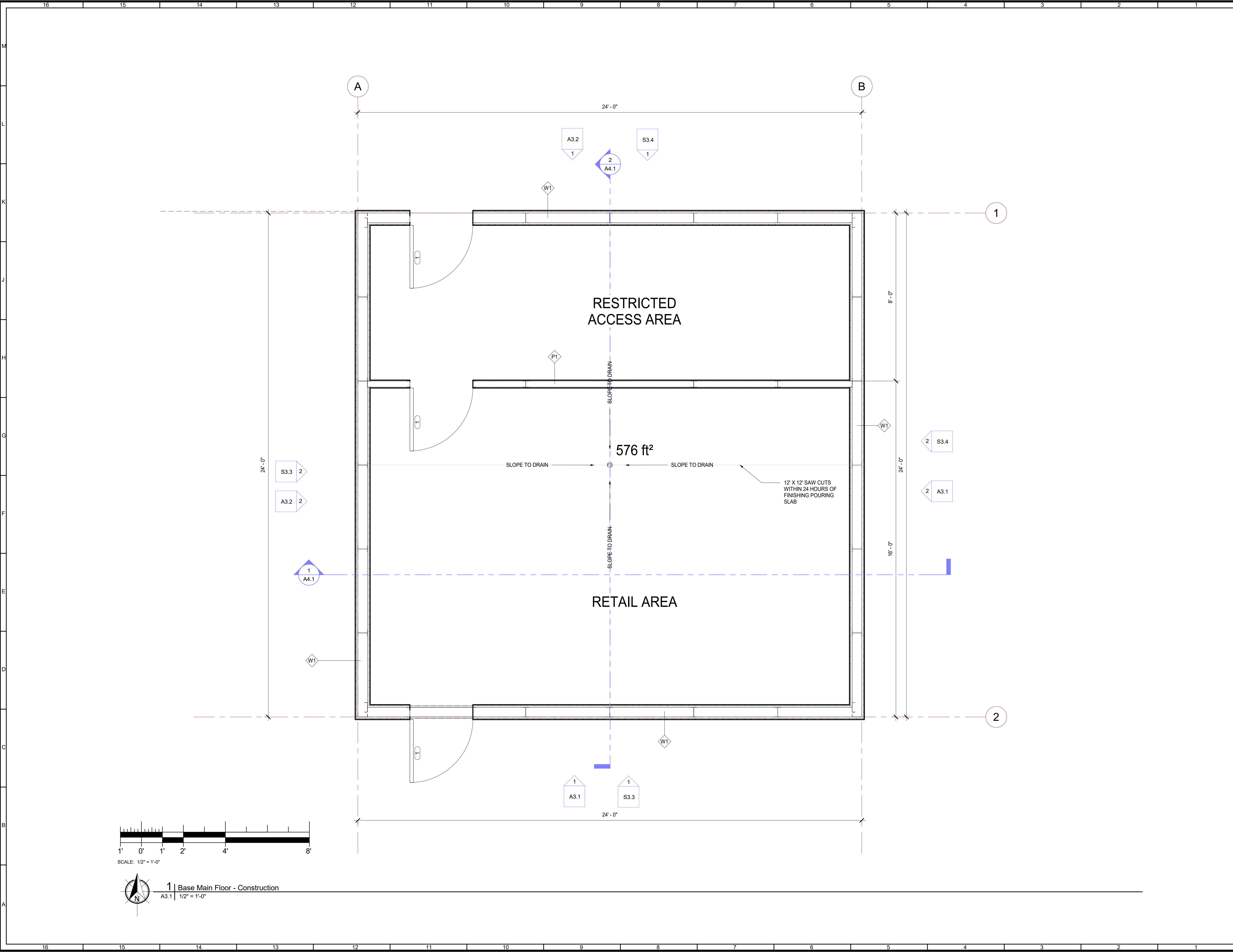
2022 06 06

REV. NO. SHEET NO.

A0.3

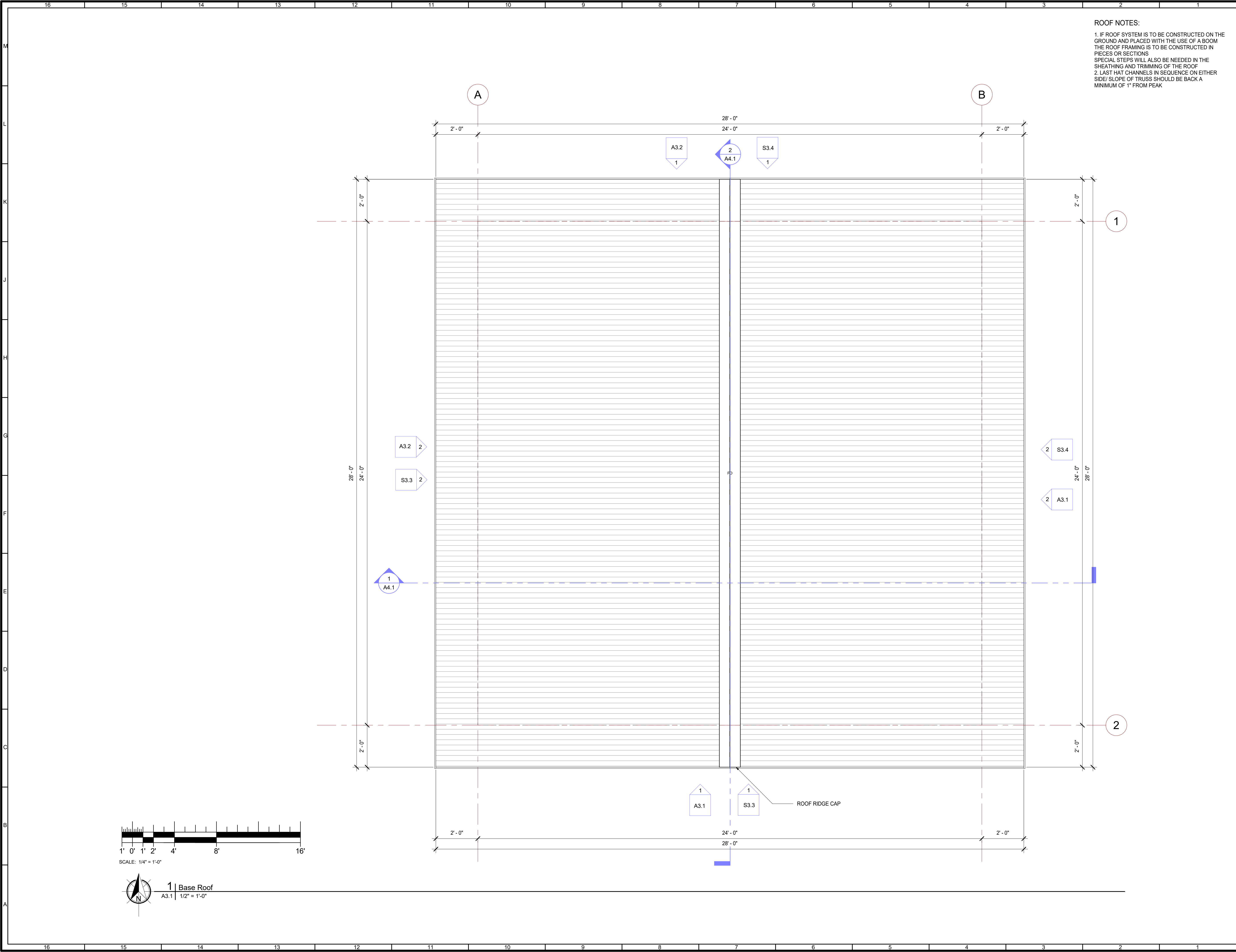
PROJECT: **All Steel 24 x 24**

Fairbanks, AK



1 | Base Main Floor - Construction
A3.1 | 1/2" = 1'-0"

KEYPLAN:
LEGAL DESCRIPTION:
CLIENT INFORMATION:
Revision Schedule
CONSULTANT LOGO:
NO. BY DESCRIPTION DATE
Norland Coach House
#10 90001 Range Road 212,
Lethbridge County, Alberta
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ALVIN REINHARD FRITZ
ARCHITECT
INC.
PROFESSIONAL SEAL:
PROJECT: All Steel 24 x 24
LOCATION: Fairbanks, AK
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DRAWING TITLE:
Main Floor Plan
DRAWN BY: TO: CHECKED BY: DK
SCALE: 1/2" = 1'-0"
PROJECT: 22236 NASHD
ISSUE FOR: Issue for Review
ISSUE DATE: 2022 06 06
REV. NO. SHEET NO. A2.2



ROOF NOTES:
1. IF ROOF SYSTEM IS TO BE CONSTRUCTED ON THE GROUND AND PLACED WITH THE USE OF A BOOM THE ROOF FRAMING IS TO BE CONSTRUCTED IN PIECES OR SECTIONS
SPECIAL STEPS WILL ALSO BE NEEDED IN THE SHEATHING AND TRIMMING OF THE ROOF
2. LAST HAT CHANNELS IN SEQUENCE ON EITHER SIDE/ SLOPE OF TRUSS SHOULD BE BACK A MINIMUM OF 1" FROM PEAK

KEYPLAN:

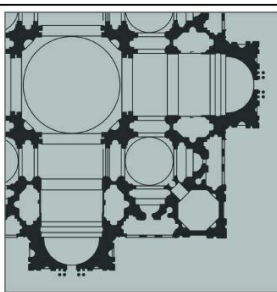
LEGAL DESCRIPTION:

CLIENT INFORMATION:

#	Description	Date
Revision Schedule		

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE



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ALVIN REINHARD FRITZ
ARCHITECT
INC.

PROFESSIONAL SEAL:

PROJECT :
All Steel 24 x 24

LOCATION :
Fairbanks , AK

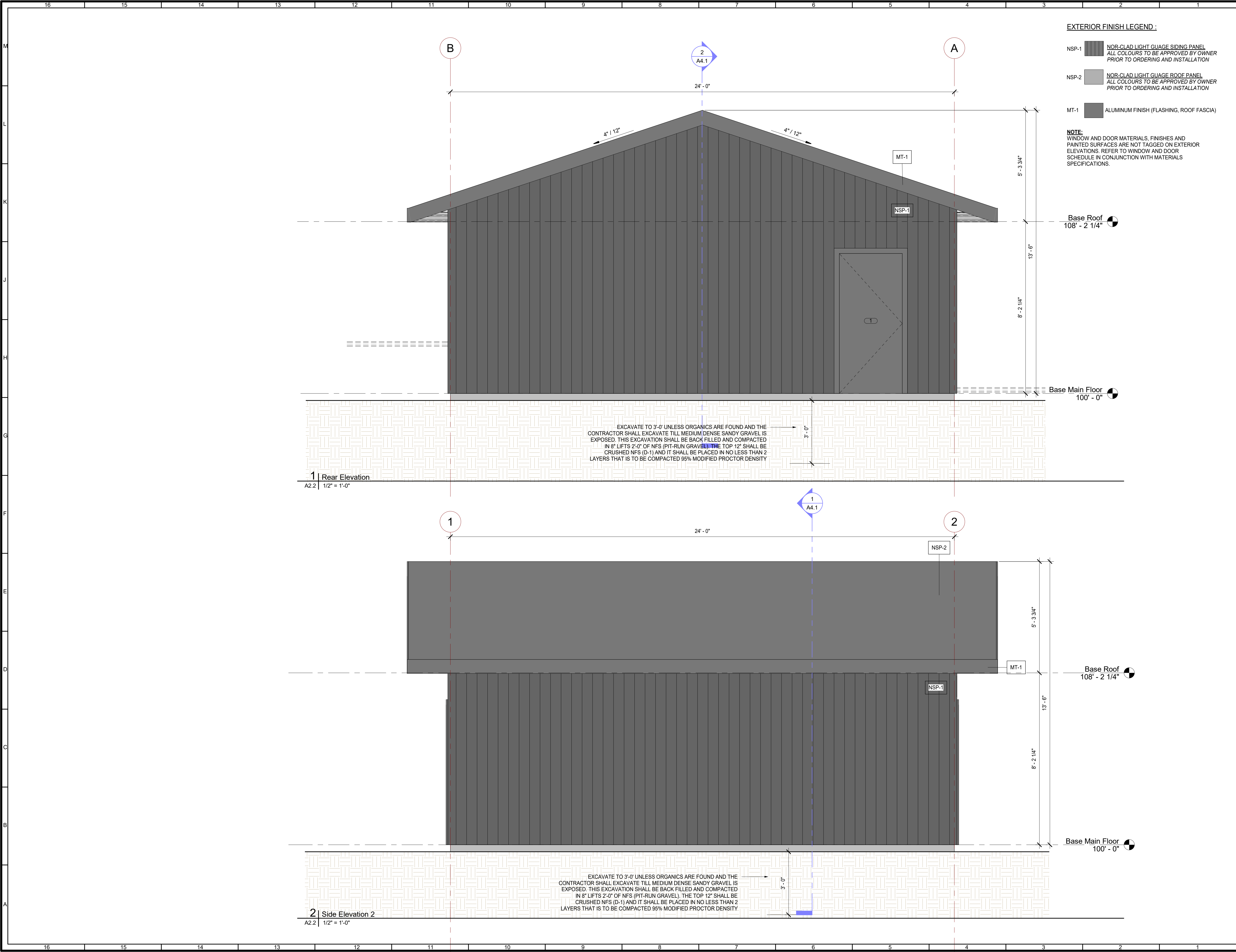
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DRAWING TITLE:
Roof Plan

DRAWN BY : TO	CHECKED BY : DK
SCALE : As indicated	
PROJECT : 22236 NASHD	
ISSUE FOR: Issue for Review	
ISSUE DATE: 2022 06 06	
REV. NO.	SHEET NO. A2.3

PROJECT : All Steel 24 x 24	LOCATION : Fairbanks , AK	DRAWN BY :	CHECKED BY :
		TO	DK
		SCALE : As indicated	
		PROJECT : 22236 NASHD	
		ISSUE FOR:	
		Issue for Review	
		ISSUE DATE:	
		2022 06 06	
		REV. NO.	SHEET NO.
			A3.1



KEYPLAN:

LEGAL DESCRIPTION:

CLIENT INFORMATION:

#	Description	Date
Revision Schedule		

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE

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ALVIN REINHARD FRITZ
ARCHITECT
INC.

PROFESSIONAL SEAL:

PROJECT :

All Steel 24 x 24

LOCATION :

Fairbanks , AK

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DRAWING TITLE:

Elevations

DRAWN BY :	CHECKED BY :
Author	Checker
SCALE :	As indicated
PROJECT :	22236 NASHD
ISSUE FOR:	Issue for Review
ISSUE DATE:	2022 06 06
REV. NO.	SHEET NO.
	A3.2

<div>PROJECT :</div> <div>ALL Steel 24 x 24</div>	<div>LOCATION :</div> <div>Fairbanks, AK</div>	<div>This design is and shall at all times remain the exclusive property of ALVIN REINHARD FRITZ ARCHITECT INC. and shall not be reproduced or used without the architect's written permission.</div>	
		<div>Do not scale this drawing. All dimensions, data and levels, shall be verified prior to construction and all errors or omissions shall be reported to the architect immediately.</div>	
		<div>DRAWING TITLE:</div> <div>Building Section</div>	
		<div>DRAWN BY :</div> <div>TO</div>	<div>CHECKED BY :</div> <div>DK</div>
		<div>SCALE : 1/2" = 1'-0"</div> <div>PROJECT : 22236 NASHD</div> <div>ISSUE FOR:</div> <div>Issue for Review</div>	
<div>ISSUE DATE:</div> <div>2022 06 06</div>			
<div>REV. NO.</div> <div>SHEET NO.</div> <div>A4.1</div>			

DRAWN BY : TO		CHECKED BY : DK	
SCALE : As indicated			
PROJECT : 22236 NASHD			
ISSUE FOR: Issue for Review			
ISSUE DATE: 2022 06 06			
REV. NO.		SHEET NO. A7 1	

LEGAL DESCRIPTION:

CLIENT INFORMATION:

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE
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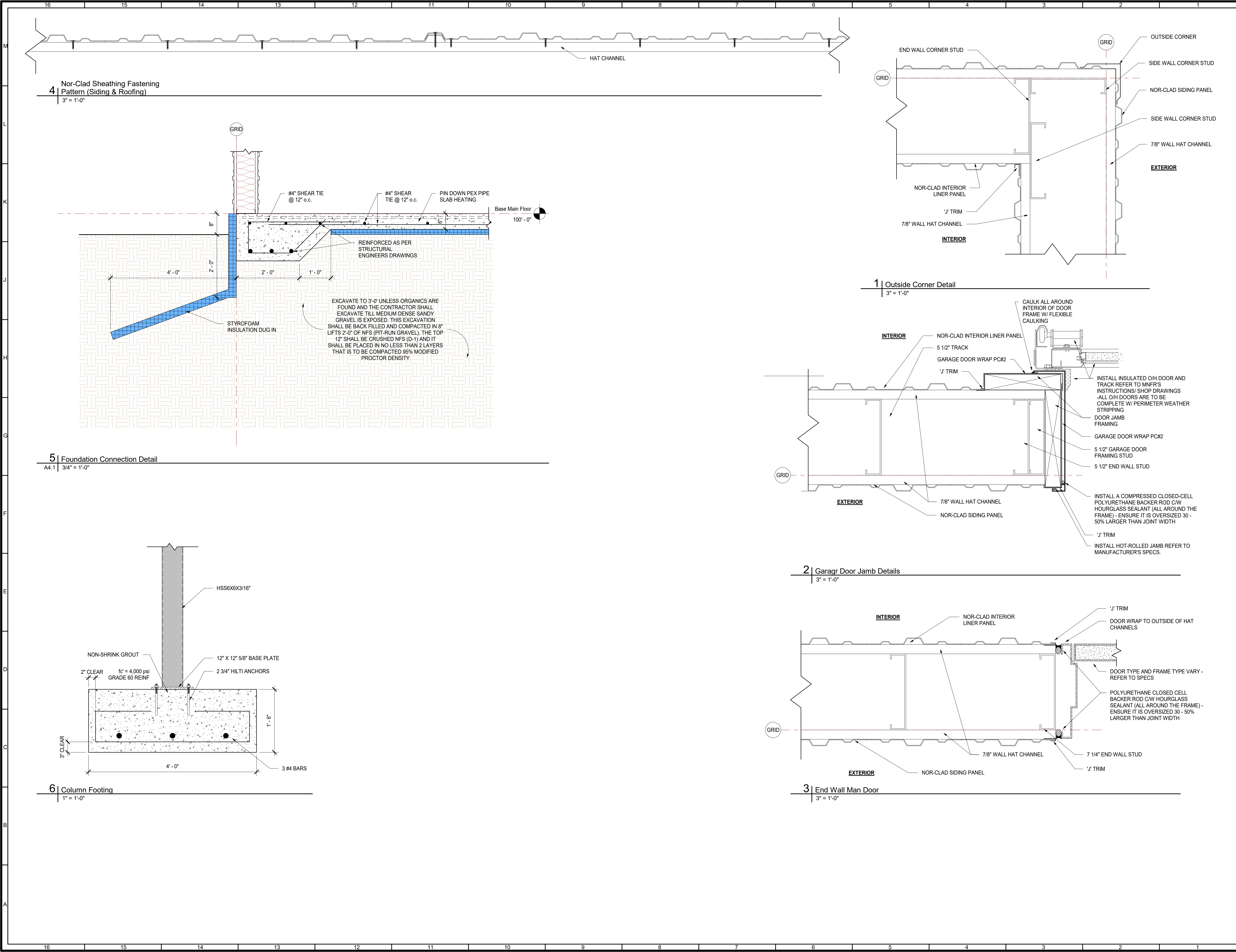
PROFESSIONAL SEAL:

Do not scale this drawing. All dimensions, data and levels, shall be verified prior to construction and all errors or omissions shall be reported to the architect immediately.

DRAWING TITLE:

**Construction Details
- Sheathing & Trim**

<div>PROJECT:</div> <div>ALL Steel 24 x 24</div>	<div>LOCATION:</div> <div>Fairbanks , AK</div>	DRAWN BY : TO		CHECKED BY : DK	
		SCALE : As indicated			
		PROJECT : 22236 NASHD			
		ISSUE FOR:			
		Issue for Review			
		ISSUE DATE:		2022 06 06	
		REV. NO.		SHEET NO.	
				A7.2	



KEYPLAN:

LEGAL DESCRIPTION:

CLIENT INFORMATION:

#	Description	Date
Revision Schedule		

CONSULTANT LOGO:

NO. BY DESCRIPTION DATE

ALVIN REINHARD FRITZ ARCHITECT INC.

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DRAWING TITLE:
Construction Details - Sheathing & Trim

DRAWN BY:
TO: DK

CHECKED BY:
DK

SCALE: As indicated

PROJECT: 22236 NASHD

ISSUE FOR:
Issue for Review

ISSUE DATE:
2022 06 06

REV. NO.
SHEET NO.

A7.3

PROJECT: All Steel 24 x 24

LOCATION: Fairbanks, AK

<div>PROJECT :</div> <div>All Steel 24 x 24</div> <div>LOCATION :</div> <div>Fairbanks , AK</div>	<div>This design is and shall at all times remain the exclusive property of ALVIN REINHARD FRITZ ARCHITECT INC. and shall not be reproduced or used without the architect's written permission.</div>
	<div>Do not scale this drawing. All dimensions, data and levels, shall be verified prior to construction and all errors or omissions shall be reported to the architect immediately.</div>
	<div>DRAWING TITLE:</div> <div>Construction Details</div> <div>- Roof</div>
	<div>DRAWN BY :</div> <div>TO</div>
	<div>CHECKED BY :</div> <div>DK</div>
	<div>SCALE: As indicated</div> <div>PROJECT : 2236 NASHD</div> <div>ISSUE FOR:</div> <div>Issue for Review</div> <div>ISSUE DATE: 2022 06 06</div> <div>REV. NO. SHEET NO.</div> <div>A7.4</div>

<div>PROJECT :</div> <div>ALL Steel 24 x 24</div> <div>LAYOUT :</div> <div>Fairbanks , AK</div>	<div>This design is and shall at all times remain the exclusive property of ALVIN REINHARD FRITZ ARCHITECT INC. and shall not be reproduced or used without the architect's written permission.</div>	
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	<div>DRAWING TITLE:</div> <div>Construction Details</div> <div>- Roof</div>	
	<div>DRAWN BY :</div> <div>TO</div>	<div>CHECKED BY :</div> <div>DK</div>
	<div>SCALE: 1 1/2" = 1'-0"</div> <div>PROJECT : 22636 NASHD</div> <div>ISSUE FOR:</div> <div>Issue for Review</div>	
<div>ISSUE DATE:</div> <div>2022 06 06</div>		
<div>REV. NO.</div> <div>SHEET NO.</div> <div>A7.5</div>		

LEGAL DESCRIPTION

CLIENT INFORMATION

CONSULTANT LOGO:

NO.	BY	DESCRIPTION	DATE
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ph. (403) 320-8100
Fax (403) 327-3373
general@alvinfritzarchitect.com

PROFESSIONAL SEAL:

PROJECT: **All Steel 24 x 24**

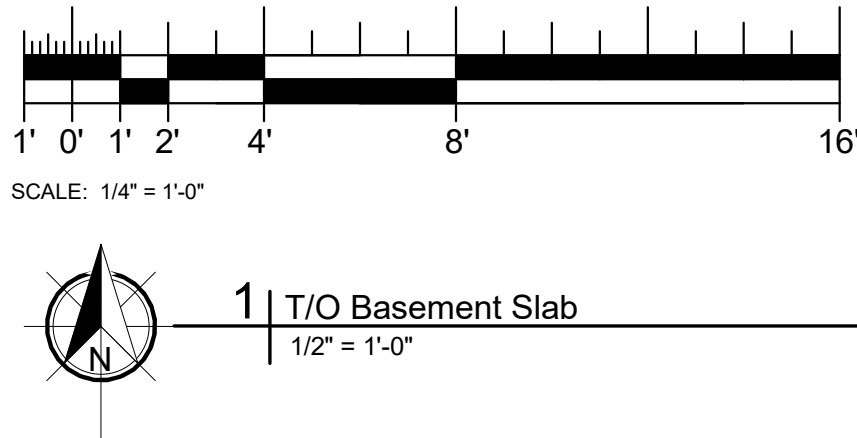
Fairbanks , AK

Do not scale this drawing. All dimensions, data and levels, shall be verified prior to construction and all errors or omissions shall be reported to the architect immediately.

DRAWING TITLE:

**Construction Details
Finishes**

DRAWN BY : TO	CHECKED BY : DK
SCALE : 3" = 1'-0"	
PROJECT : 22236 NASHD	
ISSUE FOR: Issue for Review	
ISSUE DATE: 2022 06 06	
REV. NO.	SHEET NO. A7.6



KEYPLAN:

LEGAL DESCRIPTION:

CLIENT INFORMATION:

#DescriptionDate

Revision Schedule

CONSULTANT LOGO:

</

1. IF ROOF SYSTEM IS TO BE CONSTRUCTED ON THE GROUND AND PLACED WITH THE USE OF A BOOM THE ROOF FRAMING IS TO BE CONSTRUCTED IN PIECES OR SECTIONS
SPECIAL STEPS WILL ALSO BE NEEDED IN THE SHEATHING AND TRIMMING OF THE ROOF
2. LAST HAT CHANNELS IN SEQUENCE ON EITHER SIDE/ SLOPE OF TRUSS SHOULD BE BACK A MINIMUM OF 1" FROM PEAK

LEGAL DESCRIPTION:

CLIENT INFORMATION:

CONSULTANT LOGO:[illegible]

NO.	BY	DESCRIPTION	DATE
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ph. (403) 320-8100
Fax (403) 327-3373
general@alvinfritzarchitect.com

PROFESSIONAL SEAL:

Fairbanks, AK

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DRAWING TITLE:

Roof Plan - Framing

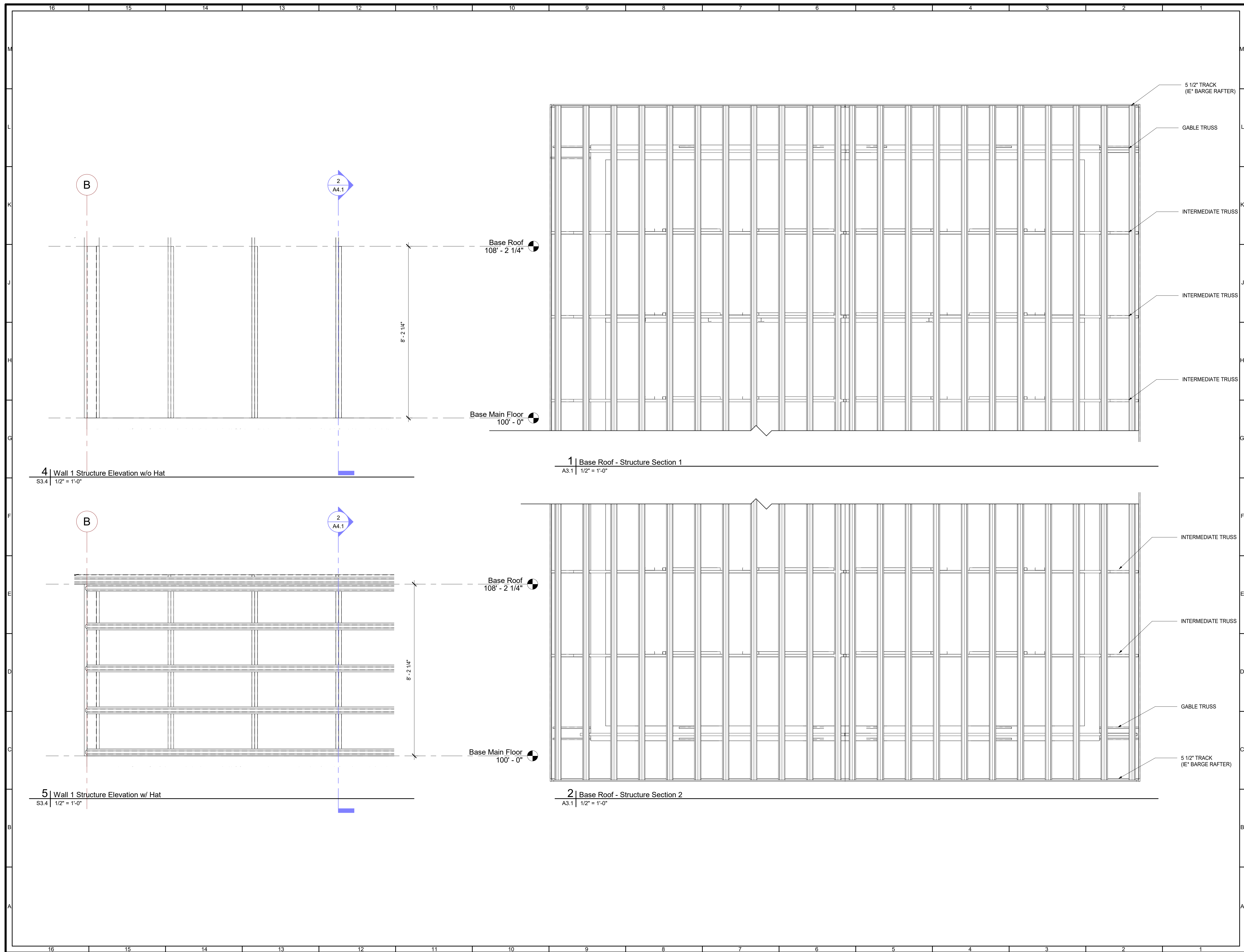
DRAWN BY : JO	CHECKED BY : DK
SCALE : As indicated	
PROJECT : 22236 NASHD	
ISSUE FOR: Issue for Review	
ISSUE DATE: 2022 06 06	
REV. NO.	SHEET NO. 325

S2.5

<div>PROJECT :</div> <div>ALL Steel 24 x 24</div> <div>LOCATION :</div> <div>Fairbanks , AK</div>	<div>This design is and shall at all times remain the exclusive property of ALVIN REINHARD FRITZ ARCHITECT INC. and shall not be reproduced or used without the architect's written permission.</div>
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	<div>DRAWING TITLE:</div> <div>Elevations - Framing</div>
	<div>DRAWN BY :</div> <div>TO</div>
	<div>CHECKED BY :</div> <div>DK</div>
	<div>SCALE: 1/2" = 1'-0"</div> <div>PROJECT: 2422 NASHD</div> <div>ISSUE FOR:</div> <div>Issue for Review</div> <div>ISSUE DATE: 2022 06 06</div> <div>REV. NO. SHEET NO.</div> <div>S3.3</div>

REV. NO.	SHEET NO.
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S3.4




LEGAL DESCRIPTION:

CONSULTANT LOGO:

#	Description	Date
Revision Schedule		

NO.	BY	DESCRIPTION	DATE
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Norland Coach House
#10 90001 Range Road 212,
Lethbridge County, Alberta
T1J 5N9
alvinfritzarchitect.com

ph. (403) 320-8100
Fax (403) 327-3373
general@alvinfritzarchitect.com

ALVIN REINHARD FRITZ
ARCHITECT
INC.

PROFESSIONAL SEAL:

Fairbanks , AK

DRAWING TITLE:

Elevations - Framing - Close up

DRAWN BY : TO	CHECKED BY : DK
SCALE : 1/2" = 1'-0"	
PROJECT : 22236 NASHD	
ISSUE FOR: Issue for Review	
ISSUE DATE: 2022 06 06	
REV. NO.	SHEET NO. S3.5

PUBLIC HEARING

QUASI-JUDICIAL

Resolution No. 23-33

D&S Trail Rides

(Pages 57 - 168)

PUBLIC HEARING



MATANUSKA-SUSITNA BOROUGH

Planning and Land Use Department

Development Services Division

350 East Dahlia Avenue • Palmer, AK 99645

Phone (907) 861-7822

www.matsugov.us

DEVELOPMENT SERVICES DIVISION STAFF REPORT


Date: August 9, 2023

File Number: 10244

Applicant/Owner: Steve Van Troba, dba D & S Alaskan Trail Rides

Request: Planning Commission Resolution 23-33
Conditional Use Permit to operate a commercial use (horse and wagon rides) in accordance with MSB Chapter 17.17 – Denali State Park Special Land Use District

Location: 10578 E. Walter Harper Way, Tax ID#s 29N05W33D012 & 29N05W33D028; within Township 29 North, Range 5 West, Section 33, Seward Meridian

Reviewed By: Jason Ortiz, Development Services Manager 

Staff: Peggy Horton, Planner II

Staff Recommendation: Approval with conditions

EXECUTIVE SUMMARY

Mr. Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District to operate a commercial use (horse & wagon rides).

MSB 17.17 – Denali State Park Special Land Use District requires a conditional use permit for commercial uses within the district. The area within the boundaries of this special land use district is zoned for recreational uses. Commercial uses are prohibited from operating within the district without a lawfully issued Conditional Use Permit.

HISTORY

The subject properties were originally a small portion of native allotment land in 1985, which at the time totaled approximately 140 acres. In 1993, the portions of the native allotment were sold to private individuals and no longer had native allotment status. In 1997, private use easements were recorded to construct access roads for the privately owned parcels. Also, in 1997, the applicant began leasing the subject parcels D012 & D028 for his commercial operation. In 2019, the applicant purchased said parcels.

In May 1997, the applicant obtained a Conditional Use Permit (#C97-02) to operate this commercial use on two different parcels (Tax ID # 29N05W33D006 & 29N05W33D021). The Conditional Use Permit was never exercised on Parcels D006 or D021; however, commercial use began operating on the subject parcels (D012 & D028).

In 2015, the Borough received a complaint related to D & S Alaskan Trail Rides. The complaint was related to blocking the road, building new unpermitted trails, trespassing, and operating the business within the Special Land Use District without a Conditional Use Permit. In August 2018, the assigned Code Compliance Officer found a discrepancy between the location of the business and the location approved by the 1997 Conditional Use Permit.

In January 2019, Mr. Van Troba submitted an incomplete application for Conditional Use Permit to operate a commercial use within the Denali Special Land Use District. Despite several weeks of staff reiterating that vital information was missing from the application, Mr. Van Troba insisted on a hearing. At the March 21, 2022 meeting, the Planning Commission unanimously denied the permit primarily for insufficient information within the application.

On February 8, 2023, Mr. Van Troba submitted a new application for the Denali State Park Special Land Use District Conditional Use Permit. Working with staff, he willingly submitted all the information required. On June 14, 2023, staff sent Mr. Van Troba an email telling him we had a complete application and we had scheduled a public hearing.

LAND USE

Existing Land Use:

The subject parcels are both five acres in size. The commercial use is centrally located within the two parcels. Mr. Van Troba stated that a portion of the horse stables' south end would be removed to bring the structure into compliance with the setback requirements in MSB 17.55 prior to the public hearing.

Surrounding Land Uses:

Parcels to the north, east, and west are five acres in size. Some parcels are undeveloped, while others have been developed with recreational/residential structures. Further west, the McKinley Princess Lodge has developed on approximately 150 acres. Approximately 1,500 feet to the east, a telecommunication tower has been constructed. Both the lodge and tower have obtained a Conditional Use Permit. The parcel to the south is approximately 2,158 acres in size, developed for recreational use by the Boy Scouts of America.

REVIEW OF APPLICABLE CRITERIA AND FINDINGS

MSB 17.03 – Public Notification

Borough staff mailed 14 notices on July 13, 2023, to all property owners within 600 feet of the subject parcels and the State of Alaska Department of Natural Resources, Division of Parks and Outdoor Recreation. The Frontiersman published the public hearing notice in the June 30, 2023 issue. Staff posted the application material on the Borough's website and emailed the public notice, application material, and a request for comments to outside agencies on June 23, 2023. The subject parcels are not within a community council. Staff did not receive any comments from the public.

MSB 17.17.060 CONDITIONAL USES.

(A) *The following uses may be permitted by obtaining a conditional use permit issued in accordance with MSB 17.17.140 through 17.17.180:*

- (1) *two-family dwellings;*
- (2) *multiple-family dwellings with three or more units;*
- (3) *group homes;*
- (4) *churches and related buildings;*
- (5) *commercial uses;*
- (6) *private campgrounds;*
- (7) *recreational vehicle parks;*
- (8) *utility substations;*
- (9) *highway maintenance yards;*
- (10) *public gravel pits;*
- (11) *group camps.*

Findings of Fact:

1. According to borough records, D & S Alaskan Trail Rides first received a conditional use permit in the Denali State Park Special Land Use District in 1997, offering commercial horse and wagon rides to the public.
2. According to borough records, D & S Alaskan Trail Rides 1997 Conditional Use Permit was approved for properties separate from the subject properties.
3. The subject property consists of two five-acre parcels.
4. The subject property includes those parcels currently identified as Tax ID #29N05W33D012 and #29N05W33D028, known as D012 and D028.
5. According to the application material, the operation transports guests from Mt. McKinley Princess Lodge to the horseback riding trailhead on the subject parcels.
6. The proposed use will access East Walter Harper Way, a private road.
7. The surrounding area has a mixture of recreational, residential, and commercial uses.

Conclusion of Law: Based on the above findings, the commercial use requires a Conditional Use Permit (MSB 17.17.060(A)(5)).

MSB 17.17.090 BUILDING HEIGHT LIMITS.

The maximum building height shall be 35 feet above finished grade.

Finding of Fact:

1. The application material indicates that the maximum height of any structure on the property is approximately 25 feet above finished grade.

Conclusion of Law: Based on the above finding, the commercial use is developed in accordance with the building height requirements (MSB 17.17.090).

MSB 17.17.110 SETBACK REQUIREMENTS

Except for signs, no structure shall be erected within 75 feet of the right-of-way of the George Parks Highway. In all other cases, the setback requirements specified in MSB 17.55 shall apply.

MSB 17.55.010 SETBACKS

(A) No structure or building line shall be placed within 25 feet from the right-of-way line of any public right-of-way, except no furthestmost protruding portion of any structure shall be placed within ten feet from the right-of-way line of any public right-of-way when the pre-existing lot:

(1) measures 60 feet or less in frontage on a public right-of-way, and is not located on a cul-de-sac bulb; or

(2) comprises a nonconforming structure erected prior to July 3, 1973. This setback shall be known as the structure or building line setback.

(B) Except where specifically provided other-wise by ordinance, no furthestmost protruding portion of any structure or building line shall be located nearer than ten feet from any side or rear lot line.

MSB 17.55.020 SETBACKS FOR SHORELANDS

(A) Except as provided in subsection (B) of this section, no structure or footing shall be located closer than 75 feet from the ordinary high water mark of a body of water. Except as provided otherwise, eaves may project three feet into the required setback area.

Findings of Fact:

1. The subject property is more than 3,700 feet from the Parks Highway right-of-way.
2. The subject property includes those parcels currently identified as Tax ID #29N05W33D012 and #29N05W33D028, known as D012 and D028.
3. According to the application material, the horse stables are located 123.6' from the western property line, approximately 100' from the creek shown on the site plan, approximately 550' from the northern property line, approximately 150' feet from the east property line and approximately 0.4' from the common property line with parcel D012.
4. According to the application material, the structures on parcel D028, other than the horse stables, meet the setback requirements of MSB 17.55.
5. According to the application material, the single-story wood frame house and two sheds lie within parcel D012 and meet all setback requirements of MSB 17.55.

6. The closest public right-of-way is a 50' wide section line easement adjoining the south property line of parcel D012 and approximately 650' to the south of the single-story wood frame house.

Discussion: The building labeled horse stables on the as-built is currently within four inches of the common lot line between D012 & D028. Mr. Van Troba agreed to staff's recommendation that a portion of the building be removed to bring the structure into compliance with the setbacks.

Conclusion of Law: Based on the above findings, and with conditions, the application demonstrates the commercial use is developed in accordance with the setback requirements (MSB 17.17.110).

MSB 17.17.130 SIGNS

The following types of signs are prohibited: portable signs; signs mounted on top of buildings; and flashing, rotating, animated or intermittent lighted signs. In no case shall any sign exceed 32 square feet in area. Signs within 660 feet of the right-of-way of the Parks Highway are regulated by the Code of Federal Regulations.

Finding of Fact:

1. According to the application material, no signs exist for the facility.

Conclusion of Law: Based on the above finding, the application material demonstrates the commercial use is developed in accordance with the signage requirements. (MSB 17.17.130).

MSB 17.17.180 GENERAL STANDARDS

(A) A conditional use may be approved only if it meets the requirements of this section in addition to any other standards required by this chapter.

(1) the conditional use will not detract from the value, character or integrity of Denali State Park;

Findings of Fact:

1. According to the application material, the operation is seasonal, May through September.
2. According to the application material, approximately three visitor trips are taken each day.
3. According to the application material, the operation will be open approximately eight hours a day, six days a week.
4. The surrounding area has a mixture of recreational, residential, and commercial uses.
5. According to the application material, the horse stables are located 123.6' from the western property line, approximately 100' from the creek shown on the site plan, approximately 550' from the northern property line, approximately 150' feet from the east property line and approximately 0.4' from the common property line with parcel D012.
6. According to the application material, a single-story wood frame house and two sheds lie within parcel D012 and meet all setback requirements of MSB 17.55.

7. According to the application material, the structures on parcel D028, other than the horse stables, meet the setback requirements of MSB 17.55.
8. Site visit photos taken by staff on August 5, 2023, confirm that Mr. Van Troba has begun dismantling that portion of the horse stables' south end that lies within the setback.
9. According to the application material, the operation transports guests from Mt. McKinley Princess Lodge to the horseback riding trailhead on the subject parcels.
10. According to the application material, Diane L. Murray, the owner of five neighboring lands to the west, granted permission for D & S Alaskan Trail Rides to use her property for the commercial use.
11. Within the application material, the D & S Trail Rides map indicates the trails used during the guided horse and wagon rides.
12. According to the application material, the horse and wagon rides will remain on trails where permission has been granted to occur.
13. The subject parcels are more than 3,700 feet from the Parks Highway right-of-way.

Conclusion of Law: Based on the above findings, and with conditions, the application material demonstrates the commercial use does not detract from the value, character, or integrity of Denali State Park (MSB 17.17.180(A)(1)).

(2) that the conditional use fulfills all other requirements of this chapter pertaining to the conditional use in question;

Finding of Fact:

1. The applicant provided information fulfilling the requirements of this chapter pertaining to the conditional use permit.

Conclusion of Law: Based on the above finding and with conditions, the application material meets the requirements of this chapter pertaining to the conditional use permit for a commercial use (MSB 17.17.180(A)(2)).

(3) that granting the conditional use permit will not be harmful to the public health, safety, convenience and welfare;

Findings of Fact:

1. The surrounding area has a mixture of recreational, residential, and commercial uses.
2. The closest residential structure to the use is approximately 500 feet southwest of the single-story wood frame house on parcel D012.
3. The applicant is not proposing any outdoor amplified sound activities.
4. According to the application material, the operation transports guests from Mt. McKinley Princess Lodge to the horseback riding trailhead on the subject parcels.

5. According to the application material, Diane L. Murray, the owner of five neighboring lands to the west, granted permission for D & S Alaskan Trail Rides to use her property for the commercial use.
6. Within the application material, the D & S Trail Rides map indicates the trails used during the guided horse and wagon rides.
7. According to the application material, the guided horse and wagon rides will remain on trails where permission has been granted to occur.

Conclusion of Law: Based on the above findings, the proposed use will not be harmful to the public health, safety, convenience, and welfare (MSB 17.17.180(A)(3)).

(4) that sufficient access, setbacks, lot area, parking space, buffers, and other safeguards and being provided to meet the conditions; and

Findings of Fact:

1. The subject property consists of two five-acre parcels.
2. The surrounding area has a mixture of recreational, residential, and commercial uses.
3. The proposed use will access East Walter Harper Way, a private road.
4. Within the application material, the D & S Trail Rides map indicates the trails used during the guided horse and wagon rides.
5. According to the application material, Diane L. Murray, the owner of five neighboring lands to the west, granted permission for D & S Alaskan Trail Rides to use her property for the commercial use.
6. According to the application material, the horse stables are located 123.6' from the western property line, approximately 100' from the creek shown on the site plan, approximately 550' from the northern property line, approximately 150' feet from the east property line and approximately 0.4' from the common property line with parcel D012.
7. According to the application material, the single-story wood frame house and two sheds lie within parcel D012 and meet all setback requirements of MSB 17.55.
8. According to the application material, the structures on parcel D028, other than the horse stables, meet the setback requirements of MSB 17.55.
9. The closest residential structure to the use is approximately 500 feet southwest of the single-story wood frame house on parcel D012.
10. The commercial site has parking area for their two commercial vans and employees.
11. According to the application material, sufficient parking is provided at Mt. McKinley Princess Lodge.
12. According to the application material, the operation transports guests from Mt. McKinley Princess Lodge to the horseback riding trailhead on the subject parcels.
13. According to the application material, the applicant intends to keep the natural vegetation on the property, acting as a buffer.

Discussion: The building labeled horse stables on the as-built is currently within four inches of the common lot line between D012 & D028. Mr. Van Troba agreed to staff's recommendation that a portion of the building be removed to bring the structure into compliance with the setbacks.

Conclusion of Law: Based on the above findings, the commercial use provides sufficient access, setbacks, lot area, parking space, buffers, and other safeguards to meet the conditions (MSB 17.17.180(A)(4)).

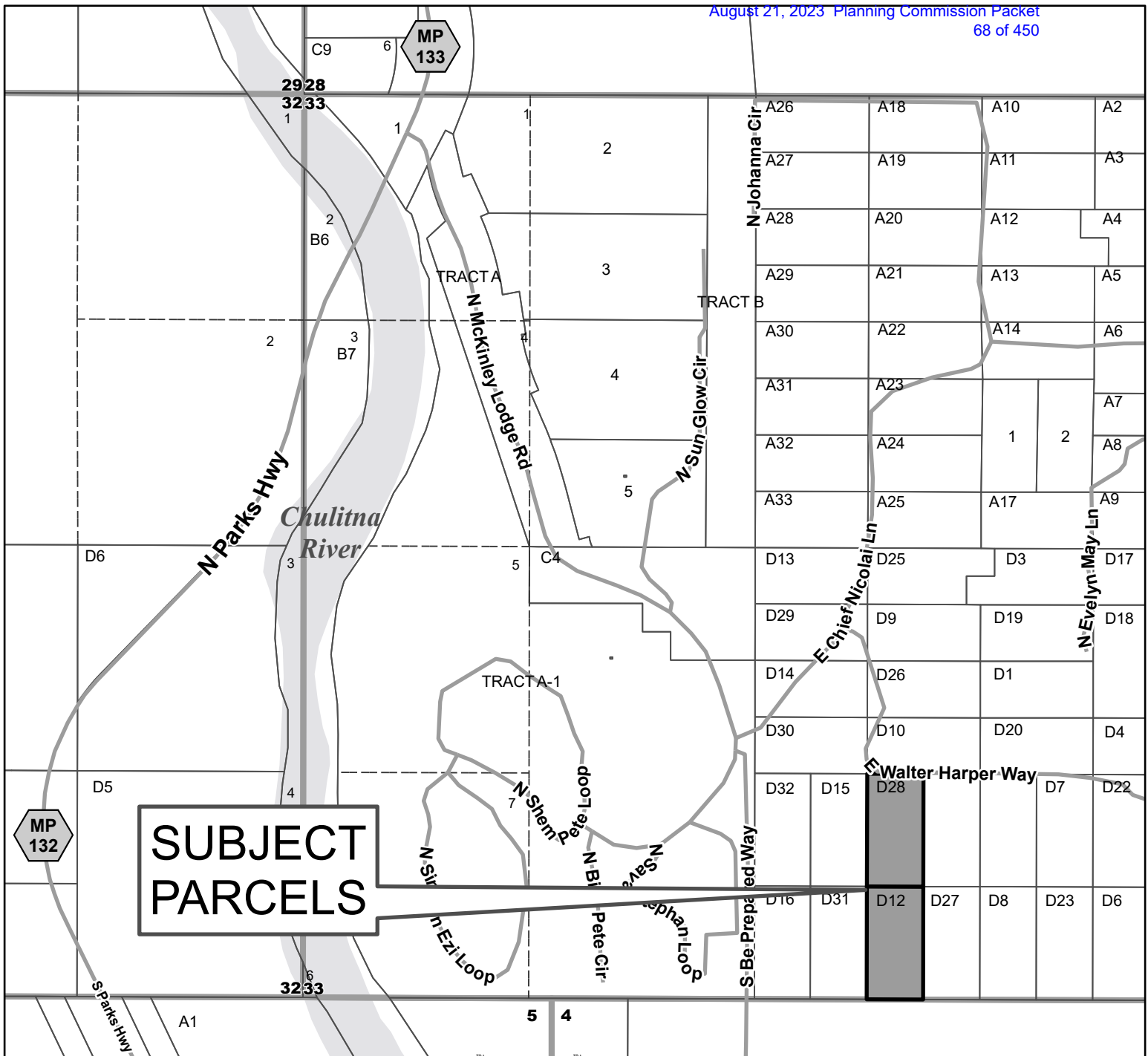
STAFF RECOMMENDATIONS

Staff recommends approval of the Denali State Park Special Land Use District Conditional Use Permit to operate a commercial use (horse and wagon rides) at 10578 E. Walter Harper Way, Tax ID#s 29N05W33D012 & 29N05W33D028. The proposed use meets all applicable standards of MSB 17.17. Staff also recommends the following conditions:

1. The operation shall comply with all other applicable federal, state, and local regulations.
2. All aspects of the operation shall comply with the description detailed in the application material and with the conditions of this permit. An amendment to the Conditional Use Permit shall be required prior to any expansion of the conditional use.
3. The operation shall comply with the maximum permissible sound level limits allowed, per MSB 17.61.080 – Noise Standards requirements and MSB 8.52 – Noise, Amplified Sound, and Vibration.
4. Remove a portion of the horse stable's south end to bring the building into compliance with MSB 17.55. Provide staff with photographic evidence of the removal.

If the Planning Commission chooses to deny this permit, findings for denial must be prepared by the Commission.

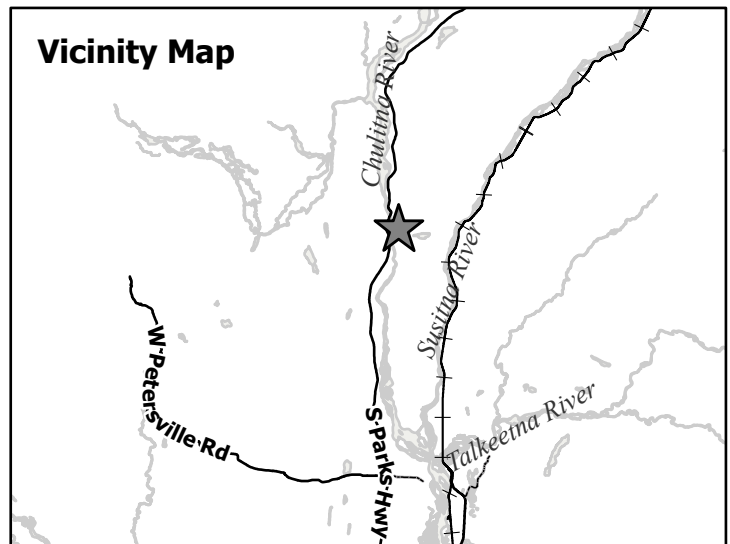
MAPS



29N05W33D012
29N05W33D028



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E Walter Harper Way



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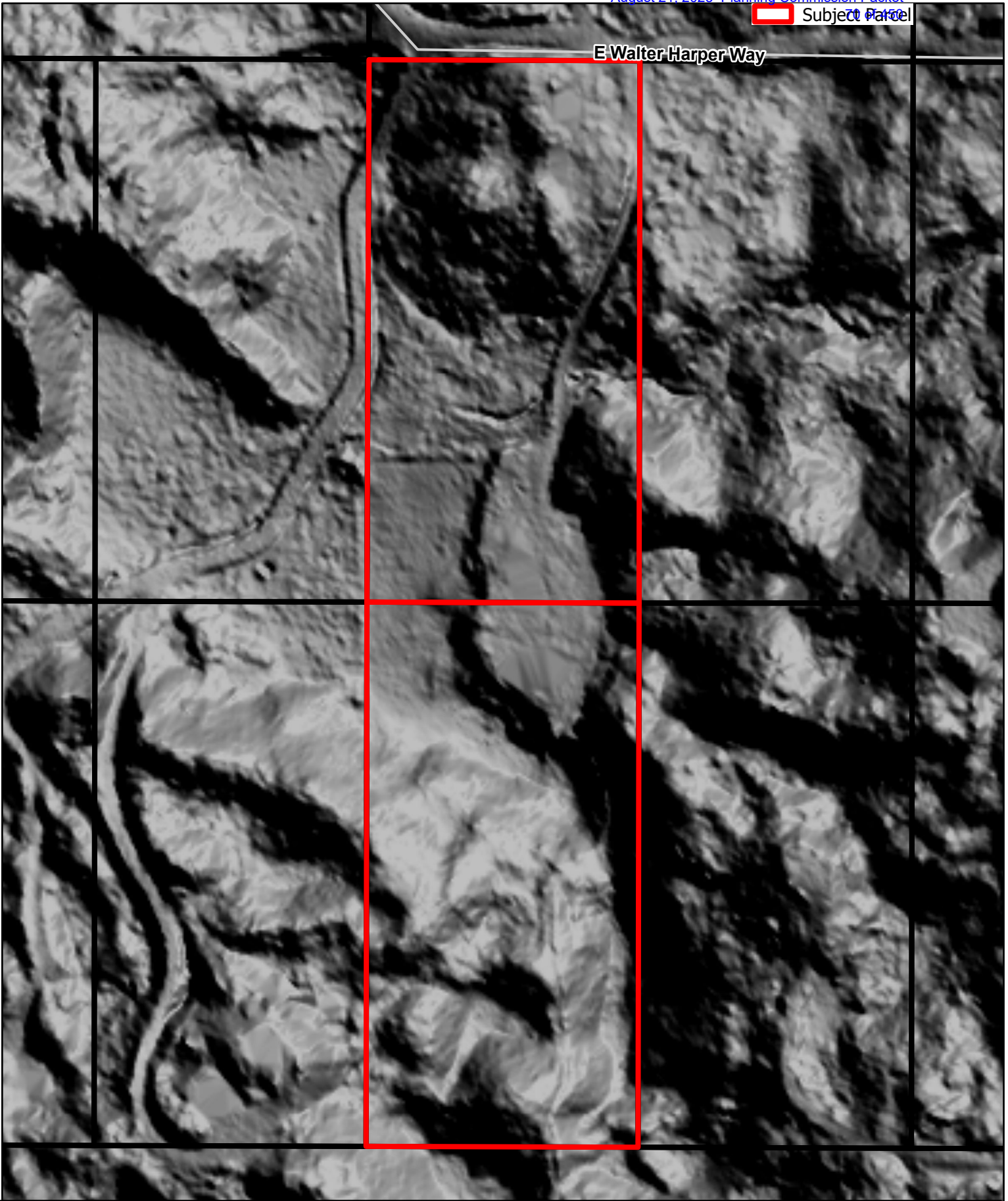


2011 Hillshade map of 29N05W33D012 & 29N05W33D028

0 100 200
Feet



E Walter Harper Way



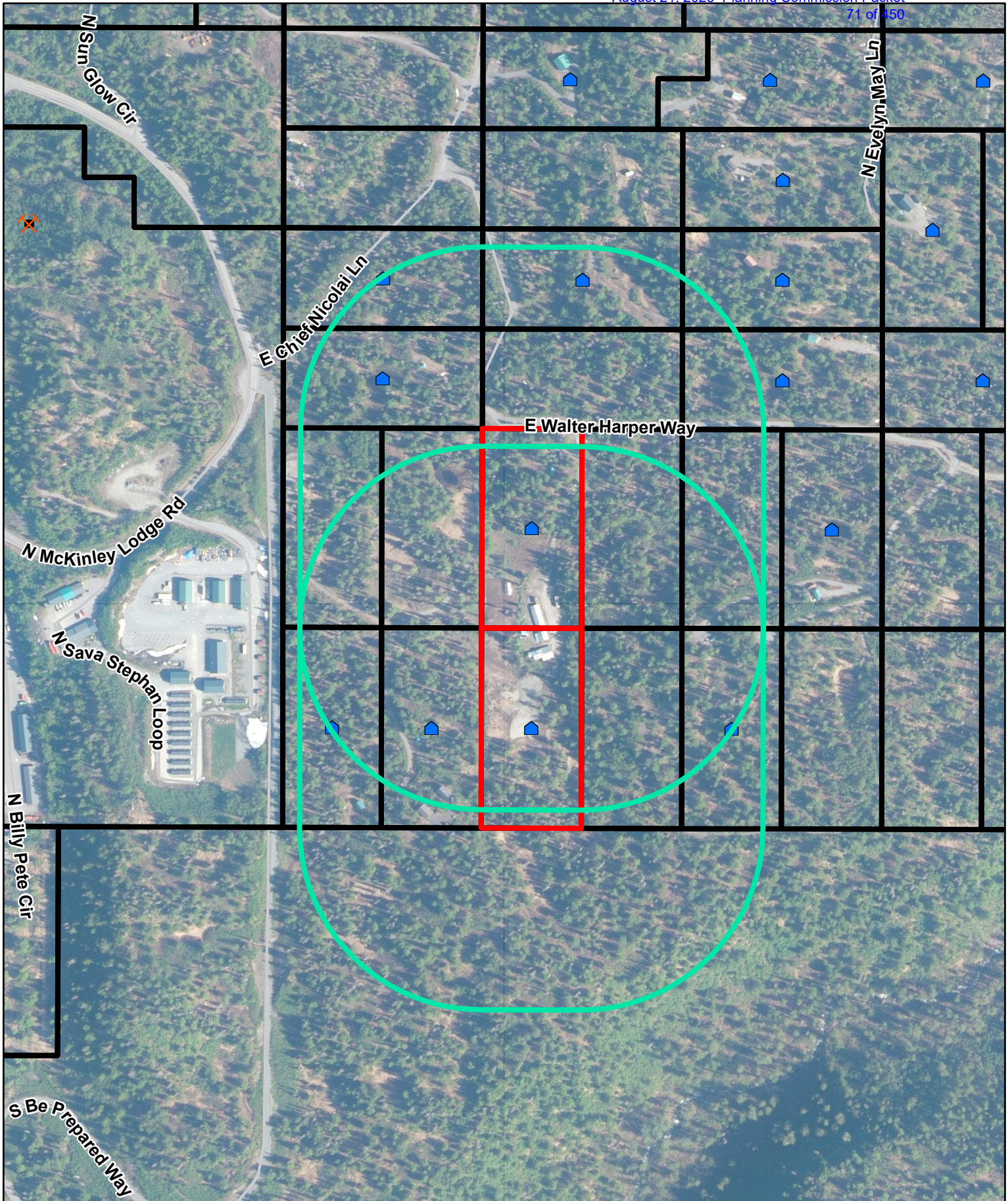
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2011 Hillshade map of 29N05W33D012 & 29N05W33D028

0 100 200
Feet





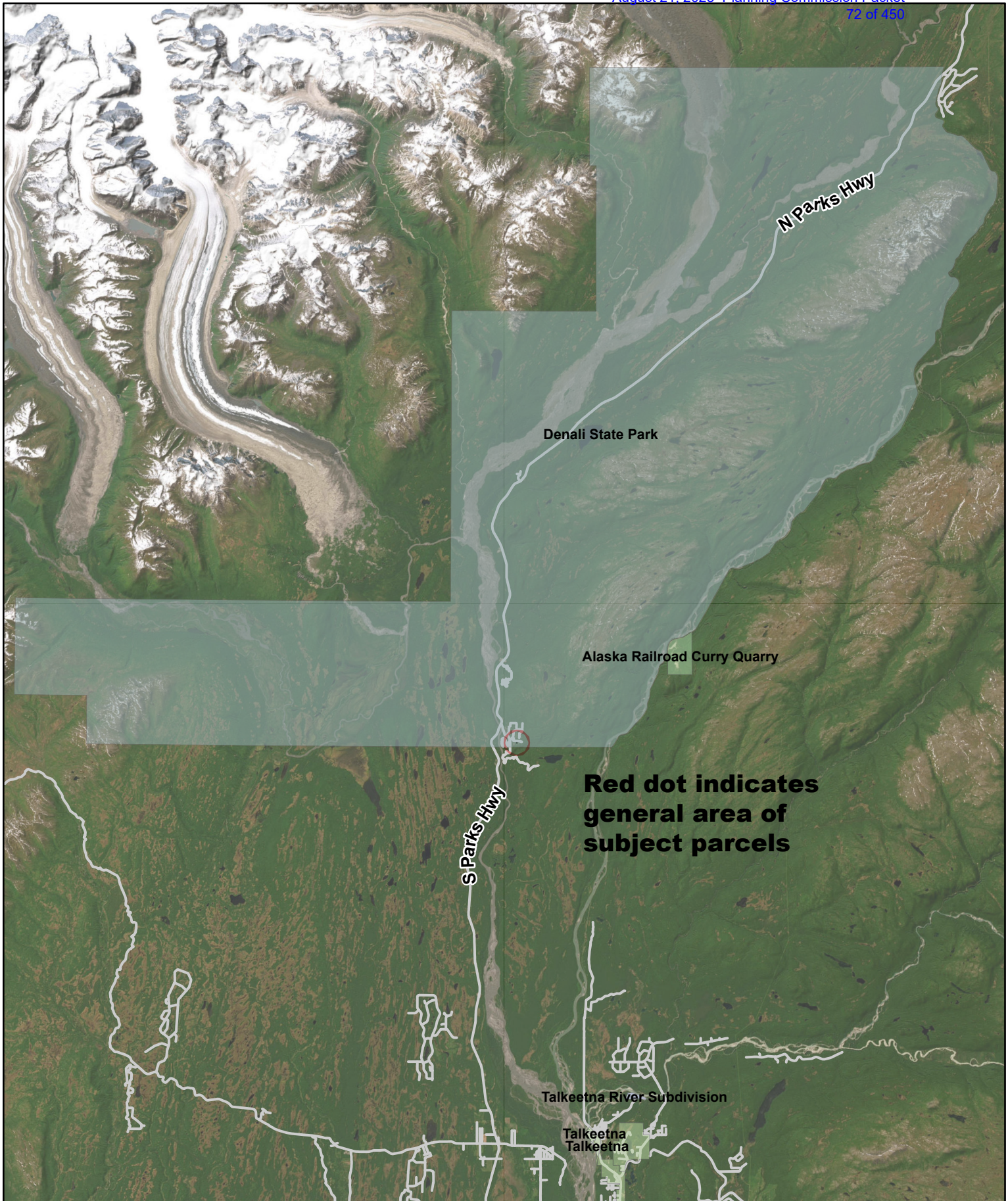
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Land Use Map with 600 foot buffer

0 330 660
Feet





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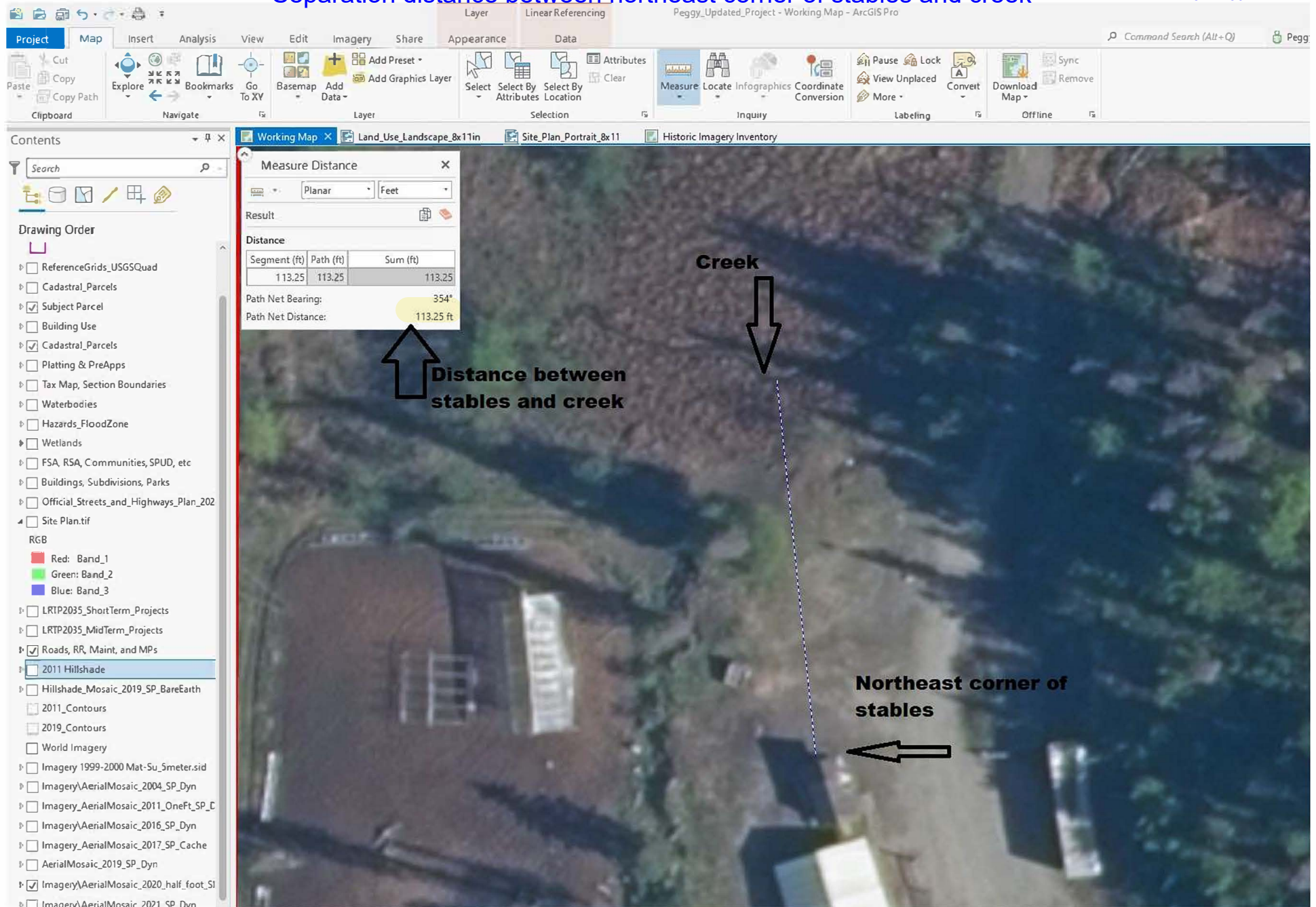
Denali State Park Boundary Map

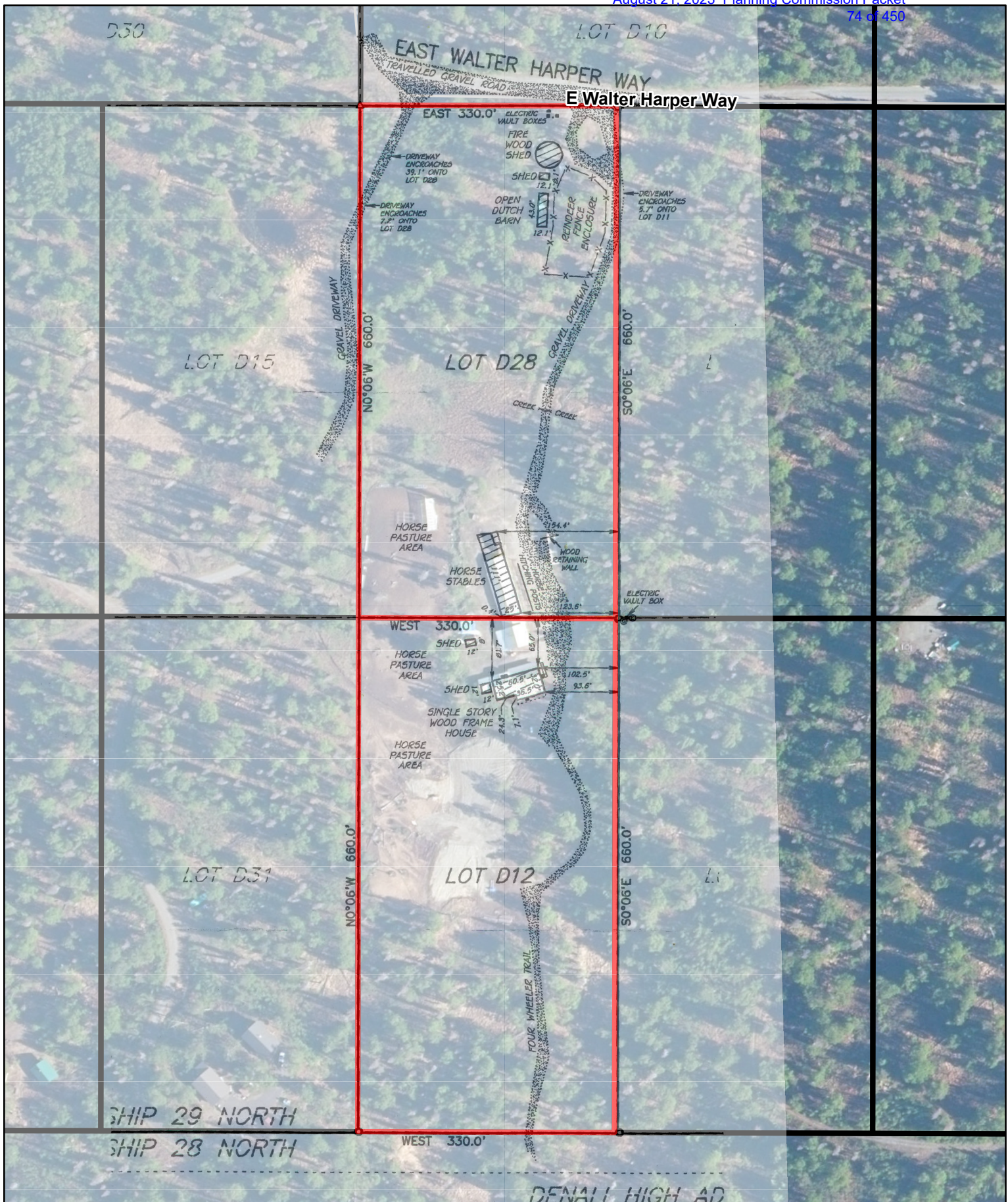


Separation distance between northeast corner of stables and creek

August 21, 2023 Planning Commission Packet

73 of 450





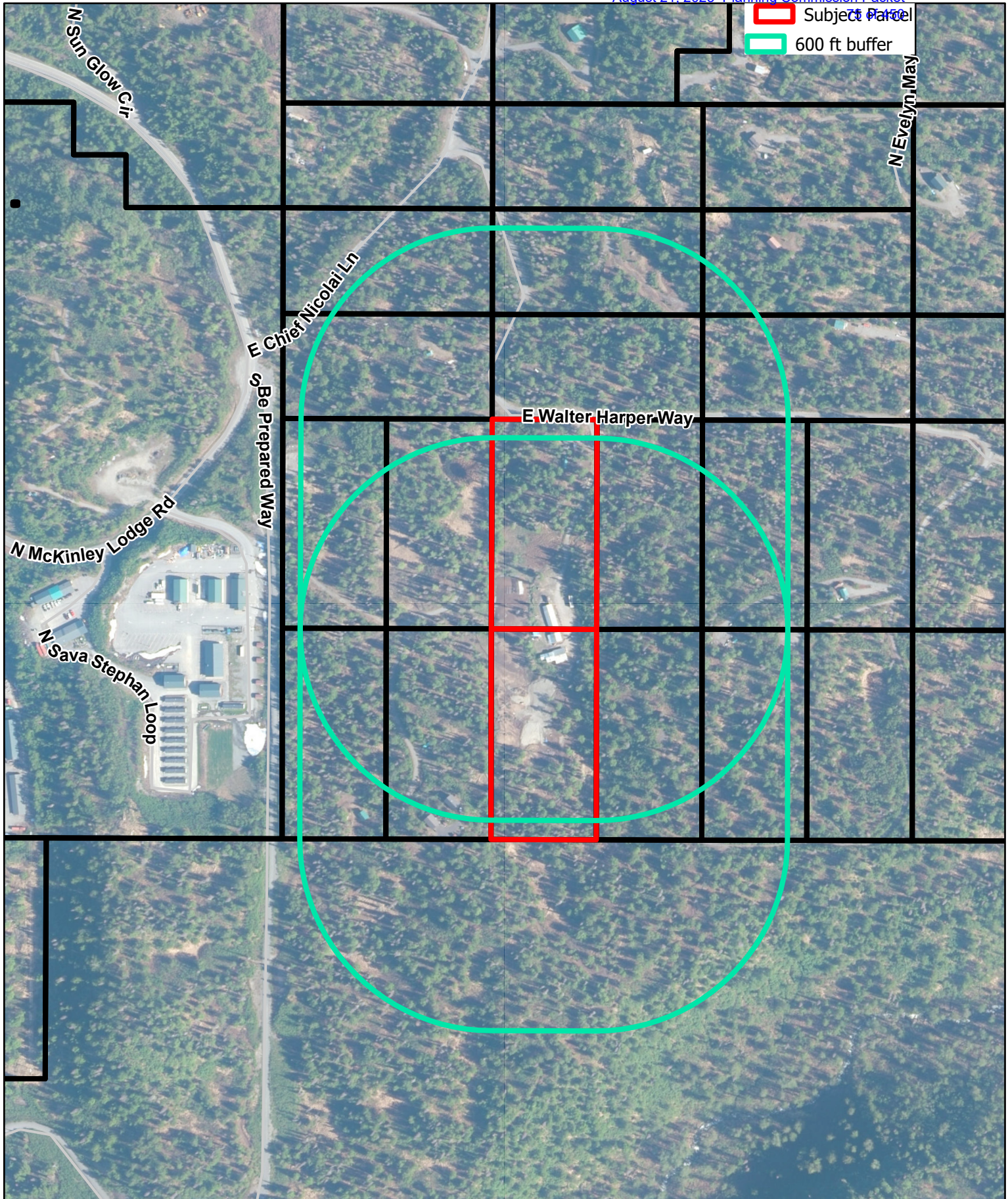
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Site Plan Overlay 29N05W33D012 & 29N05W33D028

0 125 250 Feet





Subject Parcel
 600 ft buffer

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600 foot buffer
29N05W33D012 &
29N05W33D028

0 300 600
 Feet



PUBLIC NOTICING

From: [Peggy Horton](#)
To: [Corinne Lindfors \(Corinne.Lindfors@matsugov.us\)](#); [Karol Riese](#)
Subject: Mailing: D&S Alaskan Trail Rides CUP
Date: Friday, June 23, 2023 4:25:00 PM
Attachments: [29N05W33D012_D028 Main Layout \(002\).pdf](#)
[Public Notice Mailing.DOCX](#)

Good Morning,

Please prepare and send the attached mailing on or before Friday, July 14, 2023. The notification area is 600 feet. This property is outside of any community council area. Please send the notice to SOA DNR at the following address:

State of Alaska
Department of Natural Resources
Division of Parks and Outdoor Recreation
550 W 7th Ave, Suite 1380
Anchorage AK 99501

Thank you,
Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862



Certificate of Bulk Mailing – Domestic

Fee for Certificate

Up to 1,000 pieces (1 certificate for total number)

For each additional 1,000 pieces, or fraction thereof

Duplicate Copy ☐

Use
Current
Price List
(Notice 123)

Postage: Mailers must affix meter, PC Postage®, or (uncanceled) postage stamps here in payment of total fee due.



US POSTAGE PAID BY PITNEY BOWES
ZIP 99645 **\$ 010.90⁰**
02 4W
0000368428 JUL 13 2023

Acceptance employee must cancel postage affixed (by round-date) at the time of mailing.

If payment of total fee due is being paid by Permit Imprint, include the

PostalOne!® Transaction Number here: _____

Number of Identical Weight Pieces 14	Class of Mail 1ST	Postage for Each Mailpiece Paid <input type="checkbox"/> Verified	Number of Pieces to the Pound 45
Total Number of Pounds 4.902	Total Postage Paid for Mailpieces 8.82	Fee Paid 10.90	

Mailed For

Dev Serv. Ginger

Mailed By

Postmaster's Certification

It is hereby certified that the number of mailpieces presented and the associated postage and fee were verified. This certificate does not provide evidence that a piece was mailed to a particular address.

(Postmaster or Designee)



	Tax Account	OWNER_1	NAME_2	MAILING_ADDRESS_LINE_A	MAILING_ADDRESS_LINE_B	MAILING_ADDRESS_CITY	MAILING_ADDRESS_STATE	MAILING_ADDRESS_ZIP
1	29N05W33D015	MOLYNEUX VAL FAMILY TRUST	MOLYNEUX SANDRA TRE	UNIT 207	1110 W 6TH AVE	ANCHORAGE	AK	99501
2	29N05W33D026	JGR TRUST			681 W LAZY K LN	WASILLA	AK	99654
3	29N05W33D012	D&S ALASKAN TRAIL RIDES INC			PO BOX 1437	PALMER	AK	99645
4	29N05W33D001	RASMUSSEN FAMILY TRUST			HC 89 BOX 8121	TALKEETNA	AK	99676
5	6946000T00B	GREAT ALASKA COUNCIL	BOY SCOUTS OF AMERICA		3117 PATTERSON ST	ANCHORAGE	AK	99504
6	29N05W33D014	TURNER E LYNN			PO BOX 13125	TRAPPER CREEK	AK	99683-0125
7	29N05W33D031	UPCHURCH PARA D	PEYRALANS JACQUES RENIOR		PO BOX 13225	TRAPPER CREEK	AK	99683
8	29N05W33D010	GEERTSEN THOS M& MICHELLE		# E	4303 NORTHWOOD	ANCHORAGE	AK	99517
9	29N05W33D027	MURRAY DIANE L			3707 RUNESTAD CIR	ANCHORAGE	AK	99502
10	29N05W33D020	HOOSER DALE & SUSIE			3330 DOVE LN	PALMER	AK	99645
11	29N05W33D011	SCHIKORA FREDERICK J			528 EAGLE RIDGE RD	FAIRBANKS	AK	99712
12	29N05W33D016	WILLIAMS RANDALL S			2020 REVERE CIR	ANCHORAGE	AK	99515
13	29N05W33D032	UPCHURCH PARA			PO BOX 13225	TRAPPER CREEK	AK	99683-3225
14	29N05W33D030	BUCHANAN STEVEN J & JENNIFER L			2435 HILAND RD	EAGLE RIVER	AK	99577-9400

Instructions



Matanuska-Susitna Borough
Development Services Division
350 E. Dahlia Avenue
Palmer, Alaska 99645

«NAME»
«ADDRESS_1»
«ADDRESS_2»
«ADDRESS_3»

The Matanuska-Susitna Borough Planning Commission will consider the following:

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

The Matanuska-Susitna Borough Planning Commission will conduct a public hearing concerning the application on **Monday, August 21, 2023, at 6:00 p.m.** in the Borough Assembly Chambers at 350 E. Dahlia Avenue in Palmer. This may be the only presentation of this item before the Planning Commission, and you are invited to attend. Planning Commission members may not receive or engage in ex-parte contact with the applicant, other interested parties in the application, or members of the public concerning the application or issues presented in the application.

Application materials may be viewed online at www.matsugov.us by clicking “All Public Notices & Announcements.” For additional information, contact Peggy Horton, Planner II, at 907-861-7862. Provide written comments by e-mail to peggy.horton@matsugov.us or by mail to MSB Development Services Division, 350 E. Dahlia Avenue, Palmer, AK 99645.

The public may provide verbal testimony at the meeting or telephonically by calling 1-855-290-3803. To be eligible to file an appeal from a decision of the Planning Commission, a person must be designated an interested party. See MSB 15.39.010 for the definition of an interested party. The procedures governing appeals to the Board of Adjustment and Appeals are contained in MSB 15.39.010-250, which is available on the Borough home page: www.matsugov.us, in the Borough Clerk’s office, and at various libraries within the borough.

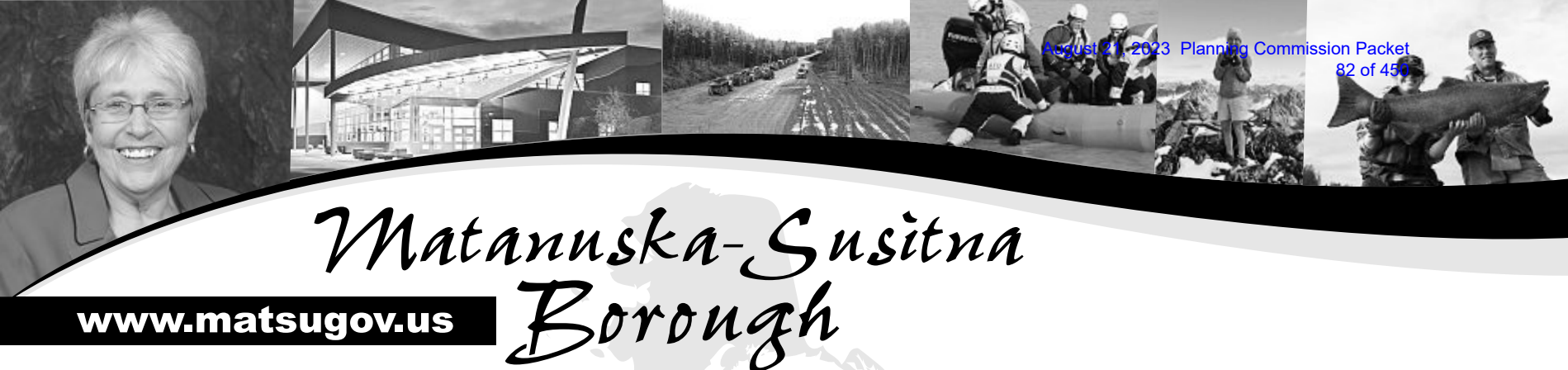
Comments are due on or before **August 4, 2023**, and will be included in the Planning Commission packet. Please be advised that comments received from the public after that date will not be included in the staff report but will be provided to the Commission at the meeting.

Name: _____ **Mailing Address:** _____

Location/Legal Description of your property: _____

Comments: _____

Note: Vicinity Map is Located on Reverse Side



www.matsugov.us

Matanuska-Susitna Borough

MATANUSKA-SUSITNA BOROUGH NOTICE OF PUBLIC MEETINGS

All meetings of recognized boards, committees, and commissions of the Borough are open to the public and are held at Borough offices, 350 E. Dahlia Ave., Palmer, AK, unless specified otherwise. Three or more Assembly Members may be present at advertised public meetings of federal, state, and local governments or other entities. Meetings are scheduled as follows:

BOARD	DATE	TIME	LOCATION
Knik RSA Board of Supervisors	07/03/23	6:30 pm	Point MacKenzie Community Center
Abbreviated Plat	07/05/23	8:30 am	Conference Room 110
Animal Care and Regulation Board Appeal Hearing For Case No. 23-01 Appeal the Denial of a Kennel License. Laura Boyd, Appellant	07/05/23	10:00 am	Assembly Chambers
Talkeetna Sewer and Water Board of Supervisors	07/05/23	2:00 pm	Talkeetna Library Conference Room
Planning Administrative Hearing Re: MSB 17.30 - Earth Materials Extraction Activities (Ficklin Located at 20254 West Susitna Parkway, Big Lake, AK 99652)	07/06/23	9:00 am	Assembly Chambers
Platting Board (For Telephonic Public Participation Call 1-855-225-1887 Conference ID: 8573)	07/06/23	1:00 pm	Assembly Chambers
West Lakes FSA Board of Supervisors	07/10/23	6:00 pm	West Lakes, Station 7-3
Caswell Lakes RSA Board of Supervisors	07/10/23	6:30 pm	Sheep Creek Lodge
Abbreviated Plat	07/12/23	8:30 am	Assembly Chambers
Joint Caswell/Willow FSA Boards of Supervisors	07/12/23	6:30 pm	Caswell, Station 13-1
Butte FSA Board of Supervisors Budget Meeting	07/13/23	10:00 am	Butte, Station 2-1
Greater Butte RSA Board of Supervisors	07/13/23	6:00 pm	Butte Community Center
Greater Talkeetna RSA Board of Supervisors	07/13/23	6:30 pm	Zoom Meeting ID: 860 9267 0291 Passcode: 875024

If you would like further information on any of these meetings or are interested in serving on any of the advisory boards, please call the Borough Clerk's Office at 907-861-8683, Monday through Friday, 8 a.m. to 5 p.m. The Borough's website address is: <https://www.matsugov.us/publicmeetings>

Disabled persons needing reasonable accommodation in order to participate at a Borough Board/Commission meeting should contact the Borough ADA Coordinator at 907-861-8432 at least one week in advance of the meeting.

The Planning Team meetings scheduled are: (Planning teams consist of Planning staff and members of the public who are appointed by the Planning Commission to work on community comprehensive plans and special land use districts, and community planning meetings.)
None

The Community Council meetings scheduled are: (Community Councils are not agencies or subgroups of the Borough. There may be a quorum of Mat-Su Borough advisory boards in attendance at community council meetings.)

Talkeetna Community Council	07/03/23	7:00 pm	Talkeetna Library & Zoom ID: 450 310 5937 Passcode: 610720
Willow Area Community Organization	07/05/23	7:00 pm	Willow Community Center
Susitna Community Council	07/06/23	7:00 pm	Upper Susitna Senior Center
Fishhook Community Council — Board Meeting	07/10/23	6:00 pm	St Herman's Church
Big Lake Community Council	07/11/23	7:00 pm	Big Lake Lions Club
Chickaloon Community Council	07/12/23	7:00 pm	Chickaloon Community Center
Meadow Lakes Community Council (mlccak.org for Zoom)	07/12/23	7:00 pm	Meadow Lakes Senior Center

Publish Date: June 30, 2023

0623-27



JUST A REMINDER...

PERSONAL USE OF FIREWORKS IS ILLEGAL WITHIN THE MATANUSKA-SUSITNA BOROUGH, **WITH THE EXCEPTION OF NEW YEARS EVE** from 6pm to 1am **ONLY**.

ORGANIZED EVENTS USING FIREWORKS REQUIRE SPECIAL PERMITS.

VIOLATORS ARE SUBJECT TO FINES OF UP TO \$500 PER OCCURRENCE.

FOR MORE INFORMATION, CONTACT THE MATANUSKA-SUSITNA BOROUGH CODE COMPLIANCE OFFICE AT (907) 861-7822
CONTACT THE CITIES FOR THEIR FIREWORKS ORDINANCES
PLEASE HELP US KEEP THE VALLEY SAFE!!



**YOUR
COOPERATION
IS GREATLY
APPRECIATED!**

MATANUSKA-SUSITNA BOROUGH PUBLIC NOTICE

-ABANDONED VEHICLE SUBJECT TO DISPOSAL-

The following abandoned vehicle is subject to disposal by the Matanuska-Susitna Borough's Land & Resource Management Division. The vehicle was tagged as abandoned.

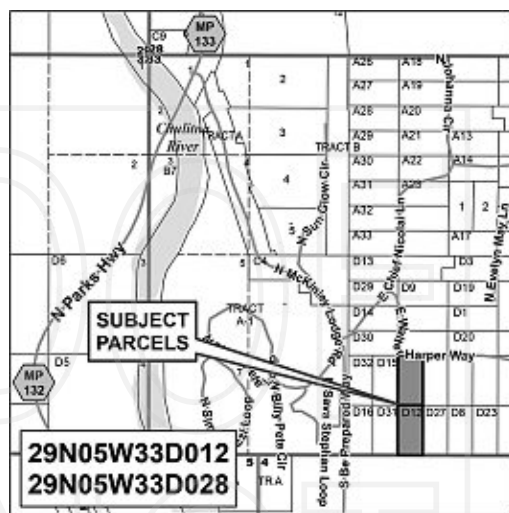
- 1992 Red Toyota Tercel, located in the parking lot at the Jim Creek Recreational Area, Butte AK
- 2008 Blue Toyota Scion, located in the parking lot at the Wasilla Pool Parking Area, Wasilla AK

The vehicle will be disposed of by an auto wrecker on or after July 17, 2023.

FOR MORE INFORMATION, call the MSB Land & Resource Management Division at (907) 861-7867.

Publish Date: June 30, 2023

0623-21



PUBLIC HEARING

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

The Matanuska-Susitna Borough Planning Commission will conduct a public hearing concerning the application on **Monday, August 21, 2023, at 6:00 p.m.** in the Borough Assembly Chambers at 350 E. Dahlia Avenue in Palmer. This may be the only presentation of this item before the Planning Commission, and you are invited to attend. Planning Commission members may not receive or engage in ex-parte contact with the applicant, other interested parties in the application, or members of the public concerning the application or issues presented in the application.

Application materials may be viewed online at www.matsugov.us by clicking "All Public

Notices & Announcements." For additional information, contact Peggy Horton, Planner II, at 907-861-7862. Provide written comments by e-mail to peggy.horton@matsugov.us or by mail to MSB Development Services Division, 350 E. Dahlia Avenue, Palmer, AK 99645.

The public may provide verbal testimony at the meeting or telephonically by calling 1-855-290-3803. To be eligible to file an appeal from a decision of the Planning Commission, a person must be designated an interested party. See MSB 15.39.010 for the definition of an interested party. The procedures governing appeals to the Board of Adjustment and Appeals are contained in MSB 15.39.010-250, which is available on the Borough home page: www.matsugov.us, in the Borough Clerk's office, and at various libraries within the borough.

Comments are due on or before **August 4, 2023**, and will be included in the Planning Commission packet. Please be advised that comments received from the public after that date will not be included in the staff report but will be provided to the Commission at the meeting.

Publish Date: June 30, 2023

0623-26

PRIOR TO BUYING OR BUILDING IN THE MAT-SU BOROUGH CONTACT DEVELOPMENT SERVICES PERMIT CENTER



Different rules apply in different parts of the borough. Important information: maps, codes, forms and applications are available on the Borough website www.matsugov.us
Phone 907-861-7822 or email permitcenter@matsugov.us

DID YOU KNOW?

There are required **SETBACKS** in the borough.

Setbacks for structures are:

10 feet from a property line, 25 feet from a Right-Of-Way or Public Use Easement, and 75 feet from any water body.

Docks, piers, marinas, aircraft hangars and boathouses may be located closer than 75 feet and over the water, provided they are not used for habitation and do not contain sanitary or petroleum fuel storage facilities. Structures permitted over water under this subsection shall conform to all applicable state and federal statutes and regulations.

Violating these setbacks may make financing your property (buying or selling) difficult, and could be expensive to correct after you build.



Edna DeVries, Mayor
(907) 861-8682 - Work
(907) 795-8133 - Cell
Edna.DeVries@matsugov.us

Tim Hale, #1
(907) 590-8243
TimHaleDistrict1@gmail.com

Stephanie Nowers, #2
(907) 831-6299
StephanieNowersDistrict2@gmail.com

Dee McKee, #3
(907) 373-3630
Dee.McKee@matsugov.us

Robert Yundt, #4
(907) 232-8340
robbyundtmsb@gmail.com

Clayton Tew, #5
(907) 841-8688
mokietew@gmail.com

Dmitri Fonov, #6
(907) 861-8546
fonov@matsugov.us

Ron Bernier, #7
(907) 354-7877
Ron.Bernier@matsugov.us

From: [Peggy Horton](#)
To: meadow@mea.coop; row@enstarnaturalgas.com; row@mtasolutions.com; [GCI ROW \(ospdesign@gci.com\)](mailto:GCI ROW (ospdesign@gci.com)); [Fire Code \(Fire.Code@matsugov.us\)](mailto:Fire Code (Fire.Code@matsugov.us)); regpagemaster@usace.army.mil; [Margie Cobb \(Margie.Cobb@matsugov.us\)](mailto:Margie Cobb (Margie.Cobb@matsugov.us)); Eric Phillips; [Tom Adams \(Tom.Adams@matsugov.us\)](mailto:Tom Adams (Tom.Adams@matsugov.us)); Debbie Bakic; Don Thomas; [Jamie Taylor \(Jamie.Taylor@matsugov.us\)](mailto:Jamie Taylor (Jamie.Taylor@matsugov.us)); [Charlyn Spannagel \(Charlyn.Spannagel@matsugov.us\)](mailto:Charlyn Spannagel (Charlyn.Spannagel@matsugov.us)); [Alex Strawn \(Alex.Strawn@matsugov.us\)](mailto:Alex Strawn (Alex.Strawn@matsugov.us)); planning@matsugov.us; [Fred Wagner](#); [Permit Center](#); [Jason Ortiz](#); [Corinne Lindfors \(Corinne.Lindfors@matsugov.us\)](mailto:Corinne Lindfors (Corinne.Lindfors@matsugov.us)); [Andy Dean](#); [John Aschenbrenner](#); [Ron Bernier](#); parkpermitting@alaska.gov; colton.percy@alaska.gov; dnr.scro@alaska.gov; stuart.leidner@alaska.gov; msb.hpc@gmail.com
Subject: Request for Review and Comments: D&S Alaskan Trail Rides CUP
Date: Friday, June 23, 2023 4:53:00 PM

Greetings,

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District, to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

The Planning Commission will conduct a public hearing on this request on August 21, 2023. Application materials may be viewed online at www.matsugov.us by clicking on 'All Public Notices & Announcements'. A direct link to the application material is here:

[Matanuska-Susitna Borough - Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit Application for commercial use \(horse and wagon rides\) \(matsugov.us\)](#)

Comments are due on or before **August 4, 2023**, and will be included in the Planning Commission packet for the Commissioner's review and information. Please be advised that comments received from the public after that date will not be included in the staff report to the Planning Commission but will be provided to the commissioners at the hearing. Thank you for your review.

Regards,
Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

**Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit
Application for commercial use (horse and wagon rides)**

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

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Site Visit Photos

Photographs taken
By Code Compliance Officer
Nick Uphus
on August 5, 2023 site visit

















































































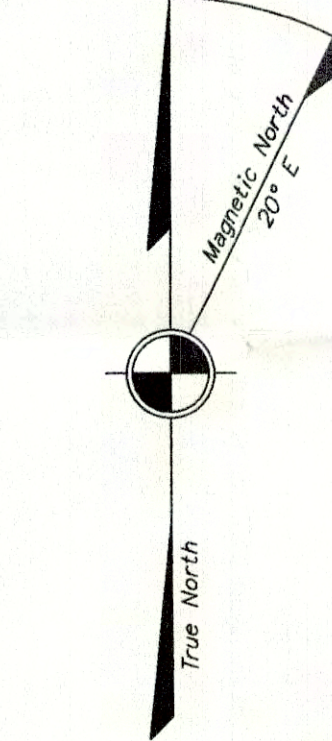








SITE PLAN



MAGNETIC DECLINATION
PER USGS QUADRANGLE MAP
TALKEETNA (C-1), 1958

LEGEND

- | | |
|----------|---|
| ✕ | FOUND 3/4" BLM BRASS
CAP |
| ○ | FOUND 1 1/4" PLASTIC CAP |
| △ | FOUND 5/8" REBAR |
| ○ | FOUND IRON ROD
(SEE NOTE 2) |
| ⊗ | ELECTRIC GROUND REBAR |
| ⊙ | ELECTRIC METER |
| ● | METAL FENCE POST
(SEE NOTE 2) |
| XX XX XX | COMPUTED DATA FROM
RECORD DATA PER PLAT
OF U.S. RECTANGULAR
PLAT OF TOWNSHIP 29
NORTH, RANGE 4 WEST,
SEWARD MERIDIAN |
| —X—X— | FENCING |

NOTE

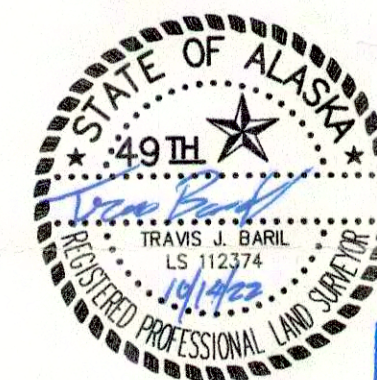
1. THERE IS A BLANKET EASEMENT ACROSS THE SOUTHEAST QUARTER (SE1/4) OF SECTION 33, TOWNSHIP 29 NORTH, RANGE 5 WEST, SEWARD MERIDIAN. THIS IS A PERPETUAL ACCESS ROAD AND UTILITY CONSTRUCTION AND MAINTENANCE EASEMENT. THIS EASEMENT WAS RECORDED AS BOOK 154, PAGE 47 (TALKEETNA RECORDING DISTRICT).
2. THE METAL FENCE POSTS AND IRON ROD ARE NOT CONSIDERED TO BE THE PROPERTY CORNERS OF THE LOTS SHOWN IN THIS AS-BUILT SURVEY.

The improvements situated thereon are within the property lines and do not encroach or overlap onto adjacent properties, nor do any improvements from adjacent properties encroach or overlap onto this property, except as indicated hereon.

No lot corners have been set. The property dimensions shown are from the record plat and are not necessarily as we may have measured. The improvement locations are approximate and have been determined only to the extent that enables us to determine if there are any encroachments onto or off of the lot. Drafting and reproduction may produce graphic inconsistencies; therefore, scaling should be used to obtain the most accurate results. The use of the plat is not intended to provide adequate to additional construction or for determining the exact location of property lines. Only easements from the recorded subdivision plat noted above are shown hereon and it is the responsibility of the owner to determine if any other easements, covenants, or restrictions apply.

© McClintock Land Associates, Inc. (MLA) 2022

This document is copyrighted and is authorized to be used for one real property transaction or project only. Any copy is to be considered unauthorized unless it bears an original surveyor's signature (usually in blue ink) or a stamp showing recording office data. Copyright is not to be used for any other restrictions. If you wish this document has been officially recorded. Lending institutions may also make additional copies for their own records. Liability to MLA is limited to fees received for this project.



RECEIVED
FEB - 8 2023
Mat-Su Borough
Development Services

GRAPHIC SCALE



Scale in Feet
1 inch = 100 feet

AS-BUILT SURVEY OF
LOT D12
(W1/2 SE1/4 SW1/4 SE1/4)
and
LOT D28
(W1/2 NE1/4 SW1/4 SE1/4)

SITUATED WITHIN
SECTION 33, TOWNSHIP 29 NORTH, RANGE 5 WEST,
SEWARD RIVER MERIDIAN, ALASKA

CONTAINING 10.00 ACRES, MORE OR LESS
TALKEETNA RECORDING DISTRICT

PREPARED BY:

McCLINTOCK LAND ASSOCIATES, INC.
16942 NORTH EAGLE RIVER LOOP ROAD
EAGLE RIVER, ALASKA 99577
(907) 694-4499

PREPARED FOR:

STEVE VAN TROBA
P.O. BOX 1437
PALMER, ALASKA 99645
(907) 232-2207

PLOT: 1"=100'	CHK: TB	JOB: 22-222	DWG: AB22-222	FB NO: LL
GRID:TALKEETNA	DWN: JC	DATE: 9-21-22	DISK: MLASERVER	SHEET: 1 OF

DENALI HIGH ADVENTURE SCOUT BASE
TRACT B
(PLAT 2010-2)

APPLICATION MATERIAL



MATANUSKA-SUSITNA BOROUGH

Planning and Land Use Department

Development Services Division

350 East Dahlia Avenue • Palmer, AK 99645

Phone (907) 861-7822

Email: permitcenter@matsugov.us

BASIC APPLICATION FOR A CONDITIONAL USE PERMIT

NOTE: Carefully read instructions and applicable borough code. Fill out forms completely. Attach information as needed. Borough staff will not process incomplete applications.

Required Attachments:

- _____ \$1,500 application fee
- _____ Site plan as detailed on Page 2
- _____ Narrative with operational details and all information required on Pages 1 and 2

Subject Property:

MSB Tax Account ID#(s): 229N05W33D012 - 229N05W33D028

Street Address: 10978 E Walter Harper Way

Facility/Business Name: D & S Alaskan Trail Rides LLC

Name of Property Owner

Steve Van Troba

Mailing: P.O. Box 1437

Palmer AK 99645

Phone: Hm 907-745-2207 Fax 745-2208

Work 745-2208 Cell 907-232-2207

E-mail: akrides@mtaonline.net

Name of Agent / Contact for application

Same

Mailing: _____

Phone: Hm _____ Fax _____

Work _____ Cell _____

E-mail: _____

Application is for a conditional use permit under MSB Code Chapter _____.

NARRATIVE	Attached
1. Provide a description of the use, in detail.	
2. Is the conditional use compatible with and will it preserve or not materially detract from the value, character, and integrity of the surrounding area?	

3. Will the granting of the conditional use permit be harmful to the public health, safety, convenience, and welfare?	no
4. Are sufficient setbacks, lot area, buffers, and other safeguards being provided?	yes
5. Explain in detail how the proposed use meets each standard of the applicable MSB code.	Submitted
6. Hours and days of the week of operation	8hr 6 day
7. Number of trips (average daily traffic and peak hour) associated with use	8hr 6 day 3
8. Noise and or sound mitigation measures	n/a
9. Mitigation measures to minimize or eliminate off-site impacts	n/a

SITE PLAN – Attach a detailed, to scale, site plan clearly showing the following information:	Attached
1. Proposed and existing structure(s) on the site, including well and septic system. Indicate which structure(s) will be used for this use. Draw lot dimensions and indicate setback distance of structure(s) from the lot lines, rights-of-way, and waterbodies.	Submitted
2. Dimensions of all structures	"
3. Location of the use or uses to be made of the development	"
4. Location of other uses on the site	"
5. Signage – Existing and Proposed	n/a
6. Location and dimensions for all access points to and from the site to public rights-of-way or public access easements	Submitted
7. Buffering – Fences, trees, topography or berms	"
8. Drainage	"
9. Vehicular and pedestrian circulation patterns	n/a
10. Exterior site lighting	Submitted
11. Location and dimensions of parking areas to be provided	n/a
12. Location of other uses on the site	n/a
13. Scale and north arrow	Submitted

OTHER – Attach all other permits, plans, and approvals, which are associated with the proposed use, such as: driveway permits, ADEC approvals, Fire Marshal reviews etc...	Attached	Applied For
Permit Name/#		
Permit Name/#		
Permit Name/#		
Permit Name/#		

Prior to the public hearing, the applicant must also pay the mailing and advertising fees associated with the application. Staff will provide applicant with a statement of advertising and mailing charges. Payment must be made **prior** to the application presentation before the Borough Planning Commission.

OWNER'S STATEMENT: I am owner or authorized agent of the following property:

MSB Tax Account ID #(s) 229N05W330028-229N05W10012 and,
I hereby apply for approval an conditional use permit on that property as described in this application.

I understand all activity must be conducted in compliance with all applicable standards of MSB 17.60 and with all other applicable borough, state, and federal laws.

I understand that other rules such as local, state, and federal regulations, covenants, plat notes, and deed restrictions may be applicable and other permits or authorizations may be required. I understand that the borough may also impose conditions and safeguards designed to protect the public's health, safety, and welfare, and ensure the compatibility of the use with other adjacent uses.

I understand that it is my responsibility to identify and comply with all applicable rules and conditions, covenants, plat notes, and deed restrictions, including changes that may occur in such requirements.

I understand that changes from the approved conditional use permit may require further authorization by the Borough Planning Commission.

I grant permission for borough staff members to enter onto the property as needed to process this application and monitor compliance. Such access will at a minimum, be allowed when the activity is occurring and, with prior notice, at other times necessary to monitor compliance.

The information submitted in this application is accurate and complete to the best of my knowledge.

Steve Van Troba Steve Van Troba 2/2/23
Signature: Property Owner Printed Name Date

Steve Van Troba Steve Van Troba 2/2/23
Signature: Agent Printed Name Date

D & S Alaskan Trail Rides, Inc
P.O. Box 1437
Palmer, Alaska 99645
Tel/Fax (907)745-2208

March 28, 2022

Matanuska-Susitna Borough
Planning Commission
East Dahlia
Palmer, Alaska 99645

Reference: Statement of Purpose for Mat-Su Borough Planners and Assembly

Following D & S Alaskan Trail Rides Inc. application and documentation for a Conditional Use Permit in the Mat-Su Borough's Land use District. Our proposal is to establish a horse and wagon seasonal (May-September) operations facility at Mile 133 of the Parks Highway just east of the Mt. McKinley Princess Lodge. The site is well situated for horse rides and wagon tours by visitors to the Mat-Su Borough. The proposed site incorporates zoning requirements set forth by **Borough Ordinance, Chapter 17.17.**

D & S Alaskan Trail Rides, Inc. Plans conform to the requirement stated in **Article 1. General Provisions, section 17.17.020, Purpose**-The area within the boundaries of this special land use is zoned for recreational uses.

D & S Alaskan Trail Rides, Inc. planned operations are of a recreational nature and for support of like activities. Guided horse riding and wagon operations will include sightseeing and trail riding in remote location. The guests will be transported from Mt. McKinley Princess Lodge to the horseback riding trail head.

D & S Alaskan Trail Rides, Inc. permit application fulfills the requirement of **ARTICLE 11, Section 17.17.060 Conditional Uses; A.5**, commercial uses; and all requirements of **ARTICLE 11, Section 17.17.080 through SECTION 17.17.130**, (see Below), as well as meeting the requirements of **Article 11**

GENERAL, STANDARDS SECTION 17.17.180, A.2, the conditional use fulfills all other requirements of this chapter pertaining to the conditional use in question; as follows:

Section 17.17.080 COMPLIANCE- no building, structure, land or water area located within this special land use district shall thereafter be used or occupied, and no building, structure or part thereof be erected, constructed, moved repaired, or structurally altered except in conformity with regulations in this chapter and D & S Alaskan Trail Rides, Inc. plans to conform with regulations in this chapter and D & S Alaskan Trail Rides, Inc. does not propose any use stated in **ARTICLE 11 Section 17.17.070, PROHIBITED USES**.

Section 17.17.090 Building Height Limits- the maximum building height shall be 35 feet above finished grade.

D & S Alaskan Trail Rides, Inc. plans a maximum height of 25 feet in our current buildings. The pole barn will be open sided with a roof, 21 X 14 feet and Bunk house for employees is 32 X 68.

SECTION 17.17. 100 LOT AREA- the minimum lot area for any use shall be ten acres. The lot length to width ratio shall be a maximum of 1:1. The site proposed by D & S Alaskan Trail Rides, Inc. is approximately 10 acres in size with length and width measurements nearly equal.

SECTION 17.17.110 SETBACK REQUIREMENTS; NO Signage

Section 17.17.120 VEGETATION BUFFER; A natural vegetation buffer shall be maintained for a minimum of 50 feet around LOT 55 and 56.

D & S Alaskan Trail Rides, Inc. proposed facility will be located on ten acres (TAX # 29N05W33D012 & 29N05W33D028). 160 acres, to the east of Mt McKinley Princess Lodge. This area has natural buffer vegetation which will remain to an acceptable extent. D & S Alaskan Trail Rides, Inc. plans to keep the natural buffer vegetation.

Section 17.17.130 SIGNS- Again addressed no signs: on top of building flashing rotating, animated or intermittent lighted. Signs cannot exceed 32 square feet in area and are regulated by CFR'S.

D & S Alaskan Trail Rides, Inc. planned operations meets all of the requirements stated in **ARTICLE 111 Section 17.17.180 GENERAL Standards, A**; the conditional use will not detract from the value, character, or integrity of LOT 55 & 56.

Our proposed activities will add to the value, character, and integrity of the Mat-Su Borough . We anticipate using 20 horses offering Borough visitors sightseeing tours by guided horseback. Horses are an environmentally sound means of access to remote areas.

D & S Alaskan Trail Rides, Inc. proposal meets the requirement of **ARTICLE 11GENERAL STANDARDS, SECTION 17.17.180, A. 3**; granting the conditional use permit to the public health safety, convenience

and welfare; D & S Alaskan Trail Rides, Inc. proposed facility will enhance public health, safety, convenience, and welfare in several ways.

We would provide additional protection of sealed storage Van to store our horse grain, making it a safe place to store grain in without traction of bears.

We have the experience of fulfilling various needs for emergency response. The addition of our horses based in this region would provide for an additional level of emergency response for residents as well as visitors to the northern reaches of MAT-Su Borough. When weather prohibits helicopters and aircraft from flying.

D & S Alaskan Trail Rides, Inc. planned operations will provide several job opportunities for local residents. D & S Alaskan Trail Rides, Inc. prefers to hire locally if qualified applicants are available.

Local revenues will be realized by business in the area. D & S Alaskan Trail Rides, Inc. will seek local suppliers in the area for construction and operational requirements. Trickle down effects of revenue and wage earnings from D & S Alaskan Trail Rides, Inc. planned operations will provide economic benefits to the area indirectly.

D & S Alaskan Trail Rides, Inc. proposal meets all requirements of **ARTICLE 11 SECTION 17.17.180, A.4-** sufficient access, setbacks, lot area, parking spaces, buffers, and other safeguards are being provided to meet the condition; as follows: Sufficient parking will be provided at Mt McKinley Princess Lodge also and sufficient turnaround space will be provided at D & S Alaskan Trail Rides, Inc. ten acres trail head site.

Lot Area, setbacks and buffers are identified in previous descriptions above.

D & S Alaskan Trail Rides, Inc. proposal will meet the commissions requirements of **ARTICLE 111, SECTION 17.17.180, A.5 -**, if the permit is for public use or structure, the commission must find that the proposed use or structure is located in a manner which will maximize public benefits; D & S Alaskan Trail Rides, Inc. will maximize public benefit increasing the tourism infrastructure in the area.

D & S Alaskan Trail Rides, Inc. proposed location benefits the public immensely through close proximity to overnight lodging at the Mt McKinley Princess Lodge and Mary's McKinley View Lodge. Additional overnight facilities are speculated to develop in the vicinity as well. Horseback and wagon rides as an activity, is a fast growing experience of visitors to Alaska. Many highway travelers see Mt. McKinley at or near this location and desire horseback, or wagon ride in the untouched wilderness of the Talkeetna Mountains, The proposed location will help to meet visitor expectations of an "experience of a lifetime" activity by enhancing this area of the Mat-Su Borough.

If you have any questions about this statement of our proposed use or about the Conditional Land Use Permit Application, you can direct them to Steve Van Troba at (907)745-2208 or cell # (907) 232-2207.

Sincerely,



Steve Van Troba

From: Diane dianedenali@gmail.com
Subject: Permission to use Property in Denali
Date: Mar 14, 2023 at 8:26:12 PM
To: akrides@mtaonline.net



To Whom it May Concern:

I Diane L. Murray give Permission to Steve van Troba DBA, D&S AK Trail Rides to use my property located at Mile 133 Parks Highway with legal description of TOWNSHIP 29 N RANGE 5W SECTION 33 with Tax account #'s of: 229N05W33D007, 229N05W33D024, 229N05WD023, 229N05W33D008, 229N05W33D027.

Dated: 03/14/2023

Diane I. Murray

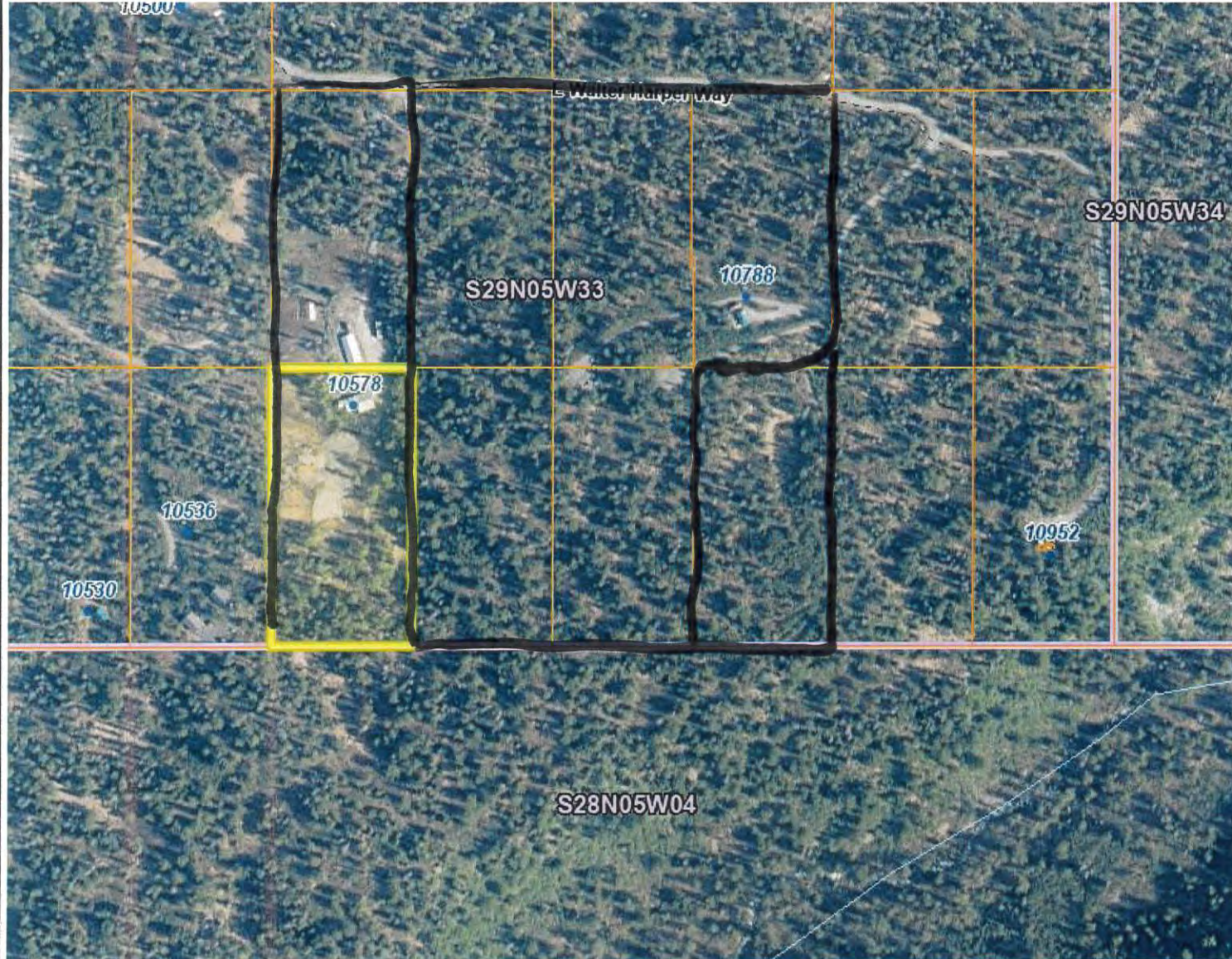
[907-952-2003](tel:907-952-2003)

OFS Trail Rides

August 21, 2023 Planning Commission Packet
141 of 450



Matanuska-Susitna Borough



Legend

○ Road Mileposts

Roads

- Highway
- Major Road
- Medium Road
- Minor Road
- Ramp
- Primitive Road
- Private Road
- - - Not Constructed

+ Alaska Railroad

▣ Mat-Su Borough Boundary

▣ Incorporated Cities

• Address Numbers

▣ Parcels

— Government Lot Lines

▣ Lakes and Rivers

— Streams

▣ 100 year Flood Zone

▣ Section Lines

RECEIVED
APR 17 2023

Mat-Su Borough
Development Services

1:9,028



0.3 0 0.14 0.28 Miles

THIS MAP IS NOT TO BE USED FOR NAVIGATION

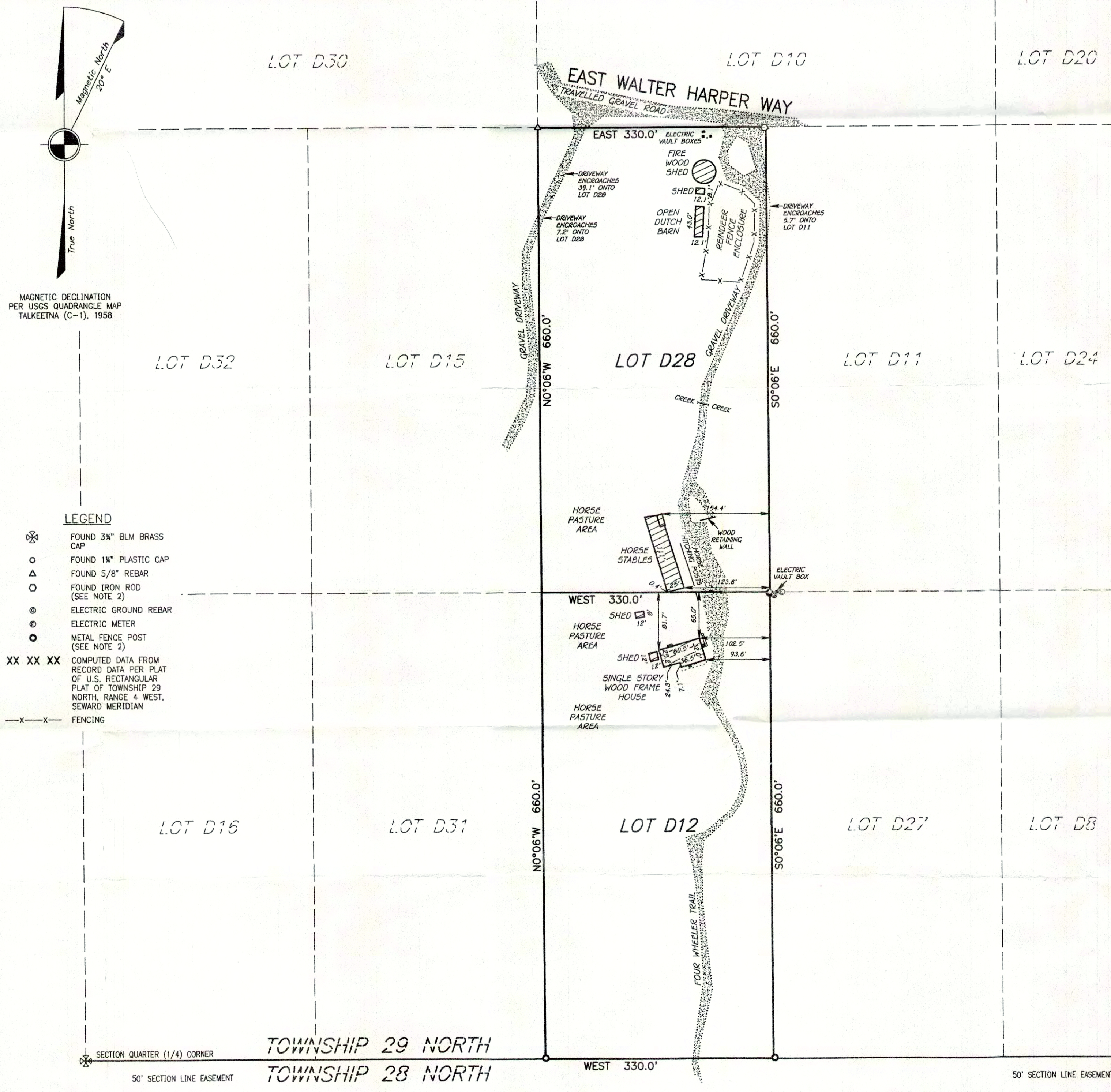
WGS_1984_Web_Mercator_Auxiliary_Sphere
© Matanuska-Susitna Borough

Reported on 04/17/2023 03:19 PM

This map is solely for informational purposes only. The Borough makes no express or implied warranties with respect to the character, function, or capabilities of the map or the suitability of the map for any particular purpose beyond those originally intended by the Borough. For information regarding the full disclaimer and policies related to acceptable uses of this map, please contact the Matanuska-Susitna Borough GIS Division at 907-861-7858.

Notes

This map was automatically generated using Geocortex Essentials.



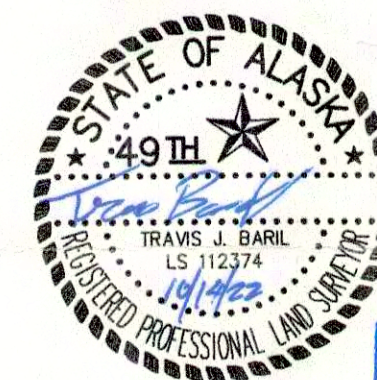
NOTE

1. THERE IS A BLANKET EASEMENT ACROSS THE SOUTHEAST QUARTER (SE1/4) OF SECTION 33, TOWNSHIP 29 NORTH, RANGE 5 WEST, SEWARD MERIDIAN. THIS IS A PERPETUAL ACCESS ROAD AND UTILITY CONSTRUCTION AND MAINTENANCE EASEMENT. THIS EASEMENT WAS RECORDED AS BOOK 154, PAGE 47 (TALKEETNA RECORDING DISTRICT).
2. THE METAL FENCE POSTS AND IRON ROD ARE NOT CONSIDERED TO BE THE PROPERTY CORNERS OF THE LOTS SHOWN IN THIS AS-BUILT SURVEY.

The improvements situated thereon are within the property lines and do not encroach or overlap onto adjacent properties, nor do any improvements from adjacent properties encroach or overlap onto this property, except as indicated hereon.

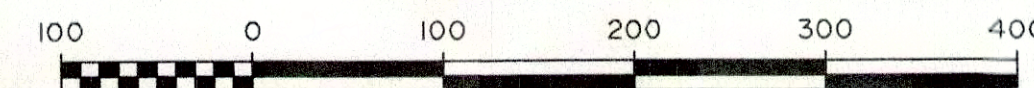
No lot corners have been set. The property dimensions shown are from the record plat and are not necessarily as we may have measured. The improvement locations are approximate and have been determined only to the extent that enables us to determine if there are any encroachments onto or off of the lot. Drafting and reproduction may produce graphic inconsistencies; therefore scaling should not be attempted to determine unshown dimensions. This drawing is not adequate for additional construction or for determining the exact location of property lines. Only easements from the recorded subdivision plat noted above are shown hereon and it is the responsibility of the owner to determine if any other easements, covenants, or restrictions exist.

© McClintock Land Associates, Inc. (MLA) 2022
This document is copyrighted and is authorized to be used for one real property transaction or project only. Any copy is to be considered unauthorized unless it bears an original surveyor's signature (usually in blue ink) or a stamp showing recorder's office data. Copyright restrictions (but not re-use restrictions) are waived if this document has been officially recorded. Lending institutions may also make additional copies for their own records. Liability to MLA is limited to fees received for this project.



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Mat-Su Borough
Development Services

GRAPHIC SCALE



Scale in Feet
1 inch = 100 feet

AS-BUILT SURVEY OF
LOT D12
(W1/2 SE1/4 SW1/4 SE1/4)
and
LOT D28
(W1/2 NE1/4 SW1/4 SE1/4)

SITUATED WITHIN
SECTION 33, TOWNSHIP 29 NORTH, RANGE 5 WEST,
SEWARD RIVER MERIDIAN, ALASKA

CONTAINING 10.00 ACRES, MORE OR LESS
TALKEETNA RECORDING DISTRICT

PREPARED BY: McCLINTOCK LAND ASSOCIATES, INC. 16942 NORTH EAGLE RIVER LOOP ROAD EAGLE RIVER, ALASKA 99677 (907) 694-4499	PREPARED FOR: STEVE VAN TROBA P.O. BOX 1437 PALMER, ALASKA 99645 (907) 232-2207
---	--

PLOT: 1"=100'	CHK: TB	JOB: 22-222	DWG: AB22-222	FB NO: LL
GRID: TALKEETNA	DWN: JC	DATE: 9-21-22	DISK: MLASERVER	SHEET: 1 OF 1

DENALI HIGH ADVENTURE SCOUT BASE
TRACT B
(PLAT 2010-2)

LOT D30

LOT D10

EAST WALTER HARPER WAY
TRAVELLED GRAVEL ROAD

EAST 330.0'

ELECTRIC
VAULT BOXES

FIRE
WOOD
SHED

SHED

OPEN
DUTCH
BARN

REINDEER
FENCE
ENCLOSURE

DRIVEWAY
ENCROACHES
39.1' ONTO
LOT D28

DRIVEWAY
ENCROACHES
7.2' ONTO
LOT D28

DRIVEWAY
ENCROACHES
5.7' ONTO
LOT D11

WOODS

LOT D15

LOT D28

LOT D11

NATURAL
VEGETATION
CREEK

HORSE
PASTURE
AREA

HORSE
STABLES

HITCHING
POST

WOOD
RETAINING
WALL

ELECTRIC
VAULT BOX

WEST 330.0'

SHED

HORSE
PASTURE
AREA

SHED

SINGLE STORY
WOOD FRAME
HOUSE

HORSE
PASTURE
AREA

LOT D31

LOT D12

LOT D27

WOODED AREA

WOODED

FOUR WHEELER TRAIL

TOWNSHIP 29 NORTH
TOWNSHIP 28 NORTH

WEST 330.0'

DENALI HIGH ADVENTURE SC
TRACT B

Action: 06/06/23

**MATANUSKA-SUSITNA BOROUGH
RESOLUTION SERIAL NO. 23-055**

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY WAIVING THE FEE FOR A CONDITIONAL USE PERMIT WITHIN DENALI STATE PARK SPECIAL LAND USE DISTRICT.

WHEREAS, MSB 17.17 Denali State Park Special Land Use District was adopted in 1991; and

WHEREAS, a conditional use permit is required for all commercial uses within the boundary of the district; and

WHEREAS, in 1997 Steven K. Van Troba dba Alaska Trail Rides Inc. was issued a conditional use permit for the operation of a seasonal trail wagon ride business and tourist-oriented gold panning operation within the boundary of the district; and

WHEREAS, in 1997 the conditional use permit and associated application material were erroneously developed for a separate property described as the SE 1/4 SE 1/4 SE 1/4 of Township 29 North, Range 5 West, Section 33, Seward Meridian, which was not owned by Mr. Van Troba and was approximately 1000 feet from the property owned by him; and

WHEREAS, Mr. Van Troba operated his business for nearly 20 years without knowledge of the error and without any complaint's being made against him, but would now like to modify the conditional use permit in order to accurately reflect the correct location of the business; and

WHEREAS, the fee for modification of a Denali State Park conditional use permit is \$1,500; and

WHEREAS, this process has taken over two years and is still ongoing and now requires a new permit with a new fee of \$1,500.

NOW, THEREFORE, BE IT RESOLVED, the fee for a conditional use permit for commercial use within the Denali State Park Special Land Use District on Parcels W 1/2 SE 1/4 SW 1/4 SE 1/4, 29N05W33D012 and W 1/2 NE 1/4 SW 1/4 SE 1/4, 29N05W33D028 is hereby waived.

BE IT FURTHER RESOLVED, this is a one-time fee waiver and shall expire two years from the date of adoption of this resolution.

ADOPTED by the Matanuska-Susitna Borough Assembly this 6 day of June, 2023.


EDNA DeVRIES, Borough Mayor

ATTEST:


LONNIE R. McKECHNIE, CMC, Borough Clerk

(SEAL)

PASSED UNANIMOUSLY: Hale, Nowers, McKee, Yundt, Tew, Fonov and
Bernier

CORRESPONDENCE

From: [Peggy Horton](#)
To: akrides@mtaonline.net
Subject: Mailing and Advertising fees for Conditional use permit
Date: Friday, August 4, 2023 5:07:00 PM

Mr. Van Tropa,

In accordance with the requirements of MSB 17.03 – Public Notification, the applicant shall pay the cost of mailings or advertisements required by the ordinance specific to that action. Please be advised of the following charges:

Advertising:	\$101.25
Mailing:	<u>\$ 10.90</u>
TOTAL DUE:	\$112.15

The advertisement was published in the June 30, 2023, Frontiersman. ***These fees must be paid prior to the permit being issued.*** Should you have any questions or require additional information please contact me.

Thank you,
Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

From: [Peggy Horton](#)
To: akrides@mtaonline.net
Subject: D&S Alaskan Trail Rides Public Hearing 8-21-23
Date: Wednesday, June 14, 2023 8:38:00 AM

Good Morning,

I have scheduled a public hearing at the **August 21, 2023** Planning Commission meeting. The meeting will be held in the Borough Assembly Chambers at 350 E. Dahlia Avenue in Palmer.

I will conduct a site visit prior to the public hearing and let you know the findings of the visit. I'll be sure to work out the date of the visit with you later.

Please note: You will have a 15-minute period at the public hearing. You may use this time as you wish. Some applicants chose to have a presentation to clarify any topics of concern, but most use it just to be available for questions the Planning Commission may have. In any event, it is available to you.

Public Notice Process: I will begin the public notice process this week. I will let you know if we receive any public comments. After receiving the mailing and advertising costs, I will send you a letter asking for payment. You can expect the request in about 3-5 weeks via email.

Please let me know if you have any questions. Thank you for your time.

Respectfully,

Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

From: [Peggy Horton](#)
To: ["akrides@mtaonline.net"](mailto:akrides@mtaonline.net)
Subject: Assembly approved fee waiver for D&S Trail Rides Conditional Use Permit
Date: Friday, June 9, 2023 4:13:00 PM
Attachments: [Assembly Resolution 2023-RS-055 fee waiver.pdf](#)

Good day,

I've attached the approved and signed fee waiver for D&S Trail Rides. Planning staff can now move forward with the processing of the Conditional Use Permit. You'll be hearing from me within the week about a public hearing date for the conditional use permit.

Thank you,

Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

From: [Peggy Horton](#)
To: akrides@mtaonline.net
Subject: D & S Trail Rides CUP Application
Date: Wednesday, April 19, 2023 12:03:00 PM
Attachments: [Basic CUP Application.pdf](#)

Mr. Van Troba,

In reviewing your application, I find page 2 of the application needs to be added. Please fill out page 2 of the attached application form, and send it back to me, or you may bring it in this afternoon. Our meeting with Mr. George Hays is at 1:30 p.m. today in Room 204, upstairs in the borough building.

Thank you,

Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

COMMENTS

Agency Comments

From: [Daniel Dahms](#)
To: [Peggy Horton](#)
Cc: [Brad Sworts](#); [Jamie Taylor](#); [Tammy Simmons](#)
Subject: RE: Request for Review and Comments: D&S Alaskan Trail Rides CUP
Date: Wednesday, July 12, 2023 11:17:41 AM

Peggy,

Easement (document number 2015-000173-0) for East Walter Harper Way, states, “Private Easement. Nothing in this Agreement shall be deemed to be a dedication of any portion of the Easement Areas for public use. All rights, easements and interests herein created are private and do not constitute a grant for public use or benefit.” Applicant should provide documentation of developed and legal access to the site.

Site plan shows a creek running through the site. Petitioner should outline what measures are proposed to maintain the water quality of the creek.

Daniel Dahms, PE
Department of Public Works
Pre-Design and Engineering Division

From: Jamie Taylor <Jamie.Taylor@matsugov.us>
Sent: Monday, July 3, 2023 5:05 PM
To: Daniel Dahms <Daniel.Dahms@matsugov.us>; Tammy Simmons <Tammy.Simmons@matsugov.us>; Brad Sworts <Brad.Sworts@matsugov.us>
Subject: FW: Request for Review and Comments: D&S Alaskan Trail Rides CUP

FYI

From: Peggy Horton <Peggy.Horton@matsugov.us>
Sent: Friday, June 23, 2023 4:54 PM
To: meadow@mea.coop; row@enstarnaturalgas.com; row@mtasolutions.com; GCI ROW (ospdesign@gci.com) <ospdesign@gci.com>; Fire Code <Fire.Code@matsugov.us>; regpagemaster@usace.army.mil; Margie Cobb <Margie.Cobb@matsugov.us>; Eric Phillips <Eric.Phillips@matsugov.us>; Tom Adams <Tom.Adams@matsugov.us>; Debbie Bakic <Debbie.Bakic@matsugov.us>; Don Thomas <Don.Thomas@matsugov.us>; Jamie Taylor <Jamie.Taylor@matsugov.us>; Charlyn Spannagel <Charlyn.Spannagel@matsugov.us>; Alex Strawn <Alex.Strawn@matsugov.us>; Planning <MSB.Planning@matsugov.us>; Fred Wagner <Frederic.Wagner@matsugov.us>; Permit Center <Permit.Center@matsugov.us>; Jason Ortiz <Jason.Ortiz@matsugov.us>; Corinne Lindfors <Corinne.Lindfors@matsugov.us>; Andy Dean <Andy.Dean@matsugov.us>; John Aschenbrenner <John.Aschenbrenner@matsugov.us>; Ron Bernier <Ron.Bernier@matsugov.us>; parkpermitting@alaska.gov; colton.percy@alaska.gov; dnr.scro@alaska.gov; stuart.leidner@alaska.gov; msb.hpc@gmail.com
Subject: Request for Review and Comments: D&S Alaskan Trail Rides CUP

Greetings,

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District, to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

The Planning Commission will conduct a public hearing on this request on August 21, 2023. Application materials may be viewed online at www.matsugov.us by clicking on 'All Public Notices & Announcements'. A direct link to the application material is here:

[Matanuska-Susitna Borough - Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit Application for commercial use \(horse and wagon rides\) \(matsugov.us\)](http://www.matsugov.us)

Comments are due on or before **August 4, 2023**, and will be included in the Planning Commission packet for the Commissioner's review and information. Please be advised that comments received from the public after that date will not be included in the staff report to the Planning Commission but will be provided to the commissioners at the hearing. Thank you for your review.

Regards,
Peggy Horton
Matanuska-Susitna Borough
Development Services Division
Planner II
907-861-7862

From: [Fred Wagner](#)
To: [Peggy Horton](#)
Subject: RE: Request for Review and Comments: D&S Alaskan Trail Rides CUP
Date: Monday, June 26, 2023 11:44:45 AM

[Platting has no comments or concerns.](#)

From: Peggy Horton <Peggy.Horton@matsugov.us>
Sent: Friday, June 23, 2023 4:54 PM
To: mearow@mea.coop; row@enstarnaturalgas.com; row@mtasolutions.com; GCI ROW (ospdesign@gci.com) <ospdesign@gci.com>; Fire Code <Fire.Code@matsugov.us>; regpagemaster@usace.army.mil; Margie Cobb <Margie.Cobb@matsugov.us>; Eric Phillips <Eric.Phillips@matsugov.us>; Tom Adams <Tom.Adams@matsugov.us>; Debbie Bakic <Debbie.Bakic@matsugov.us>; Don Thomas <Don.Thomas@matsugov.us>; Jamie Taylor <Jamie.Taylor@matsugov.us>; Charlyn Spannagel <Charlyn.Spannagel@matsugov.us>; Alex Strawn <Alex.Strawn@matsugov.us>; Planning <MSB.Planning@matsugov.us>; Fred Wagner <Frederic.Wagner@matsugov.us>; Permit Center <Permit.Center@matsugov.us>; Jason Ortiz <Jason.Ortiz@matsugov.us>; Corinne Lindfors <Corinne.Lindfors@matsugov.us>; Andy Dean <Andy.Dean@matsugov.us>; John Aschenbrenner <John.Aschenbrenner@matsugov.us>; Ron Bernier <Ron.Bernier@matsugov.us>; parkpermitting@alaska.gov; colton.percy@alaska.gov; dnr.scro@alaska.gov; stuart.leidner@alaska.gov; msb.hpc@gmail.com
Subject: Request for Review and Comments: D&S Alaskan Trail Rides CUP

Greetings,

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District, to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33 Seward Meridian.

The Planning Commission will conduct a public hearing on this request on August 21, 2023. Application materials may be viewed online at www.matsugov.us by clicking on ‘All Public Notices & Announcements’. A direct link to the application material is here:

[Matanuska-Susitna Borough - Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit Application for commercial use \(horse and wagon rides\) \(matsugov.us\)](#)

Comments are due on or before **August 4, 2023**, and will be included in the Planning Commission packet for the Commissioner’s review and information. Please be advised that comments received from the public after that date will not be included in the staff report to the Planning Commission but will be provided to the commissioners at the hearing. Thank you for your review.

Regards,
Peggy Horton
Matanuska-Susitna Borough
Development Services Division

Planner II
907-861-7862

From: [Rick Antonio](#)
To: [Peggy Horton](#)
Cc: [Kim Sollien](#)
Subject: Request for Review and Comments: D&S Alaskan Trail Rides CUP #29N05W33D012 & 29N05W33D028
Date: Tuesday, August 1, 2023 12:59:29 PM

[Matanuska-Susitna Borough - Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit Application for commercial use \(horse and wagon rides\) \(matsugov.us\)](#)

Public Hearing Notice for Denali State Park Special Land Use District Conditional Use Permit Application for commercial use (horse and wagon rides)

Steve Van Troba, dba D&S Alaskan Trail Rides, submitted an application for a conditional use permit under MSB 17.17 – Denali State Park Special Land Use District to operate a commercial use (horse and wagon rides). Located at 10578 E Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028

matsugov.us

Peggy,

The Planning Division has no objection to the proposed CUP as long as operations are compliant with Denali State Park SPUD.

Rick Antonio
Planner II
Planning & Land Use Division
Matanuska-Susitna Borough
rick.antonio@matsugov.us
stormwater@matsugov.us
Office- 907.861.7815
Remote- 907.707.4591

Public Comments

Staff received no public comments for this
Conditional Use Permit

DRAFT

**PLANNING COMMISSION
RESOLUTION**

By: Peggy Horton
Introduced: August 7, 2023
Public Hearing: August 21, 2023
Action:

MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION RESOLUTION NO. 23-33

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH PLANNING COMMISSION APPROVING A CONDITIONAL USE PERMIT FOR A COMMERCIAL USE WITHIN THE DENALI STATE PARK SPECIAL USE DISTRICT AT 10578 EAST WALTER HARPER WAY, TAX ID #29N05W33D012 & 29N05W33D028, WITHIN TOWNSHIP 29 NORTH, RANGE 5 WEST, SECTION 33, SEWARD MERIDIAN.

WHEREAS, Steve Van Troba, dba D & S Alaskan Trail Rides, applied for a Conditional Use Permit to operate a commercial use (horse and wagon rides) at 10578 East Walter Harper Way, Tax ID #29N05W33D012 & 29N05W33D028, within Township 29 North, Range 5 West, Section 33, Seward Meridian; and

WHEREAS, MSB 17.17.060(A)(5) requires a Conditional Use Permit for commercial use within Denali State Park Special Land Use District; and

WHEREAS, commercial uses are prohibited from operating within the district without a lawfully issued Conditional Use Permit; and

WHEREAS, according to borough records, D & S Alaskan Trail Rides first received a conditional use permit in the Denali State Park Special Land Use District in 1997, offering commercial horse and wagon rides to the public; and

WHEREAS, according to borough records, D & S Alaskan Trail Rides 1997 Conditional Use Permit was approved for properties separate from the subject properties; and

WHEREAS, the subject property consists of two five-acre parcels; and

WHEREAS, the subject property includes those parcels currently identified as Tax ID #29N05W33D012 and #29N05W33D028, known as parcels D012 and D028; and

WHEREAS, according to the application material, the facility transports guests from Mt. McKinley Princess Lodge to the subject parcels for guided horse and wagon rides; and

WHEREAS, the proposed use will access East Walter Harper Way, a private road; and

WHEREAS, the surrounding area has a mixture of recreational, residential, and commercial uses; and

WHEREAS, the application material indicates that the maximum height of any structure on the property is approximately 25 feet above finished grade; and

WHEREAS, the subject property is more than 3,700 feet from the Parks Highway right-of-way; and

WHEREAS, according to the application material, the horse stables are located 123.6' from the western property line, approximately 100' from the creek shown on the site plan, approximately 550' from the northern property line, approximately 150' feet from the east property line, and approximately 0.4' from the common property line with parcel D012; and

WHEREAS, according to the application material, the structures on parcel D028, other than the horse stables, meet the setback requirements of MSB 17.55; and

WHEREAS, according to the application material, a single-story wood frame house and two sheds lie within parcel D012 and meet all setback requirements of MSB 17.55; and

WHEREAS, the closest public right-of-way is a 50' wide section line easement adjoining the south property line of parcel D012 and approximately 650' to the south of the single-story wood frame house; and

WHEREAS, according to the application material, no signs exist for the facility; and

WHEREAS, according to the application material, the operation is seasonal, May through September; and

WHEREAS, according to the application material, approximately three visitor trips are taken each day; and

WHEREAS, according to the application material, the operation will be open approximately eight hours a day, six days a week; and

WHEREAS, site visit photos taken by staff on August 5, 2023, confirm that Mr. Van Troba has begun dismantling that portion of the horse stables' south end that lies within the setback; and

WHEREAS, according to the application material, Diane L. Murray, the owner of five neighboring lands to the west, granted permission for D & S Alaskan Trail Rides to use her property for the commercial use; and

WHEREAS, according to the application material, the guided horse and wagon rides will remain on trails where permission has been granted to occur; and

WHEREAS, the applicant provided information fulfilling the requirements of this chapter pertaining to the conditional use permit for a commercial use; and

WHEREAS, the applicant is not proposing any outdoor amplified sound activities; and

WHEREAS, within the application material, the D & S Trail Rides map indicates the trails used during the guided horse and wagon rides; and

WHEREAS, the closest residential structure to the use is approximately 500 feet southwest of the single-story wood frame house on parcel D012; and

WHEREAS, the commercial site has sufficient parking area for their two commercial vans and employees; and

WHEREAS, according to the application material, sufficient parking is provided at Mt. McKinley Princess Lodge; and

WHEREAS, according to the application material, the applicant intends to keep the natural vegetation on the property, acting as a buffer; and

WHEREAS, the Planning Commission has reviewed this application with respect to the applicable standards set forth in MSB 17.17; and

WHEREAS, the Planning Commission conducted a public hearing on August 21, 2023, on this matter; and

NOW, THEREFORE, BE IT RESOLVED, that the Matanuska-Susitna Borough Planning Commission hereby adopts the aforementioned findings of fact and makes the following conclusions of law supporting approval of Planning Commission Resolution 23-33:

1. The commercial use requires a Conditional Use Permit (MSB 17.17.060(A)(5)).

2. The commercial use is developed in accordance with the building height requirements (MSB 17.17.090).

3. The commercial use, with conditions, is developed in accordance with the setback requirements (MSB 17.17.110).

4. The commercial use is developed in accordance with the signage requirements (MSB 17.17.130).

5. The commercial use does not detract from the value, character, or integrity of Denali State Park (MSB 17.17.180(A)(1)).

6. The application material meets the requirements of this chapter pertaining to the conditional use permit for a commercial use (MSB 17.17.180(A)(2)).

7. The commercial use will not be harmful to the public health, safety, convenience, and welfare (MSB 17.17.180(A)(3)).

8. The commercial use, with conditions, provides sufficient access, setbacks, lot area, parking space, buffers, and other safeguards to meet the conditions (MSB 17.17.180(A)(4)).

BE IT FURTHER RESOLVED, the Planning Commission finds this application does meet the standards of MSB 17.17 and does hereby

approve the conditional use permit for the commercial use within the Denali State Park Special Use District, with the following conditions:

1. The operation shall comply with all other applicable federal, state, and local regulations.
2. All aspects of the operation shall comply with the description detailed in the application material and with the conditions of this permit. An amendment to the Conditional Use Permit shall be required prior to any expansion of the conditional use.
3. The operation shall comply with the maximum permissible sound level limits allowed per the requirements of MSB 8.52 - Noise, Amplified Sound, and Vibration.
4. Remove a portion of the horse stables' south end to bring the building into compliance with MSB 17.55. Provide staff with photographic evidence of the removal.

ADOPTED by the Matanuska-Susitna Borough Planning Commission
this 21st day of August 2023.

C.J. Koan, Chair

ATTEST

CORINNE LINDFORS, Planning Clerk

(SEAL)

DRAFT

YES:

NO:

INTRODUCTION FOR PUBLIC HEARING LEGISLATIVE

Resolution No. 23-20

**2023 Coordinated Human Services
Transportation Plan**

(Pages _ - _)

INTRODUCTION FOR PUBLIC HEARING

MATANUSKA-SUSITNA BOROUGH INFORMATION MEMORANDUM

IM No. 23-149

SUBJECT: AMENDING MSB 15.24.030(B)(44) AND ADOPTING THE 2023 COORDINATED HUMAN SERVICES TRANSPORTATION PLAN.

AGENDA OF: September 5, 2023

ASSEMBLY ACTION:

AGENDA ACTION REQUESTED: Introduce and set for public hearing.

Route To	Signatures
Originator	<u> X </u>
Department Director	<u> X </u>
Finance Director	<u> X </u>
Borough Attorney	<u> X </u>
Borough Manager	<u> X </u>
Borough Clerk	<u> X </u>

ATTACHMENT (S) : Fiscal Note

Planning Commission Resolution 23-20 (4 pp)

Transportation Advisory Board Resolution 23-05
(3 pp)

2023 Coordinated Human Services Transportation Plan
Executive Summary (5 pp)

Public Comment Summary (5 pp)

2023 Coordinated Human Services Transportation Plan
(114 pp)

Ordinance Serial No. 23-074 (2 pp)

SUMMARY STATEMENT:

The 2023 Coordinated Human Services Transportation Plan (CHSTP) is an update to the 2018-2023 CHSTP, adopted in November of 2018. The plan evaluates existing transportation services for priority populations, including older adults, youth, indigenous

populations, veterans, individuals with disabilities, individuals living in poverty, and individuals with limited English proficiency.

Historically, research indicates that priority populations have higher rates of transit dependency and lower access to vehicles. The 2020 U.S. Census identified a higher rate of priority populations in the Matanuska-Susitna Borough (MSB) compared to other coordinated planning regions, underscoring the need for improved transportation services and provider coordination.

Federal Transit Administration (FTA) Circular 5310 funding through the Fixing America's Surface Transportation (FAST) Act and Alaska Mental Health Trust (AMHT) funding through the Alaska Department of Transportation and Public Facilities (ADOT&PF) focuses on the transportation needs of disadvantaged persons and those with special transportation needs that cannot be met through traditional personal automobile or public transportation means. For local providers to be eligible for FTA 5310 or AMHT funding, projects must come from a locally developed, coordinated plan that is updated at least every five years. The 2023 CHSTP is the second update to the MSB's plan; the most recent was adopted in 2018, five years ago. An update to the plan ensures continued funding for local transit and health and human services providers who offer transportation options and access to services for priority populations.

The project team gathered feedback through stakeholder interviews, advisory committee meetings, and an in-person workshop involving transit providers, health and human service organizations, and local, regional, and state agencies and organizations. The draft plan went out for a 45-day public comment period, and updates were incorporated into the final plan where appropriate.

The plan includes a prioritized list of goals and strategy recommendations created by the project advisory committee. This list provides clear steps to implement future improvements that effectively meet the transportation needs of priority populations throughout the Borough.

RECOMMENDATION OF ADMINISTRATION: Staff recommends adoption of the 2023 Coordinated Human Services Transportation Plan.

MATANUSKA-SUSITNA BOROUGH

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FISCAL NOTE

Agenda Date: September 5, 2023

SUBJECT: AMENDING MSB 15.24.030 (B) (44) AND ADOPTING THE 2023 COORDINATED HUMAN SERVICES TRANSPORTATION PLAN.

FISCAL ACTION (TO BE COMPLETED BY FINANCE)	FISCAL IMPACT YES NO
AMOUNT REQUESTED	FUNDING SOURCE
FROM ACCOUNT #	PROJECT
TO ACCOUNT :	PROJECT #
VERIFIED BY: X	CERTIFIED BY:
DATE:	DATE:

EXPENDITURES/REVENUES:

(Thousands of Dollars)

OPERATING	FY2023	FY2024	FY2025	FY2026	FY2027	FY2028
Personnel Services						
Travel						
Contractual						
Supplies						
Equipment						
Land/Structures						
Grants, Claims						
Miscellaneous						
TOTAL OPERATING						

CAPITAL						
---------	--	--	--	--	--	--

REVENUE						
---------	--	--	--	--	--	--

FUNDING:

(Thousands of Dollars)

General Fund						
State/Federal Funds						
Other						
TOTAL						

POSITIONS:

Full-Time						
Part-Time						
Temporary						

ANALYSIS: (Attach a separate page if necessary)

PREPARED BY: _____ PHONE: _____

DEPARTMENT: _____ DATE: _____

APPROVED BY: X _____ DATE: _____

Comptroller

IM No. 23-149
Ordinance Serial No. 23-074

**MATANUSKA-SUSITNA BOROUGH
TRANSPORTATION ADVISORY BOARD
RESOLUTION SERIAL NO. TAB 23-05**

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH TRANSPORTATION ADVISORY BOARD RECOMMENDING THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY ADOPTION OF THE 2023 COORDINATED HUMAN SERVICES TRANSPORTATION PLAN UPDATE.

WHEREAS, the Matanuska-Susitna Borough Transportation Advisory Board advises the Assembly on transportation-related issues; and

WHEREAS, the primary focus of the Matanuska-Susitna Borough's Coordinated Human Services Transportation Plan (CHSTP) Update is to evaluate and improve transportation options and access to services for priority populations such as older adults, youth, indigenous populations, veterans, individuals with disabilities, individuals living in poverty, and individuals with limited English proficiency; and

WHEREAS, the Borough-wide Comprehensive Plan identifies completing functional plans that address each mode of transportation as a transportation priority; and

WHEREAS, historically, research indicates that priority populations have higher rates of transit dependency and lower access to vehicles; and

WHEREAS, the 2020 U.S. Census identified a higher rate of priority populations in the Matanuska-Susitna Borough (MSB) when compared to other coordinated planning regions, underscoring the need for improved transportation services and provider coordination; and

WHEREAS, the most recent update to the MSB's CHSTP was adopted in 2018; and

WHEREAS, Federal Transit Administration (FTA) Circular 5310 funding through the Fixing America's Surface Transportation (FAST) Act and Alaska Mental Health Trust (AMHT) funding through the Alaska Department of Transportation and Public Facilities (ADOT&PF) Alaska Community Transit office focus on the transportation needs of disadvantaged persons and those with special transportation needs that cannot be met through traditional personal automobile or public transportation means; and

WHEREAS, in order for local providers to be eligible for FTA 5310 or AMHT funding, projects must be derived from a locally developed, coordinated plan that is updated at least every five years; and

WHEREAS, the ADOT&PF must certify to FTA that a CHSTP includes all of the following requirements:

1. Be locally developed, evidenced by public participation that must include seniors, individuals with disabilities, representatives of public, private, nonprofit, and human services transportation providers, and other members of the public.
2. Include information on the community background, an inventory of local resources and services, a needs assessment, gaps in service, strategies, priority of

projects, and a signature page of participating agencies.

3. A resolution from the local governing body adopting the plan.

4. Must be updated every five years.

WHEREAS, the CHSTP Update gathered feedback from interviews, advisory committee meetings, and a workshop, all involving transit providers, human service organizations, and local, regional, and state agencies and organizations, and also included a 30 day public comment period; and

WHEREAS, the CHSTP Update includes a list of prioritized goals and strategy recommendations to implement future improvements that effectively meet the transportation needs of priority populations throughout the Borough.


NOW, THEREFORE, BE IT RESOLVED, the Transportation Advisory Board recommends Assembly adoption of the 2023 Coordinated Human Services Transportation Plan Update.

ADOPTED by the Matanuska-Susitna Borough Transportation Advisory Board this 23rd day of June 2023.



Joshua Cross, Chair

ATTEST:



Kim Sollien, Planning Services Manager
Staff Support

Executive Summary

The primary focus of the Matanuska-Susitna Borough's Coordinated Human Services Transportation Plan Update is improving transportation options and access to services for the following target population groups:



Older adults
(aged 65 and older)



Individuals with
Disabilities



Youth
(ages 10 to 17)



Individuals living in
poverty



Indigenous Populations



Veterans



Individuals with limited
English proficiency

Historically, all of these population groups have higher rates of transit dependency and lower access to personal vehicles.

Stakeholder Engagement

As part of the MSB Coordinated Plan's stakeholder outreach and engagement process, the project team conducted a series of stakeholder interviews between the months of July and August of 2022. Stakeholders interviewed for this task included transit providers, human service organizations, and local, regional, and state agencies and organizations. The key takeaways were themed under the following topics:

- Coordination
- COVID-19 Pandemic Impacts
- Service Needs & Gaps Planning
- Priority Populations
- Funding
- Need for more Resources

Overall Themes

guiding development of goals and strategies



Coordination and
Collaboration



Access to Key Destinations



Regional Transportation
Needs



Education and Awareness



Funding



Safety



Data Collection



Affordability

Key Findings

Public transit does not adequately serve rural populations.

Low densities, large service areas, and extensive distances between activity centers complicate the delivery of public transit in rural areas of the Mat-Su Borough. Poor connectivity to regional hubs makes it difficult for residents to get their basic needs (e.g., medical care, education, shopping, and recreation) met. Opportunities exist to improve connections between rural and urban passenger travel via improved intermodal connections.

Funding remains a key barrier for transportation improvements.

There is limited dedicated funding in place to support the transit improvements needed to address the demands of a growing and aging population. Key funding sources are restrictive and different funding types may apply only to unique services for specific populations, can be used for limited purposes, or are restricted to a defined region (urban vs rural); reimbursements for non-emergency rides through Medicaid are often delayed, impacting providers. Lack of coordination between providers can also result in duplicative services and under capacity vehicles being under funded.

ng under funded.

There is a desire to improve coordination of transportation services between transit and human service providers.

Due to limited availability of federal and state funding, it is in the best interest of transit and human service providers to coordinate transit programs and services to make the most efficient use of existing resources and to avoid duplicative efforts. The statewide long-range plan and policy references the desire to coordinate at broader scopes, stating that there is “higher demand for specialized transportation such as human service transportation, public transit, and other alternatives in various regions.” The next step is to encourage coordination at the regional level.

Several elements went into this Existing Conditions: State of the Region Report, including a demographic analysis, plan review, stakeholder interviews, a provider inventory, and a needs and gaps analysis. gaps analysis.

Lack of support to implement transportation solutions.

Several plans have been developed over the years with solutions to regional needs and growth. Transit options have yet to be implemented for various reasons, but the lack of political will is a significant factor. Nationwide, local and regional governments often support public transportation or run their own transit operations. The Mat-Su Borough has not considered this, adding additional burden to non-profit organizations working to provide affordable and reliable transportation options. Additionally, land use and development have yet to be guided in a way that plans for transit infrastructure, such as bus stops, or allows easy access to commercial or medical districts, employment, or government services.

Mat-Su Borough has a higher rate of marginalized populations than other coordinated planning regions.

The individual target demographics this plan is designed for comprise up to 29% of the local population; however, when added together, the percent of the population in the Borough that is socially or politically marginalized is much greater (for example, youth and Veterans alone make up 46% of the total population, not to mention the others). With such large numbers in need, it truly underscores the urgency for more transportation services and further coordination amongst providers.

Plan Implementation

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Below is a comprehensive list of all six goals developed by providers throughout the engagement process. The proposed strategies offer clear and actionable steps forward in improving transportation accessibility for priority populations throughout the Borough.

After listening to feedback from both the Advisory Committee and stakeholders, the proposed strategies in this plan are prioritized by placement as Low, Medium, or High priority. Implementation timelines are associated with each strategy as well. Some strategies may be ready for immediate implementation, whether ranked high or low priority. Other strategies, while ranked “high priority” may take longer to implement. Project timelines range from short (1-2 years) to medium (3-4 years) to long (5+ years).

	Strategy	Timeline	Priority	Overall Ranking
Goal 1	Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough			
1.a	Design & Develop a Travel Training Program	1-2 years	Low	7
1.b	Engagement Planning for Local Governments	1-2 years	High	2
1.c	Borough Listening Sessions/Townhalls	1-2 years	Low	5
1.d	Establish a Formal Marketing Campaign on Transportation Resources in the Mat-Su Borough	1-2 years	High	1
1.e	Develop Consistent Online Resources	3-4 years	Medium	4
1.f	Support Access to Existing Community Services by Hosting Informational Webinars, Meetings, and Providing Leave-behind Materials Informing the Community How to Use Public Transportation	1 year or less	Medium	3
1.g	Develop Educational Materials on all Mobility Options in the Region (not only public transportation)	1-2 years	Low	6

	Strategy	Timeline	Priority	Overall Ranking
Goal 2		Strengthen and Sustain Financial Opportunities		
2.a	Increase Resources for Local Match	3-4 years	High	2
2.b	Develop a System to Identify and Promote Funding Opportunities for Regional Providers and Programs	1-2 years	High	1
2.c	Grant Writing Assistance Program	3-4 years	Low	4
2.d	Continue to Support the Mat-Su Borough's Development of a Regional Metropolitan Planning Organization	3-4 years	Medium	3
Goal 3		Establish a Data Collection and Management Plan to Inform Future Planning Efforts		
3.a	Expand and Utilize Current Data Sharing Plan	1-2 years	Low	3
3.b	Analyze Travel Patterns and Regional Demographics to Better Understand Gaps in Service Areas	3-4 years	High	1
3.c	Develop a Regional Data Management Plan	3-4 years	Medium	2
Goal 4		Define and Address Regional Transportation Needs		
4.a	Develop a Borough-wide Transit Development Plan	1-2 years	High	1
4.b	Mat-Su Borough Leverages Agency Leadership to Emphasize Transit Needs	1-2 years	Medium	2
4.c	Identify "Need" to Determine if the Need Can be Fulfilled by Existing Service or Whether the "Need" Requires New Service Through Formula 5310 Funding	3-4 years	Medium	4
4.d	Develop Driver Training and Retention Programs	3-4 years	Low	3

	Strategy	Timeline	Priority	Overall Ranking
Goal 5	Support Ongoing Coordination and Collaboration, While Creating New Partnerships			
5.a	Implement Borough-wide Mobility Management Program	3-4 years	Medium	3
5.b	Formalize Agreements and Processes for Leveraging Funding Services and Planning	1-2 years	High	2
5.c	Develop Borough-wide Coordinating Committee	1-2 years	High	1
5.d	Develop Partnerships for Non-profit Agencies Who May Need Support with Vehicle Maintenance	5+ years	Low	5
5.e	Coordinate with Critical Health and Social Services to Better Provide Consistent Transportation for Those Who Rely on the Service	3-4 years	Low	4
Goal 6	Design Safe, Accessible, and Affordable Services for Borough Residents			
6.a	Develop a Program for Discounted Fares for Older Adults and Individuals with Disabilities	1-2 years	High	1
6.b	Upgrade Facilities at Bus Stops and Transfer Stations	5+ years	Medium	4
6.c	Further Identify Public Transportation Infrastructure Needs in the Borough	3-4 years	High	2
6.d	Work with Alaska DOT&PF to Support the Borough's Level of Autonomy over Road Clearing During Winter Months	1-2 years	Low	3
6.e	Develop and Support Borough-wide Technology Measures for Customers and Providers	3-4 years	Unranked	-

Public Comment	Notes
<p>I attended the meeting this week and found the information very helpful. What I would suggest is that you have some regular citizens from each participating community involved in this initial planning phase and moving forward through various committees that you deem necessary. I think that it would validity to the process because all I saw were staff, consultants or elected officials. The reason that I think it's vital because it's obvious that this is going to financially impact all homeowners and commercial property owners. I've found from past experience that having citizen involvement right from the beginning is beneficial. Thank you for keeping the public informed.</p>	<p>Attended MPO Meeting, not CHSTP</p>
<p>Hello.</p> <p>As part of the "formal marketing campaign," I'd like to see straightforward information about specific politicians and political parties and their support or lack of support for this project.</p> <p>There is a HUGE disconnect between what residents want and how they vote. Someone needs to point that out. We can't get what we want if we continue to vote for representatives who don't support our wants and needs.</p> <p>I'm not convinced people even understand what government is or does.</p> <p>Education is vital.</p> <p>Thanks for listening/reading and trying. Your patience is admirable.</p>	
<p>I think this is a good move ! Bravo Almost 75</p>	
<p>My husband and I are aging and want to spend the rest of our lives on our homestead. It is a concern of mine driving to town the older we get. Chickaloon has the CAT service but GV does not. We should have a reliable transportation service too.</p>	
<p>Thank you for the opportunity to comment on the Coordinated Human Services Transportation Plan update. I am in support of the draft plan. Specifically, my family has three of the target population groups. The rural focus is essential in holistic work that will impact our Borough for decades to come. I am a beneficiary of Sunshine Transit's operations. I offer that the integration of the Bike and Pedestrian Plan is critical. Our local library is "across the street" from the walking route. There is no caution on the road (45 mph posted, with vehicles often traveling at greater speeds) as pedestrians and bicyclists attempt to cross to get to the library, across the culvert, then across the road as there is no interconnecting path and no signage or road markings. This was an oversight which may have fatal implications. Additionally, transit options are interwoven in the access to medical appointments; patients have challenges getting to see their providers for preventive care.</p>	<p>Reference to Bike & Ped, shared with KA</p>
<p>Public transit doesn't come north. It stops in Wasilla. That's why we have sunshine transit. So I don't know how this would effect Willow or even Caswell residents. Even with their demographics you can see Willow, Caswell and Talkeetna are in need. But it seems per the norm that we are not counted as part of the Mat Su.</p>	
<p>I live in Fishhook on Edgerton Parks Rd. I have lived here for nearly 50 years & have been involved with the Fishhook Community Council FHCC for the last 20 years. I am not speaking for the FHCC only as a long time resident of Fishhook. Needless to say I have seen increased housing, traffic & recreational development in the area during the last 50 years. People move to Fishhook for different reasons but regardless of the reason the fact is the Fishhook Community Council area one of the fastest growing communities in the Mat Su Borough. Most people who live here enjoy the rural nature & larger lots available. Recently, increased recreational opportunities have spurred increased visitations & interest in the area. We now have four recreation areas - GPRA, Skeetawk, the Moose Range & Hatcher Pass within our community council boundaries. With the upcoming traffic generated by the 2024 Arctic Winter Games & the increased traffic to recreational areas in the area, a public transit system to the Fishhook would help mitigate recreational traffic as well as help those people who need help with transportation.</p>	
<p>Would like to see something done about commuter train to Anchorage.</p>	

Provider/Agency Comments	Notes
I'd certainly like fixed routes to be considered. I understand that if fixed routes are used that the Paratransit Service needs to be included. I believe Valley Transit and some of the other providers have this kind of vehicle to supplement the fixed routes.	
Estimated A Cab provides 100 rides/week for Medicaid reimbursement rides	Added A Cab reference; requested ridership from Alaska Cab Valley without response
<p>I am glad to see this happening as there is a great need in the Valley for transportation. While WASI has not been part of the planning we would like to ensure we are part of the future. We hear and see the need for senior transportation especially for those that cannot afford to pay the taxi and Uber prices. Funding is definitely a barrier to providing services.</p> <p>WASI Transportation provided:</p> <ul style="list-style-type: none"> •From July 1, 2020 to June 30, 2021- WASI Transportation provided 829 rides to 65 consumers. •From July 1, 2021 to June 30, 2022 - WASI Transportation provided 5,814 rides to 82 consumers. •From July 1, 2022 to March 31,2023 - WASI Transportation provided 7,120 rides to 146 consumers. (Total rides is not reflective of the full fiscal year. There are four months left in the fiscal year.) <p>Average Monthly Rides: 772 (X) 4 months = 3,088 additional projected rides for fiscal 2023</p> <p>7,120 Current Rides through March ending, + 3,088 Projected Rides over the next four months.</p>	Updated - sending 2023 info @ EO June
Please add the below information to the WASI transportation description. WASI's Transportation Program provides personalized, and affordable transportation for qualified seniors and disabled adults. Individuals can schedule essential transportation for doctor appointments, grocery shopping, and prescription pick-up or drop-off on a suggested donation basis or schedule non-essential transportation on a private fare basis. WASI is a Medicaid Choice Waiver and TriWest Healthcare Alliance provider for individuals 60 years of age or older, veterans, have a disability, or live with a senior that is 60 years of age or older.	Updated
There is limited dedicated funding in place to support the transit improvements needed to address the demands of a growing population experiencing demographic changes. Key funding sources are restrictive (both state and local funding sources); they can only be applied to services for specific populations and for specific purposes. As a result, under-capacity vehicles from different providers may travel the same route at the same time but are barred from picking up additional riders.- I dont believe this is exactly accurate or gives a real sense of the issue.	Updated

There are a variety of reasons as to why transportation options have yet to be implemented, but it appears that lack of cooperation and/or political will is “hamstringing” the process. Additionally, the Anchorage Metropolitan Statistical Area and Mat-Su Valley struggles with public trust issues in local government, meaning that even if a plan were to go out for vote, the support for implementation may not exist.- I think this may need to be explained more. I am confused of what is it is referring to and I think this type of accusation/statement shouldn't be made if not clear.	Updated - Several plans have been developed over the years with solutions to regional needs and growth. Transit options have yet to be implemented for various reasons, but the lack of political will is a significant factor. Nationwide, local and regional governments often support public transportation or run their own transit operations. The Mat-Su Borough has not considered this, adding additional burden to non-profit organizations working to provide affordable and reliable transportation options. Additionally, land use and development have yet to be guided in a way that plans for transit infrastructure, such as bus stops, or allows easy access to commercial or medical districts, employment, or government services.
The previous lead agency for coordinated planning efforts was the Mat-Su Health Foundation.- I thought the last time was the MSB was still the “lead agency”?	Updated - MSB & MSHF both lead agencies
Uses larger vehicles (40- to 60-foot articulated buses), allowing transit providers to move people quickly along major corridors.- no articulated buses in alaska. and not always used in other areas with commuter bus service either.	Updated - removed the reference to articulated buses
The closest fixed route system to the Mat-Su Valley is PeopleMover, offered through the Municipality of Anchorage.- This statement is untrue. Valley Transit has a fixed route within the Mat-Su connecting to Anchorage.	Commuter service is classified differently than regular "fixed route" - VT is considered a commuter service in this context; updated to say, "other than commuter route..."
In 2014, the State of Alaska mandated consolidation of Valley Transit with Mat-Su Community Transit (MASCOT), and Valley Transit has- Valley Mover with Mat-Su Community Transit (MASCOT) Regional commuter service connecting Big Lake, Mead Lakes, Wasilla, and Anchorage. – Delete Big Lake	Updated - Removed Big Lake
However, supply chain demands on bus vehicle production have vastly impacted transit agencies, and Valley Transit is no exception. The agency has been awaiting the arrival of new busses for their commuter service between the Valley and Anchorage that were slated to arrive fall of 2022; however, only some have arrived. Additionally, the agency has expressed a need to provide additional demand response services within the Borough, but do not have the resources (vehicles and drivers) to make more service work. The agency is in critical need for more funding for capital, more flexibility to plan for future needs, and for supply line issues to resolve quickly.- All the buses for fixed commuter service have been delivered since the beginning of the year. Also, we do not have an issue of lack of drivers. We have demand response service between Palmer and Wasilla that is able to provide more service at a lower cost. A fixed route between Wasilla and Palmer would be a waste of resources, with high costs and low ridership. That is the reason why there not a fixed route. I think it sort of explains that but should be expanded on.	Updated in VT's writeup
In 2018, the annual ridership for CATs was 2,500.- More ridership data should be provided, including more recent years.	The request for more current ridership data was requested but not provided citing Covid impacts
This one-call/one-click system may assign trips to the lowest-cost eligible provider and provide riders with flexible request and payment options to improve the rider's Complete Trip experience.- This is not happening. Should be updated.	Update on status of central dispatch project (p. 2-33)

Valley Transit currently works with the school district to provide some rides to schools,- I don't think we work with the school district. We would with Youth360 and provide many rides for that afterschool program and we have parents use demand response service to get their kids to and from school but not the school district directly.	Updated re: work w/ school district
Gaps in transportation service were a common theme. In particular, an identified need for some type of transit (fixed-route service) were identified as a need, particularly between Wasilla and Palmer. Stakeholders noted that many marginalized populations, including people with low incomes, reside in gaps between demand response and ADA paratransit service areas.- I would clarify that there was interest in fixed route. I think its irresponsible to say it is a "need" when the people saying it are unaware of the serviced currently offered and have no transit education to know the costs to provide this service, the regulation, the ridership data, etc. I think it actually echoes the need more marketing of current resources and implementation of travel training program.	Many stakeholders providing transportation services did express a need for fixed route. Strategies that focus on streamlined data collection or a Transit Development Plan could help inform needs and gaps more clearly in the future with data to support.
Better transportation to employment is a common need among stakeholders. Large employers lack sufficient transportation options for those without access to vehicles. In addition, there is a significant need for stronger connections between population centers; from Wasilla to Palmer, from the Mat-Su Valley to Anchorage, etc. Many people commute between the larger population areas for employment, but in the winter, those commutes become challenging and even dangerous.- I don't think data supports this. Again, I think it actually echoes the need more marketing of current resources and implementation of travel training program.	Marketing of current resources and implementation of a travel training program are definitely important, but there are still gaps in transportation to employment for priority populations and many stakeholders expressed this as an issue and need. Commuting from the Mat-Su Valley to a bus stop in Anchorage, for instance, still requires the rider to get from the bus stop to their place of employment and this comes with limitations, especially in inclement weather.
Stakeholders noted there are a lot of at-risk populations that reside within the gap between Valley Transit and Sunshine Transit service areas.- Can these areas where the gaps are be stated?	The plan identifies a high population of priority groups in rural areas of the Borough that are not covered by the Sunshine or CAT services, as is depicted in the population density maps of the Existing Conditions.
In other areas, "ride free" programs may be developed as a pilot with grant funding. In some cases, funding is provided with local sales tax and grant funding with municipalities eventually taking over long-term funding.- 5310 can fund this, why not mentioned? 5310 funding can also be match funding to transit agencies. Match and the issue of there being a lack of is brought up throughout but not noting how 5310 could help fulfill that growing need, is there a reason why it's not mentioned?	Updated to "grant funding, including FTA 5310 funds." Can mention that 5310 is also able to be used as match funding, permitted to match federal funds with federal funds, but not if the funds originate from DOT (no FTA to FTA)

I would not include strategy 2d, 4d, or 5d. I think it would be best and most efficient for Valley Transit to keep these in house and it would be effort and time exhausted with little to no benefit	First and foremost, when planning strategies for coordinated plans, all agencies are not involved in every strategy. It's an important point to make, because some strategies don't necessarily involve every agency, if that makes sense. The same is true for your plan. The strategies you're referencing were developed for other agencies, not Valley Transit, so I tried to document that as follows: 2d: Continue to Support the Mat-Su Borough's Development of a Regional Metropolitan Planning Organization (this strategy—also documented in the paragraph—is about general support around the MPO. Other than planning functions, the strategy itself has little to do with Valley Transit and your day to day operations. It's acknowledging your region is changing). 4d: Develop Driver Training and Retention Programs (This strategy is not in reference to Valley Transit and was added upon further documentation and need from Sunshine Transit). 5d: Develop Partnerships for those Non-profit Agencies who may Need Support with Vehicle Maintenance (Similarly, this strategy was added due to the great need from your regional partners. If you're wanting to help support the other agencies by offering vehicle maintenance onsite at VT for other agencies, that would be great, but that would be at your discretion).
Strategy 6a I think should come first	In reference to 6a coming first, it was ranked (by you and your fellow stakeholders) as "high", so it's already top priority. Once the plan gets approved, it will be up to you and the members of the advisory committee to vote on and determine which high priority strategy gets implemented first.



Socius Amica
LLC



Matanuska-Susitna Borough Coordinated Human Services Transportation Plan Update

2023

COORDINATED PLAN
Matanuska-Susitna Borough

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Executive Summary

The primary focus of the Matanuska-Susitna Borough's Coordinated Human Services Transportation Plan Update is improving transportation options and access to services for the following target population groups:



Older adults
(aged 65 and older)



Individuals with
Disabilities



Youth
(ages 10 to 17)



Individuals living in
poverty



Indigenous Populations



Veterans



Individuals with limited
English proficiency

Historically, all of these population groups have higher rates of transit dependency and lower access to personal vehicles.

Stakeholder Engagement

As part of the MSB Coordinated Plan's stakeholder outreach and engagement process, the project team conducted a series of stakeholder interviews between the months of July and August of 2022. Stakeholders interviewed for this task included transit providers, human service organizations, and local, regional, and state agencies and organizations. The key takeaways were themed under the following topics:

- Coordination
- COVID-19 Pandemic Impacts
- Service Needs & Gaps Planning
- Priority Populations
- Funding
- Need for more Resources

Overall Themes

guiding development of goals and strategies



Coordination and
Collaboration



Access to Key Destinations



Regional Transportation
Needs



Education and Awareness



Funding



Safety



Data Collection



Affordability

Key Findings

Public transit does not adequately serve rural populations.

Low densities, large service areas, and extensive distances between activity centers complicate the delivery of public transit in rural areas of the Mat-Su Borough. Poor connectivity to regional hubs makes it difficult for residents to get their basic needs (e.g., medical care, education, shopping, and recreation) met. Opportunities exist to improve connections between rural and urban passenger travel via improved intermodal connections.

Funding remains a key barrier for transportation improvements.

There is limited dedicated funding in place to support the transit improvements needed to address the demands of a growing and aging population. Key funding sources are restrictive and different funding types may apply only to unique services for specific populations, can be used for limited purposes, or are restricted to a defined region (urban vs rural); reimbursements for non-emergency rides through Medicaid are often delayed, impacting providers. Lack of coordination between providers can also result in duplicative services and under capacity vehicles being under funded.

There is a desire to improve coordination of transportation services between transit and human service providers.

Due to limited availability of federal and state funding, it is in the best interest of transit and human service providers to coordinate transit programs and services to make the most efficient use of existing resources and to avoid duplicative efforts. The statewide long-range plan and policy references the desire to coordinate at broader scopes, stating that there is “higher demand for specialized transportation such as human service transportation, public transit, and other alternatives in various regions.” The next step is to encourage coordination at the regional level.

Several elements went into this Existing Conditions: State of the Region Report, including a demographic analysis, plan review, stakeholder interviews, a provider inventory, and a needs and gaps analysis. gaps analysis.

Lack of support to implement transportation solutions.

Several plans have been developed over the years with solutions to regional needs and growth. Transit options have yet to be implemented for various reasons, but the lack of political will is a significant factor. Nationwide, local and regional governments often support public transportation or run their own transit operations. The Mat-Su Borough has not considered this, adding additional burden to non-profit organizations working to provide affordable and reliable transportation options. Additionally, land use and development have yet to be guided in a way that plans for transit infrastructure, such as bus stops, or allows easy access to commercial or medical districts, employment, or government services.

Mat-Su Borough has a higher rate of marginalized populations than other coordinated planning regions.

The individual target demographics this plan is designed for comprise up to 29% of the local population; however, when added together, the percent of the population in the Borough that is socially or politically marginalized is much greater (for example, youth and Veterans alone make up 46% of the total population, not to mention the others). With such large numbers in need, it truly underscores the urgency for more transportation services and further coordination amongst providers.

Plan Implementation

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Below is a comprehensive list of all six goals developed by providers throughout the engagement process. The proposed strategies offer clear and actionable steps forward in improving transportation accessibility for priority populations throughout the Borough.

After listening to feedback from both the Advisory Committee and stakeholders, the proposed strategies in this plan are prioritized by placement as Low, Medium, or High priority. Implementation timelines are associated with each strategy as well. Some strategies may be ready for immediate implementation, whether ranked high or low priority. Other strategies, while ranked “high priority” may take longer to implement. Project timelines range from short (1-2 years) to medium (3-4 years) to long (5+ years).

	Strategy	Timeline	Priority	Overall Ranking
Goal 1	Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough			
1.a	Design & Develop a Travel Training Program	1-2 years	Low	7
1.b	Engagement Planning for Local Governments	1-2 years	High	2
1.c	Borough Listening Sessions/Townhalls	1-2 years	Low	5
1.d	Establish a Formal Marketing Campaign on Transportation Resources in the Mat-Su Borough	1-2 years	High	1
1.e	Develop Consistent Online Resources	3-4 years	Medium	4
1.f	Support Access to Existing Community Services by Hosting Informational Webinars, Meetings, and Providing Leave-behind Materials Informing the Community How to Use Public Transportation	1 year or less	Medium	3
1.g	Develop Educational Materials on all Mobility Options in the Region (not only public transportation)	1-2 years	Low	6

	Strategy	Timeline	Priority	Overall Ranking
Goal 2 Strengthen and Sustain Financial Opportunities				
2.a	Increase Resources for Local Match	3-4 years	High	2
2.b	Develop a System to Identify and Promote Funding Opportunities for Regional Providers and Programs	1-2 years	High	1
2.c	Grant Writing Assistance Program	3-4 years	Low	4
2.d	Continue to Support the Mat-Su Borough's Development of a Regional Metropolitan Planning Organization	3-4 years	Medium	3
Goal 3 Establish a Data Collection and Management Plan to Inform Future Planning Efforts				
3.a	Expand and Utilize Current Data Sharing Plan	1-2 years	Low	3
3.b	Analyze Travel Patterns and Regional Demographics to Better Understand Gaps in Service Areas	3-4 years	High	1
3.c	Develop a Regional Data Management Plan	3-4 years	Medium	2
Goal 4 Define and Address Regional Transportation Needs				
4.a	Develop a Borough-wide Transit Development Plan	1-2 years	High	1
4.b	Mat-Su Borough Leverages Agency Leadership to Emphasize Transit Needs	1-2 years	Medium	2
4.c	Identify "Need" to Determine if the Need Can be Fulfilled by Existing Service or Whether the "Need" Requires New Service Through Formula 5310 Funding	3-4 years	Medium	4
4.d	Develop Driver Training and Retention Programs	3-4 years	Low	3

	Strategy	Timeline	Priority	Overall Ranking
Goal 5	Support Ongoing Coordination and Collaboration, While Creating New Partnerships			
5.a	Implement Borough-wide Mobility Management Program	3-4 years	Medium	3
5.b	Formalize Agreements and Processes for Leveraging Funding Services and Planning	1-2 years	High	2
5.c	Develop Borough-wide Coordinating Committee	1-2 years	High	1
5.d	Develop Partnerships for Non-profit Agencies Who May Need Support with Vehicle Maintenance	5+ years	Low	5
5.e	Coordinate with Critical Health and Social Services to Better Provide Consistent Transportation for Those Who Rely on the Service	3-4 years	Low	4
Goal 6	Design Safe, Accessible, and Affordable Services for Borough Residents			
6.a	Develop a Program for Discounted Fares for Older Adults and Individuals with Disabilities	1-2 years	High	1
6.b	Upgrade Facilities at Bus Stops and Transfer Stations	5+ years	Medium	4
6.c	Further Identify Public Transportation Infrastructure Needs in the Borough	3-4 years	High	2
6.d	Work with Alaska DOT&PF to Support the Borough's Level of Autonomy over Road Clearing During Winter Months	1-2 years	Low	3
6.e	Develop and Support Borough-wide Technology Measures for Customers and Providers	3-4 years	Unranked	-



Chapter 1.

Introduction & Background

This Coordinated Human Services Transportation Plan Update—or “Coordinated Plan”—aims to make transportation more seamless for older adults, individuals with disabilities, and other people facing mobility challenges in the Matanuska-Susitna Borough (MSB).

This chapter explains why this Coordinated Plan is important, who it serves, and ultimately sets the stage for subsequent chapters. It contains the following sections:

- **Why a Coordinated Plan?** This section explains why this plan is important.
- **Who does this Coordinated Plan serve?** This section lists the target population groups for this Coordinated Plan.
- **Plan Structure.** This section describes the overall structure of this plan.

Why a Coordinated Plan?

There is a common need to travel throughout the MSB in day-to-day life—whether that means getting to work, making it to a medical appointment on time, running errands, shopping for groceries, or visiting loved ones.

For many people, getting from point A to point B can be a major barrier to living life fully: older adults, people with disabilities, veterans, people with low incomes who may not be able to afford a car, youth, and people who speak limited English. (More information on target population groups is available in Chapter 3.)

This is especially true in rural areas of the Borough, where distances between destinations can be very long, weather can present challenges, and public transit is less feasible. Even when destinations are nearby, invisible barriers like city limits can push places out of reach for reasons that aren’t clear to most people. This is to say nothing of visible barriers like highways, railroads, and rivers that can have similar effects.

How can we address transportation needs and fill gaps for these target population groups? Ultimately, answering this question is the purpose of this Coordinated Plan.

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Key Terms

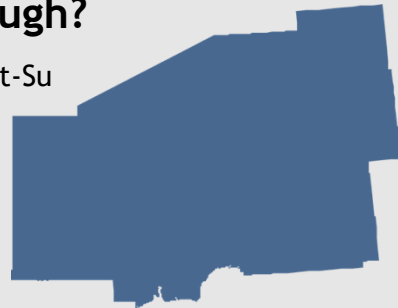
What is a Coordinated Plan?

This document is the second update of the Coordinated Plan for the MSB. Updates to coordinated plans must take place every five years.

Coordinated plans aim to improve transportation services for older adults, people with disabilities, and other marginalized populations. They are more formally known as coordinated public transit-human service transportation plans, and have a specific legal context at the federal, state, and regional levels.

What is the Matanuska-Susitna Borough?

The Matanuska-Susitna Borough, also known as the Mat-Su Borough, is located in south central Alaska. It is aptly named for both the Matanuska and Susitna rivers, which run through the Borough and empty into the Cook Inlet. It is one of the most rapidly growing areas in Alaska and is one of the few agricultural areas in the state. The seat of the Borough is the city of Palmer, though both the cities of Palmer and Wasilla have the highest population densities in the region. One of the unique demographic challenges the Borough experiences that is unlike most other regions is that the area has very high percentages of marginalized populations when compared to other places. This presents a great challenge, especially related to transit and coordination. Transit is a huge need in the MSB for these target populations; however, investment in transit and mobility is relatively limited. For more information on the target populations, see “who does this coordinated plan serve”, below.



What is the Advisory Committee?

The advisory committee for this project includes transit providers and major stakeholders in the planning and provision of transportation services. Most of the members on the advisory committee also participated in the 2018 plan update, allowing for consistency in planning and communication. The previous coordinated planning effort was led jointly by the Mat-Su Health Foundation and the Matanuska-Susitna Borough. Now that the area has experienced such rapid growth, the MSB is the current lead agency for coordinated planning efforts. More on regional growth and future planning around the U.S. Census may be found later in this plan.







Who Does this Plan Serve?

The primary focus of the MSB's Coordinated Human Services Transportation Plan Update is to improve transportation options and access to services for the following target population groups:




- Older adults (aged 65 and older)
- Youth (ages 10 to 17)
- Individuals who are:
 - Living with disabilities
 - Living in poverty
 - Limited in English proficiency
- Tribal nations
- Veterans

Historically, these population groups have higher rates of transit dependency and lower access to personal vehicles. As described in Figure 1-1, these conditions make mobility a challenge, particularly in rural areas and in locations without access to public transit services. The following sections provide a further look into the socioeconomic characteristics of the target populations within the study area, as well as a discussion of major trip generators and employers in the region.

Figure 1-1 Mobility Challenges of Target Populations

Target Population	Common Mobility Challenges
 <p>Older Adults (aged 65 and older)</p>	There are a variety of reasons older adults may drive less frequently or even at all, including health challenges, comfort behind the wheel, and the need to use or bring mobility devices. As such, older adults may need additional support for mobility, and transit can help meet that need.
 <p>Youth Populations (ages 10 to 17)</p>	Youth populations, particularly those younger than 18, may have issues accessing key destinations like schools, after school care, or community centers, due in part to the fact that many cannot yet drive themselves; however, some families may have only one or no vehicle at all. Further, families may not live in a location where they have direct access to public transportation services.
 <p>Individuals with a Disability</p>	Individuals with disabilities may have physical or cognitive challenges that make it difficult to operate a vehicle, or to travel on their own without assistance from others. Individuals with disabilities may need additional support for mobility from caregivers or family members.
 <p>Individuals Living in Poverty</p>	Individuals living in poverty tend to use transit more frequently than the general public because they may not have the financial ability to purchase, own, maintain, or fuel a personal vehicle. However, even public transportation services may be cost-prohibitive for these populations.

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Target Population	Common Mobility Challenges
 Indigenous Populations	<p>Indigenous populations commonly live on tribal lands, often located in more “rural” and isolated areas of a given region or state. Transportation is often more challenging for tribal areas because of difficult access, as well as the fact that many individuals often fall under other target categories (i.e., may have a disability, be an older adult, or medically frail).</p>
 Veterans	<p>Veterans often face several barriers to receiving care and may have financial challenges that make travel costs for healthcare appointments burdensome. Veterans living in rural areas must travel longer distances and may not have immediate access to healthcare providers or specialists. Further, many Veterans need to access the Veterans Administration and/or hospitals, which may be long distances away and have a limited number of appointments.</p>
 Limited English Speakers	<p>Limited English speakers may face additional challenges accessing and understanding available transportation programs, including public transit. The needs of this demographic group are important to consider improving access to services such as healthcare, grocery shopping, and job access.</p>


Two additional target populations this plan focuses on are **households with no vehicles** and **unhoused individuals**. Individuals and families with no vehicle have limited mobility options when there is no direct access to transit services. Without transit, these individuals must rely on rides from friends and family members. Similarly, unhoused and transitional populations often struggle with limited access to transit and often have limited means to pay for public transit services. These populations significantly benefit when transit services are designed to provide access to government services, employment, and food access.

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Transit Modes Included in this Coordinated Plan

For the purposes of this plan, two modes available in the MSB are included in the Coordinated Plan:

 <h3>Commuter/Express Bus Routes</h3> <p>Long distance service for passengers needing access to employment, education, medical, and shopping opportunities not otherwise available in their area.</p>	
Service Area: Operates from main transit centers or park-and-rides to designated stops within a given city.	Service Schedule: Fixed times, with limited stops.
<i>How does it operate?</i>	<i>Vehicles</i>
Service is more frequent during “peak commute periods,” with limited scheduled service during the middle of the day.	Uses larger vehicles allowing transit providers to move people quickly along major corridors.


 <h3>Demand Response</h3> <p>Demand response transit service is “demand-based,” operating based on the needs or schedules of the customers. It is the second largest type of public transit service in the U.S.</p>	
Service Area: Found in low-density areas or ones that are geographically widespread.	Service Schedule: Usually schedule-based, with customers scheduling 2-24 hours in advance, or subscription-based, with customers having a standing reservation to use the service.
<i>How does it operate?</i>	<i>Vehicles</i>
Some models utilize technology that allows for real-time scheduling, but most providers require reservations in advance.	Utilizes small or medium sized vehicles, such as minivans, passenger vans, or larger “cutaway” buses, typically equipped with wheelchair spaces and wheelchair lifts in order to service all passengers, no matter their abilities.


These modes represent just two out of a list of ten that are commonly represented in coordinated plans. The following section provides an overview of the other eight modes, which are not part of the MSB’s Coordinated Plan.

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What Modes of Transportation are Included in Other Coordinated Plans?

Coordinated plans include all modes of transportation available in a given region, including those that connect from other areas into the region. For individuals new to transit, the following provides a quick guide to eight additional modes of transportation outside the two available in the MSB.

 Passenger Rail 1 <p>Moves many customers over long distances, usually with high frequencies.</p>		
Service Area: Large urban areas	Service Schedule: Fixed times and stops	
<i>Sub-Types</i>	<i>Where Does it Operate?</i>	<i>Vehicles</i>
Heavy Rail	Older “legacy” city systems (Chicago and Boston)	Electric rail or diesel fuel trains
Commuter Rail	Cities with newer rail service, (Austin)	
Light Rail	San Francisco and Dallas	Overhead electric catenary system

 Bus Rapid Transit 2 <p>Implemented when fixed route bus service on a given corridor is overloaded.</p>		
Service Area: Major corridors in large cities.	Service Schedule: Fixed, with timed stops spaced to allow vehicles to move more quickly than traditional fixed routes.	
<i>How does it operate?</i>	<i>Vehicles</i>	
Cities often dedicate a traffic lane to BRT, with signal queuing to allow the service to operate competitively when compared to traditional fixed route	Uses larger vehicles (40- to 60-foot articulated buses), that allow transit providers to move people quickly along major corridors.	

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Fixed Route Bus

3

Buses operating on predetermined routes with set schedules and stops; the most common form of public transportation in the U.S.

Service Area: Typically found in urbanized areas, but also in rural areas where such service is better suited to a community.	Service Schedule: Formal, posted schedules and designated stops allow passengers to plan ahead on when and where to catch the bus. <i>Fixed-route bus service requires what the Federal Transit Administration calls “complementary paratransit” service, per the Americans with Disabilities Act (ADA). Complementary paratransit, also known more simply as “paratransit” service, is detailed in a dedicated type below.</i>
<i>How does it operate?</i>	<i>Vehicles</i>
Service is more frequent during “peak commute periods” with limited scheduled service during the middle of the day.	Typically utilizes buses ranging in size from 25-40 feet; however, vans and other smaller vehicles may be utilized depending on ridership.




Flex Route Bus


4

Also known as “deviated fixed-route,” this transit service operates on a scheduled fixed route where the bus may “deviate” off-route at the request and/or need of the customer(s).


Service Area: A good alternative for areas where fixed-route service may not be a good fit—for example, suburban and rural areas.	Service Schedule: Flexible routes are typically designed with enough “slack” in the schedule to allow for deviations, yet still allow the bus to run on time for scheduled stops. <i>Complementary paratransit is not required with flex-route service, because the vehicle may deviate off route based on customer needs.</i>
<i>How does it operate?</i>	<i>Vehicles</i>
Customers request real-time route deviations as needed.	Typically utilizes buses ranging in size from 25-40 feet; however, vans and other smaller vehicles may be utilized depending on ridership.


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	<h2>Paratransit Service 5</h2> <p>Paratransit (also known as complementary paratransit) service is designed to complement fixed-route transit services; the Americans with Disabilities Act (passed in 1990) requires that transit operators offering fixed-route services must offer “comparable” service to individuals with disabilities.</p>
<p>Service Area: Wherever an agency provides fixed route(s), it must also offer complementary paratransit within ¾ mile of the fixed route.</p>	<p>Service Schedule: Service must be provided during the same days and times as the fixed route service operation.</p>
<p><i>How does it operate?</i></p>	<p><i>Vehicles</i></p>
<p>This type of transit service may offer three main types of operations based on the policies of the provider and the needs of the customer: curb-to-curb, door-to-door, and door-through-door. Paratransit service is the costliest for a provider to offer and is offered by larger agencies in urbanized areas that provide fixed routes.</p>	<p>Paratransit service utilizes smaller vehicles (usually 25-foot “cutaway” buses) that have wheelchair lifts or ramps, with one or more spaces for wheelchairs where they can be safely ‘tied down.’</p>

	<h2>Ride Share/Transportation Network Companies (TNCs) 6</h2> <p>Ride share, also known as ride-hailing, is a form of transportation service that is a hybrid between demand-response and taxi service.</p>
<p>Service Area: TNCs are typically found in urbanized areas, though some may exist in rural settings.</p>	<p>Service Schedule: Passengers request service through mobile phone apps, usually on-demand from a specific pick-up point. However, service may also be scheduled in advance. Passengers may also request a private or shared ride, depending on timing and cost.</p>
<p><i>How does it operate?</i></p>	<p><i>Vehicles</i></p>
<p>Typically not offered by a public provider, but a series of private providers, referred to as transportation network companies (TNCs). Since rides are often offered by private companies, price escalation may be a significant factor in whether a customer chooses to book a ride through this service.</p>	<p>Typically offered in cars or SUVs; some larger transit providers offer ride sharing service that is pre-coordinated within the agency, utilizing an agency vehicle, such as a car or van.</p>

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 Volunteer Transportation Program 7	
Volunteer transportation operates a variety of ways, but centrally relies on volunteer drivers to drive passengers.	
Service Area: Volunteer transportation programs are great because they can be implemented in any setting: rural, urban, or suburban.	Service Schedule: These types of programs typically have rides scheduled in advance, though some may operate on-demand service, depending on driver availability.
<i>How does it operate?</i>	<i>Vehicles</i>
These programs are typically the lowest cost for agencies to offer; however, insurance and liability provide challenges to implementation. During the pandemic, volunteer driver programs came to a halt, and many have yet to recover.	These programs may offer their own vehicles (cars or vans) or may ask that the volunteer provide their own vehicle in exchange for cost reimbursements for fuel, mileage, and other costs.

 Carpool/Vanpool Programs 8	
Carpools and vanpools are another low-cost alternative to serve anywhere from 3-18 passengers.	
Service Area: These services are common in rural and suburban areas where a common group of individuals need to travel long distances, and where commuter transit is not a viable option.	Service Schedule: Carpool and vanpool programs are typically designed around work schedules, i.e. 8-5pm. Expenses for the rider vary, based on trip distance and frequency of use, though these options are often less expensive than driving alone.
<i>How does it operate?</i>	<i>Vehicles</i>
These programs may be offered through a transit service provider or may be more organic, established by a group of individuals who need service to common locations, such as an employer or education institution.	Some providers offer cars, minivans, or passenger vans for those signed up for the service, and those vehicles are usually left overnight at a common location, such as a shopping center or park and ride.

How Does this Coordinated Plan Fit into the Federal, State, and Regional Context?

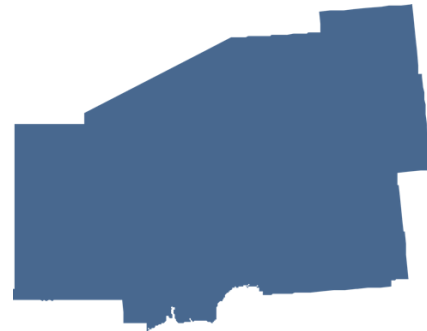
Federal

The Enhanced Mobility for Individuals and Individuals with Disabilities Program (Section 5310) is a federal source of transportation funding.

To receive funding under this program, projects must be part of a locally developed and approved coordinated public transit-human services plan—often simply called a coordinated plan.

Furthermore, coordinated plans must:

- Incorporate participation by older adults and individuals with disabilities, as well as other stakeholders, including representatives of public, private, and non-profit service providers.
- Be updated every five years—or every four years for areas that are in non-attainment.



Alaska Department of Transportation and Public Facilities (ADOT&PF) Context

According to the state's website, coordinated plans are required to have specific elements to meet FTA requirements. ADOT&PF does not approve a community's plan, only certifies to FTA all required elements are in the plan. Coordinated plans must:

1. Be locally developed. Evidenced by public participation that must include seniors, individuals with disabilities, representatives of public, private, non-profit and human services transportation providers, and other members of the public.
2. Include information on:
 - a. The community background;
 - b. Inventory of local resources and services;
 - c. Needs assessment;
 - d. Gaps in service;
 - e. Strategies;
 - f. Priority of Projects;
 - g. Signature page of participating agencies.
3. Resolution from local governing body adopting the plan
4. Must be updated every five years.

COORDINATED PLAN
Matanuska-Susitna Borough

Plan Structure

This Coordinated Plan includes seven chapters and one appendix. Chapters two through four take stock of existing conditions and lay the groundwork for the rest of the document. Chapter five presents updated regional goals and objectives. Chapters six and seven explain implementation strategies, timelines, and performance measures. Chapter eight looks ahead to future considerations.



Executive Summary

Summary of Coordinated Plan.

Chapter 1. Introduction

This chapter covers the Coordinated Plan’s background and purpose, populations served (and engaged), and plan structure.

Chapter 2. Transportation Resources in the Region

This chapter provides a list of current transportation providers and planning agencies in the MSB.

Chapter 3. Transportation Needs and Gaps

This chapter assesses the known transportation needs and gaps, with demographic maps and supporting geographic analysis.

Chapter 4. Review of Existing Plans, Studies, and Reports

This chapter describes how this Coordinated Plan aligns with other municipal, rural, and statewide transportation planning efforts.

Chapter 5. Goals and Strategies

This chapter articulates the goals and objectives (or “strategies”) of this Coordinated Plan.

Chapter 6. Plan Implementation and Funding Sources

This chapter prioritizes strategies, and proposes an implementation plan—including priority rankings, lead organization(s), and support organization(s)—to put into action when the Coordinated Plan is approved.

Chapter 7. Looking Ahead / Conclusions

This chapter includes annual reporting recommendations, public engagement planning, and future considerations related to census impacts and future transit plans.

Appendix



Chapter 2. Existing Conditions

Demographics

According to the American Community Survey (ACS), the MSB has 24,618 square miles of land and is the seventh largest borough in Alaska by size. The population of the Borough was 107,081 according to the most recent decennial census.

In Figure 2-1, these individual groups represent between 7% and 29% of the total population, respectively in the region, underscoring the need for transportation services that ensure that the needs of the region's most vulnerable groups are being met. It is important to point out, however, that though the individual groups comprise up to 29% of the local population, when added up, the percent of the population in the Borough that is socially or politically marginalized is much greater (for example, youth and Veterans alone make up 46% of the total population, not to mention the others). With such large numbers in need, it truly underscores the need for more transportation services and further coordination amongst the providers. The MSB is unique compared with other coordinated planning regions in that it has a higher percentage of the target populations than most other areas.

Figure 2-1 Target Populations

Population Group	2021	% of MSB Population
Older Adults (age 65+)	14,135	15%
Youth (age 10 to 17)	27,841	29%
Individuals with a Disability	13,921	14%
Individuals Living in Poverty	11,779	12%
Veterans	16,062	17%
American Indian / Alaska Native	6,844	7%
Limited English Speakers	6,746	7%

Sources: 2021 1-Year ACS Estimate; 2020 5-Year ACS Estimates

Data for the maps representing existing conditions comes from the American Community Survey (ACS) 5-year Estimates Selected Detailed Population Tables. The data was collected from 2016 - 2020 and released in March 2022. This is the most up-to-date data available by census tract. The ACS website explains that the boundaries (urban, rural, etc.) are based on the data collected for the 2010 census and may not "reflect the results of ongoing urbanization."

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Older Adults

Older adults account for 15% of the population of the study area. The share of older adults in the region is higher than that of the state (13%) as observed in Figure 2-2.

Figure 2-2 Older Adult Residents

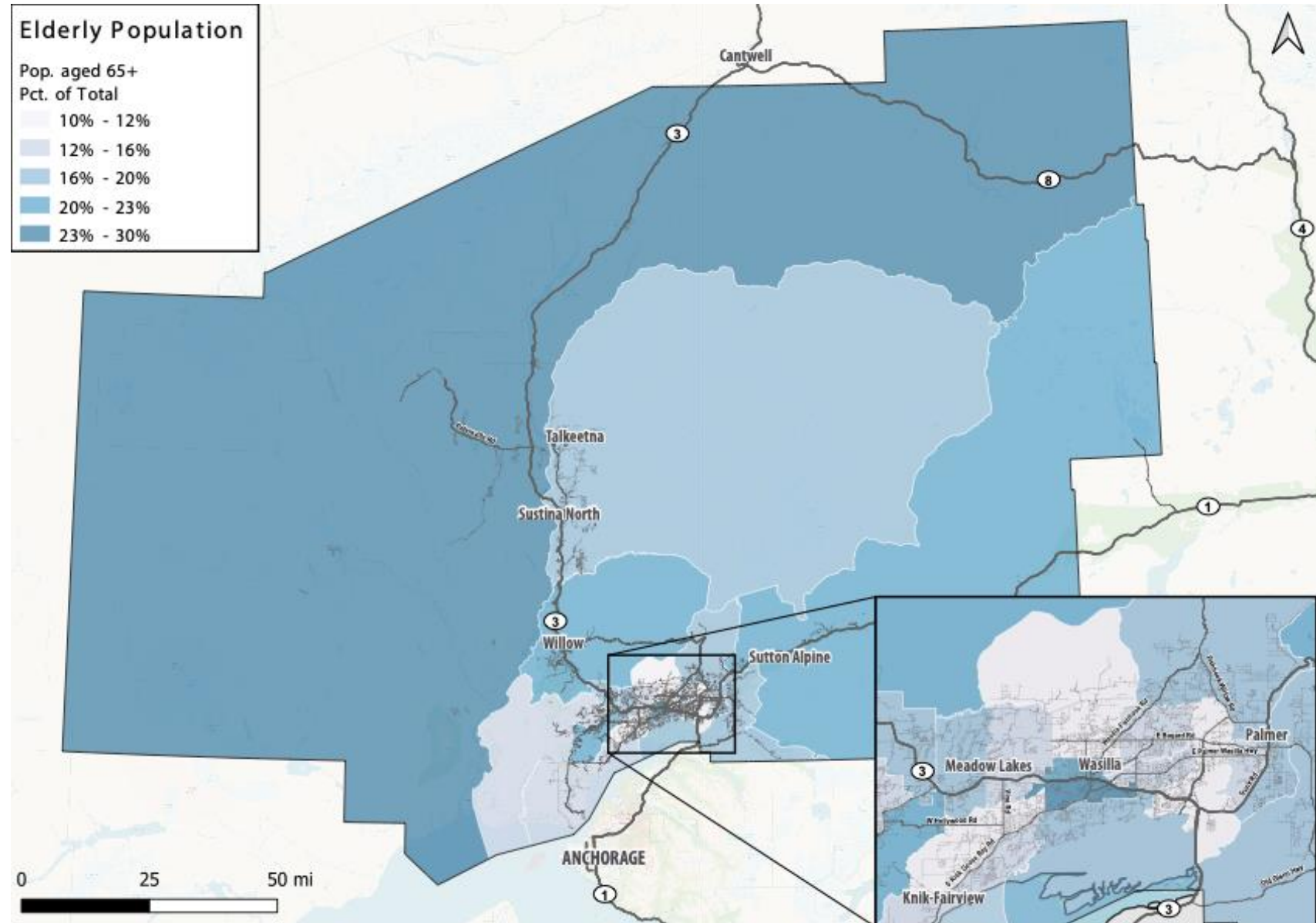
	2021	% of Population
Mat-Su Borough	14,135	15%
State of Alaska	95,341	13%

Source: 2021 1-year ACS estimates; 2020 5-year ACS estimates

The distribution of older adults in the region can be seen in Figure 2-3. Concentrations of older adults can be found south of the largest population center, Wasilla, and in the far more rural areas of the MSB to the west and north.

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Figure 2-3 Distribution of Residents Aged 65 or Older



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Youth

Individuals aged 10 to 17 years old account for 29% of the population of the study area. The share of the youth population is higher than the state (25%) as observed in Figure 2-4.

Figure 2-4 Youth Residents (Age 10 to 17)

	2021	% of Population
Mat-Su Borough	27,841	29%
State of Alaska	182,500	25%

Source: 2021 1-Year ACS Estimate

The distribution of youth in the region is shown in Figure 2-5. Concentrations of youth residents can be found in the largest population centers, primarily in and around Wasilla and Palmer. Smaller pockets with notable concentrations include Willow, Susitna North, and Talkeetna.

Youth Population

Pop. aged 10-19
Pct. of Total

- 10% or less
- 10 - 16%
- 16 - 21%
- 21 - 25%
- Greater than 25%

Map labels include: Cantwell, Talkeetna, Sustina North, Willow, Sutton Alpine, ANCHORAGE, Meadow Lakes, Wasilla, Palmer, Knik-Fairview.

Scale: 0, 25, 50 mi

COORDINATED PLAN
Matanuska-Susitna Borough



Individuals with Disabilities

Individuals with disabilities account for 14% of the population of the study area. The share of individuals with a disability is slightly higher than the population in the state (13%) as observed in Figure 2-6.

Figure 2-6 Individuals with a Disability

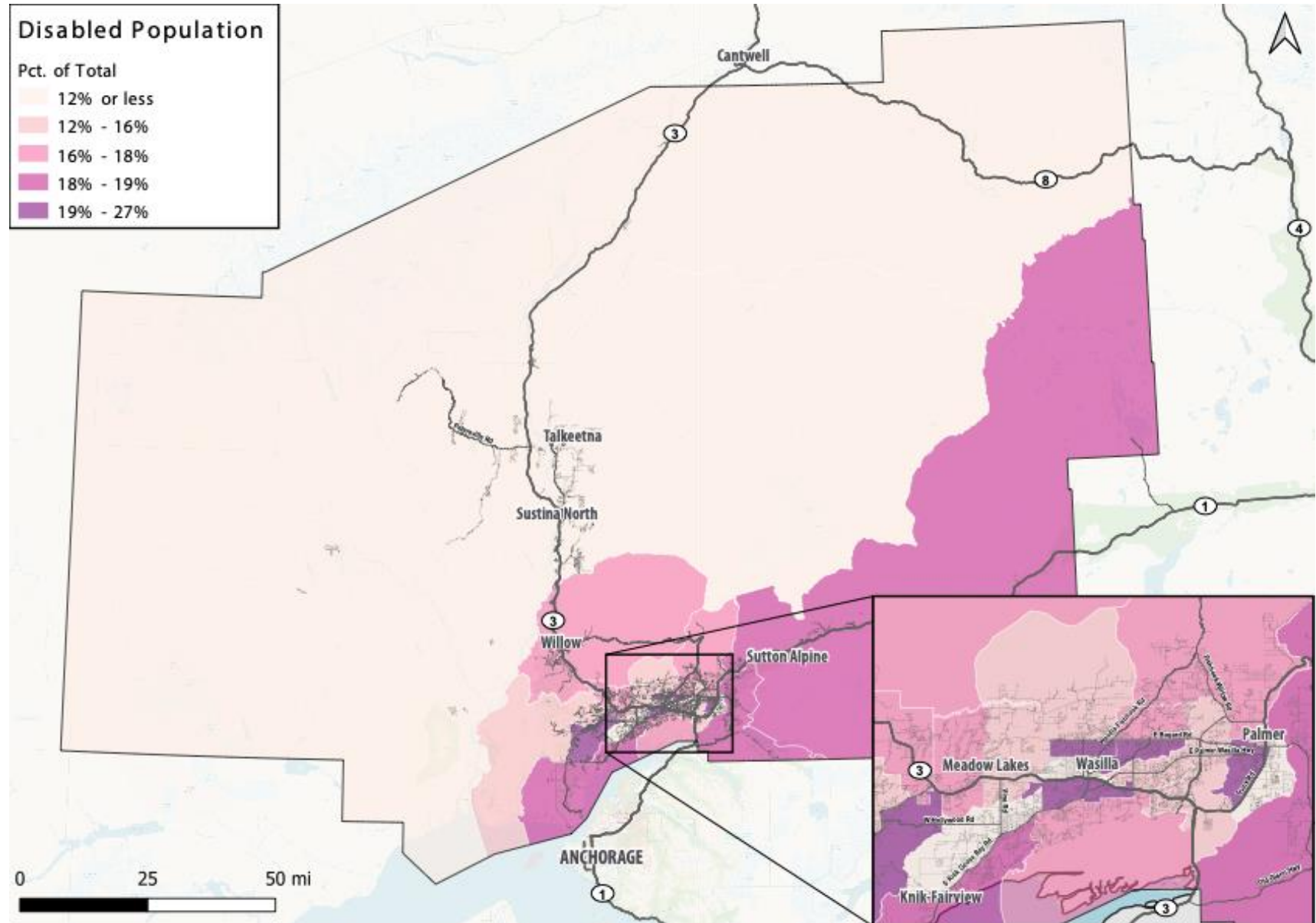
	2021	% of Population
Mat-Su Borough	13,921	14%
State of Alaska	95,340	13%

Source: 2021 5-Year ACS Estimates

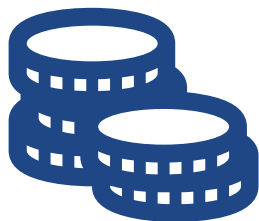
The distribution of individuals with disabilities in the study area is shown in Figure 2-7. Concentrations of individuals with disabilities are found around Wasilla, Palmer, and south of Meadow Lakes, however, there is a significant concentration of individuals with disabilities in the southeast area of the Borough, particularly Sutton Alpine.

COORDINATED PLAN
Matanuska-Susitna Borough

Figure 2-7 Population Density of Individuals with Disabilities



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Individuals Living in Poverty

Individuals living in poverty account for 12% of the population of the study area. The share of individuals living in poverty within the study area is higher than the share at the statewide level (10.5%) as observed in Figure 2-8.

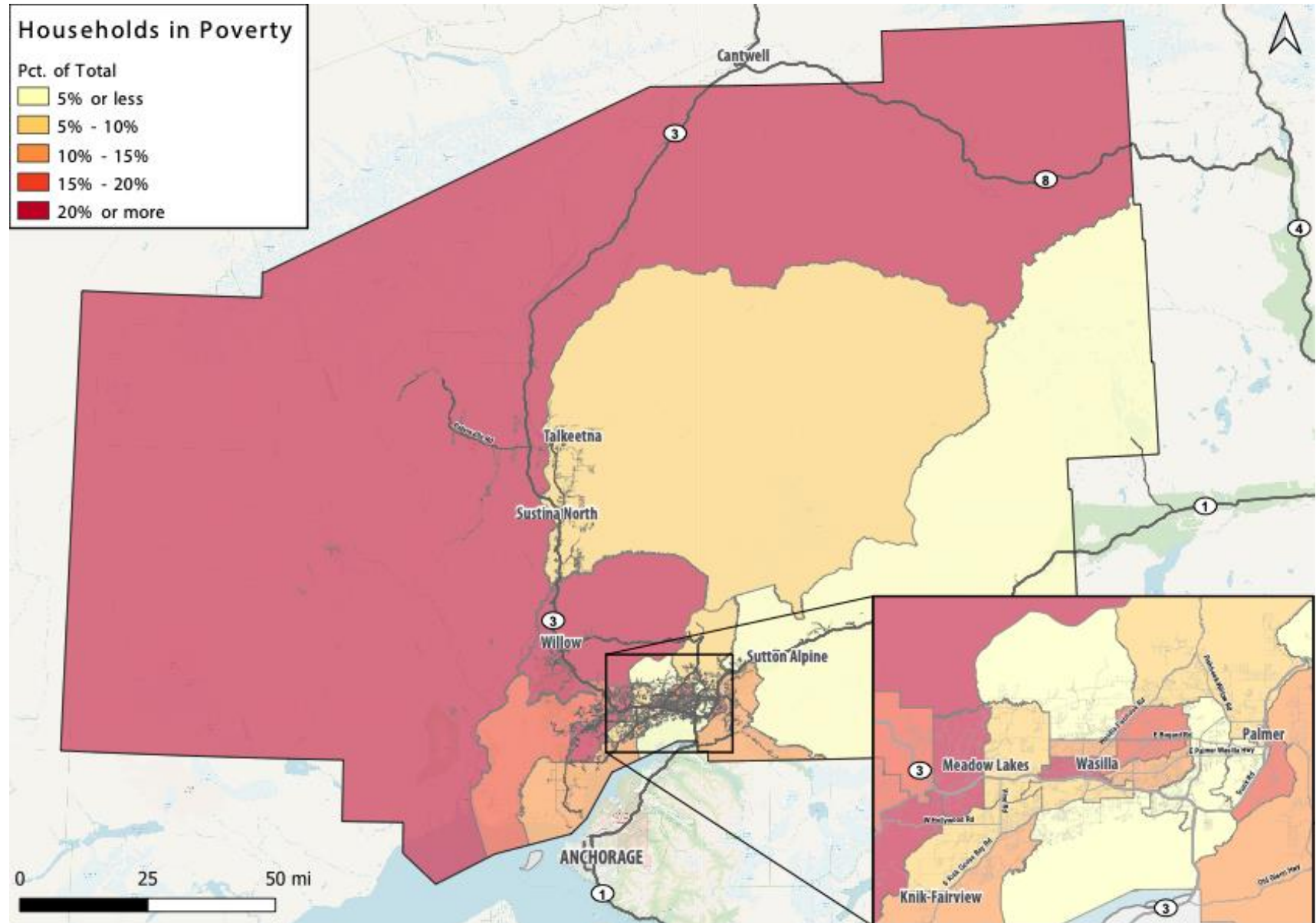
Figure 2-8 Individuals Living in Poverty

	2021	% of Population
Mat-Su Borough	11,779	12%
State of Alaska	77,006	10.5%

Source: 2021 5-Year ACS Estimates

COORDINATED PLAN
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Figure 2-9 Population Density of Individuals Living in Poverty



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Indigenous Populations

Indigenous people fall within the target population category for coordinated planning efforts. Often, tribal nations are located on lands that would otherwise be considered “rural” and are historically underserved by public transportation services. In many cases, indigenous people may spread across multiple target categories: Veteran, low-income, at-risk youth, older adult, and individuals with a disability. Therefore, it is critical to include indigenous populations in coordinated planning efforts.

Additionally, the FTA has recently announced additional funding for the Tribal Transit Program: on October 11, 2022, the FTA announced an award of over \$8 million in grants to 25 Tribal governments for projects that support transit services for American Indian Tribes and Alaska Native Villages, communities, or groups in rural areas.

American Indians/Alaska Natives account for 7% of the population of the study area. The share of individuals in the study area is lower than the share at the statewide level (16%) as observed in Figure 2-10.

Figure 2-10 American Indian / Alaska Native

	2021	% of Population
Mat-Su Borough	6,844	7%
State of Alaska	117,228	16%

Source: 2021 5-Year ACS Estimates

Indigenous Population

Pct. of Total

- 1% or less
- 1% - 12%
- 12% - 14%
- 14% - 20%
- 20% - 30%

0 25 50 mi

ANCHORAGE

Wasilla

Palmer

Meadow Lakes

Knik-Fairview

Sutton Alpine

Willow

Sustina/North

Talkeetna

Cantwell

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Veterans

Veterans account for 17% of the total population of the study area. The share of the veteran population in the study area is higher than the state average (11%) as observed in Figure 2-12. The areas with the highest share of this demographic group are to the west and north of the Borough, particularly in rural/low-density areas, in Meadow Lakes, and south of Wasilla.

Figure 2-12 Veteran Residents

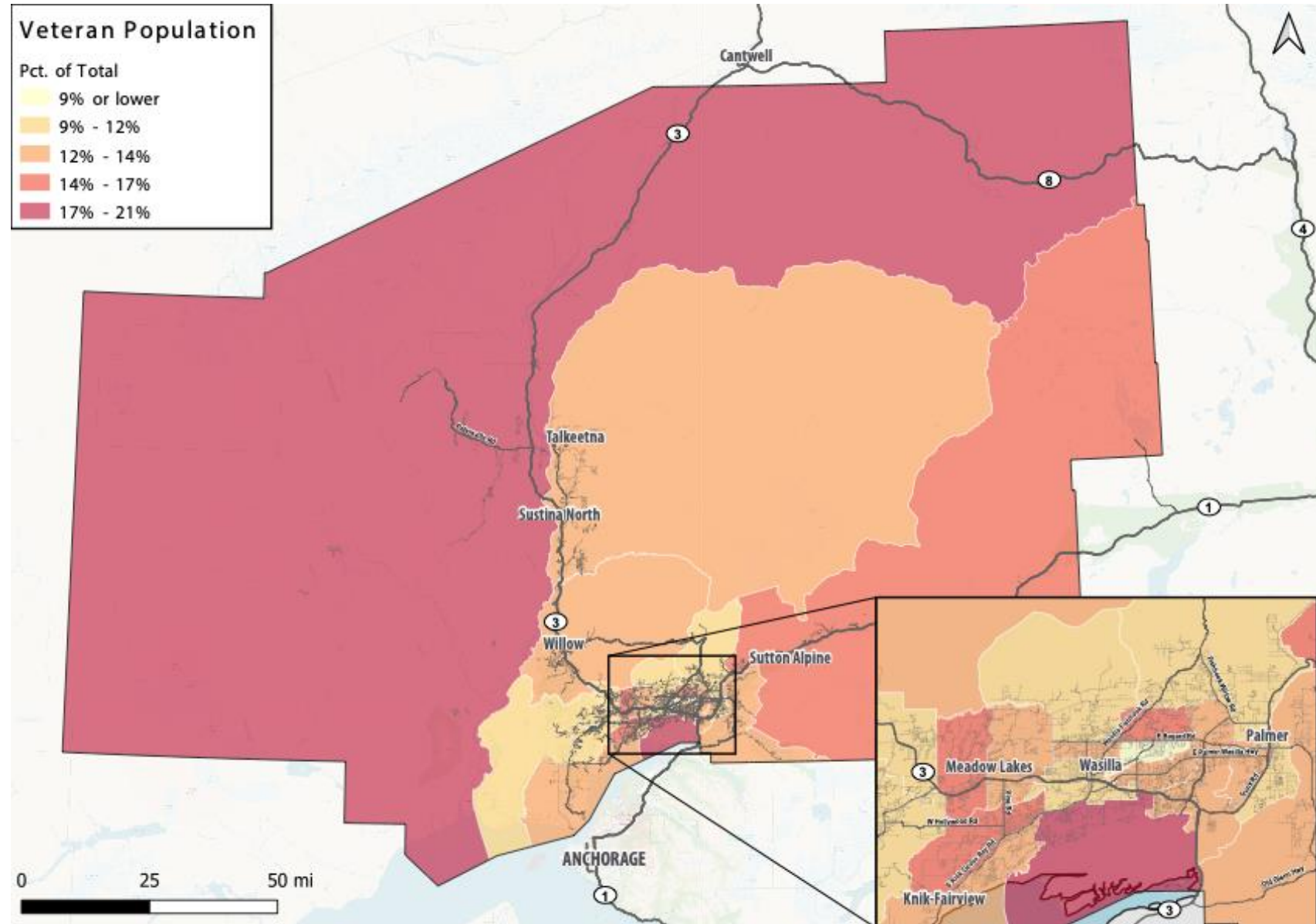
	2021	% of Population
Mat-Su Borough	16,062	17%
State of Alaska	80,673	11%

Source: 2021 5-Year ACS Estimates

The distribution of veterans in the study area is shown in Figure 2-13. Current Veterans Affairs (VA) facilities are located in Wasilla, (Mat-Su VA Clinic), Anchorage (Alaska VA Healthcare system), as well as Vet Centers in Wasilla and Anchorage.

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Figure 2-13 Veteran Population Density



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Limited English Speakers

Limited English speakers account for 7% of the population of the study area. The share of limited English speakers in the study area is slightly higher than the share at the statewide level (6%) as observed in Figure 2-14.

Figure 2-14 Limited English Speakers

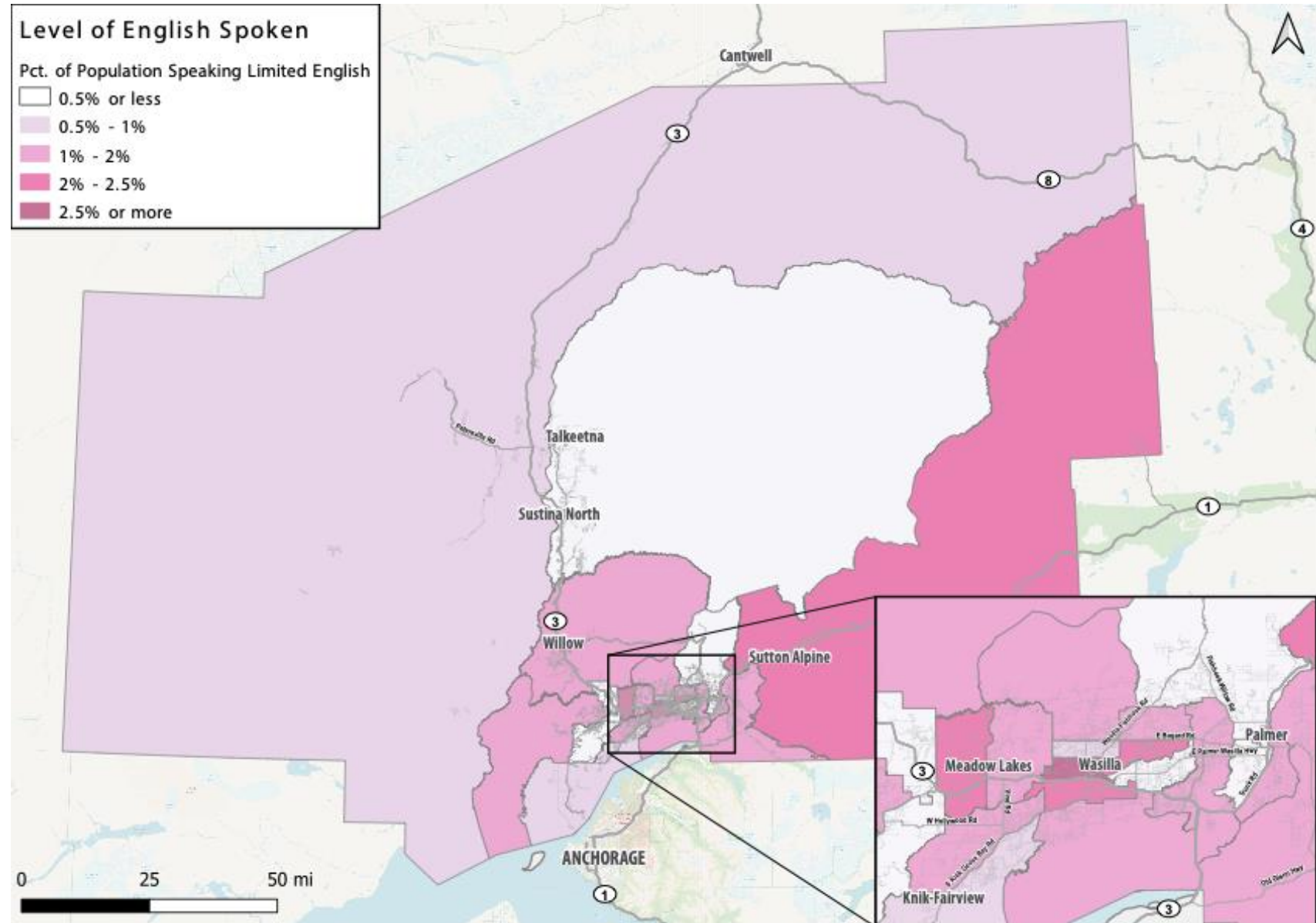
	2021	% of Population
Mat-Su Borough	6,746	7%
State of Alaska	44,003	6%

Source: 2021 5-Year ACS Estimates

The distribution of limited English speakers in the study area is shown in Figure 2-15.

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Matanuska-Susitna Borough

Figure 2-15 Density of Individuals with Limited English Proficiency



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Matanuska-Susitna Borough



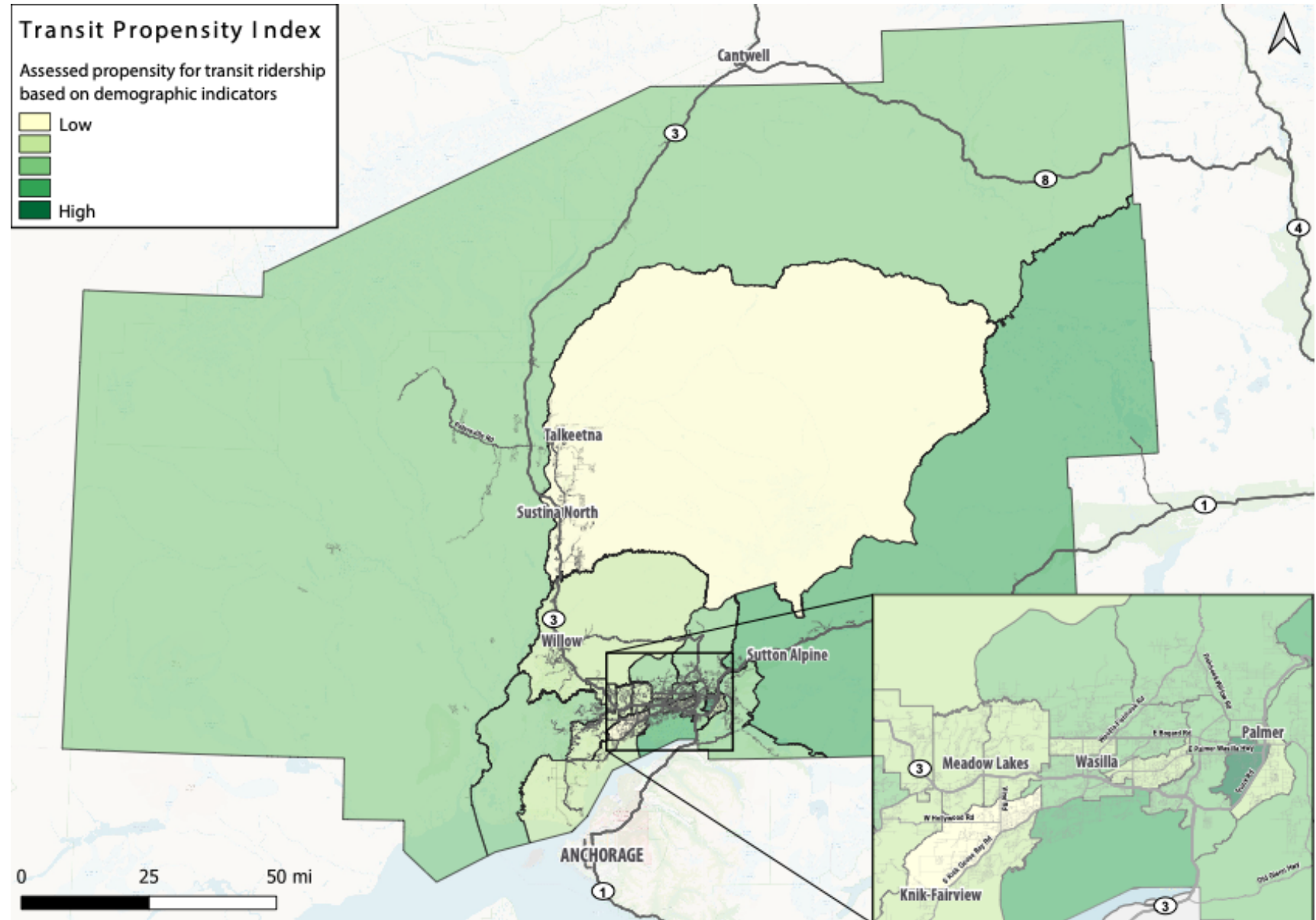
Transit Propensity Index

Transit propensity is a concept that seeks to identify potential (or likely) areas of increased transit need based on spatial geographic and socioeconomic factors. Figure 2-16 illustrates the locations within the study area that likely have the greatest need for public transportation services. The transit propensity analysis identifies the cumulative densities of demographic populations most often associated with high transit need, including older adults, individuals with disabilities, individuals living in poverty, and zero-vehicle households.

Transit need is often closely aligned with compact, urban areas—which commonly have the highest percentages of populations identifiable as ‘politically or socially marginalized.’ The MSB presents an exception to the usual profile because its marginalized populations—for whom this plan is being designed—live across a vast rural area, complicating the situation and making transit planning more difficult.

COORDINATED PLAN | EXISTING CONDITIONS
Matanuska-Susitna Borough

Figure 2-16 Transit Propensity Index Map



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Transit Deserts

Transit deserts are areas that are not currently being served by public transportation services. Individuals living in those areas face increased barriers to accessing basic services if they do not have access to a personal vehicle. It is important to note that there are many areas not currently served by transit; these areas are typically located in the extremely rural geographic locations of the Borough. Some of these areas may be considered “off-grid”, in which case they may not be necessary to serve. However, a number of individuals associated with target populations for this plan live in off-grid areas, making service for these populations a bit of a challenge should they become unable to drive or lack a personal vehicle.



Trip Generators and Travel Patterns

The expansive geography of the region—along with limited-service reach, two lane highways, and rapid population growth—can make travel difficult and time-consuming. Trip generators are concentrated in the more populated areas, including Wasilla, Palmer, and Anchorage, with significant commuter travel along Glenn Highway. Target population groups (older adults, youth, individuals living in poverty, veterans, limited English speakers, and individuals with disabilities) are likely to travel to medical facilities, human service agencies, or veterans’ facilities throughout the study area, as well as in and between their respective communities.

The MSB is one of the fastest-growing regions in Alaska, with hundreds of new residents moving into the region every year. According to the Alaska Department of Labor and Workforce Development (DOL&WD), the State of Alaska and Borough grew rapidly from the 1970s through the 2000s. Around 2010, the Alaskan population began to stabilize, growing only 3 percent between 2010 and 2022. By contrast, the Borough’s population grew 22 percent over the same period. These trends are projected to continue, with the DOL&WD forecasting a 38 percent increase in population within the MSB by 2050, compared with only 4 percent statewide.

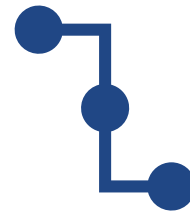
As population and employment patterns shift throughout the region, transportation plays an important role in how people access their jobs. The top industries in the region include government, professional and technical services, accommodation, and food services (10%), retail trade (10%), and health care and social assistance. The

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industries with the largest regional employment growth include professional and technical services, healthcare and social assistance, educational services, and transportation and warehousing. To access employment opportunities (in addition to medical care and educational opportunities), many residents must travel long distances, particularly in rural areas.

REVIEW OF PROVIDERS IN REGION

Transit in the MSB is a major component of the overall transportation network; it is a contributor to quality-of-life in the MSB with the promise of safe journeys, connectivity, and expanded accessibility and options for people who cannot—or will not—own or access an automobile.



Because it is a service provided in the public interest, transit is rarely profitable. To share the burdens and challenges of providing such a service, partnerships may be formed to ensure the funding, operating, and managing of transit. For example, a public agency could be responsible for funding and marketing a new bus route that serves the population, but they may contract private or non-profit entities to operate the service itself (including the hiring, training, and managing of drivers, fleet ownership/maintenance, and customer service). The structure of such partnerships will depend on context and other factors, such as financial constraints, liabilities, and human capital.

This section focuses on shared and mass transportation systems sometimes known as “community transit” or the “coordinated transportation system” as part of a larger network of transportation options. It is arranged primarily on the definition of the routes (fixed-route vs. demand-response) and secondarily on the nature of the provider (public vs. private/non-profit).

Fixed-Route Transit

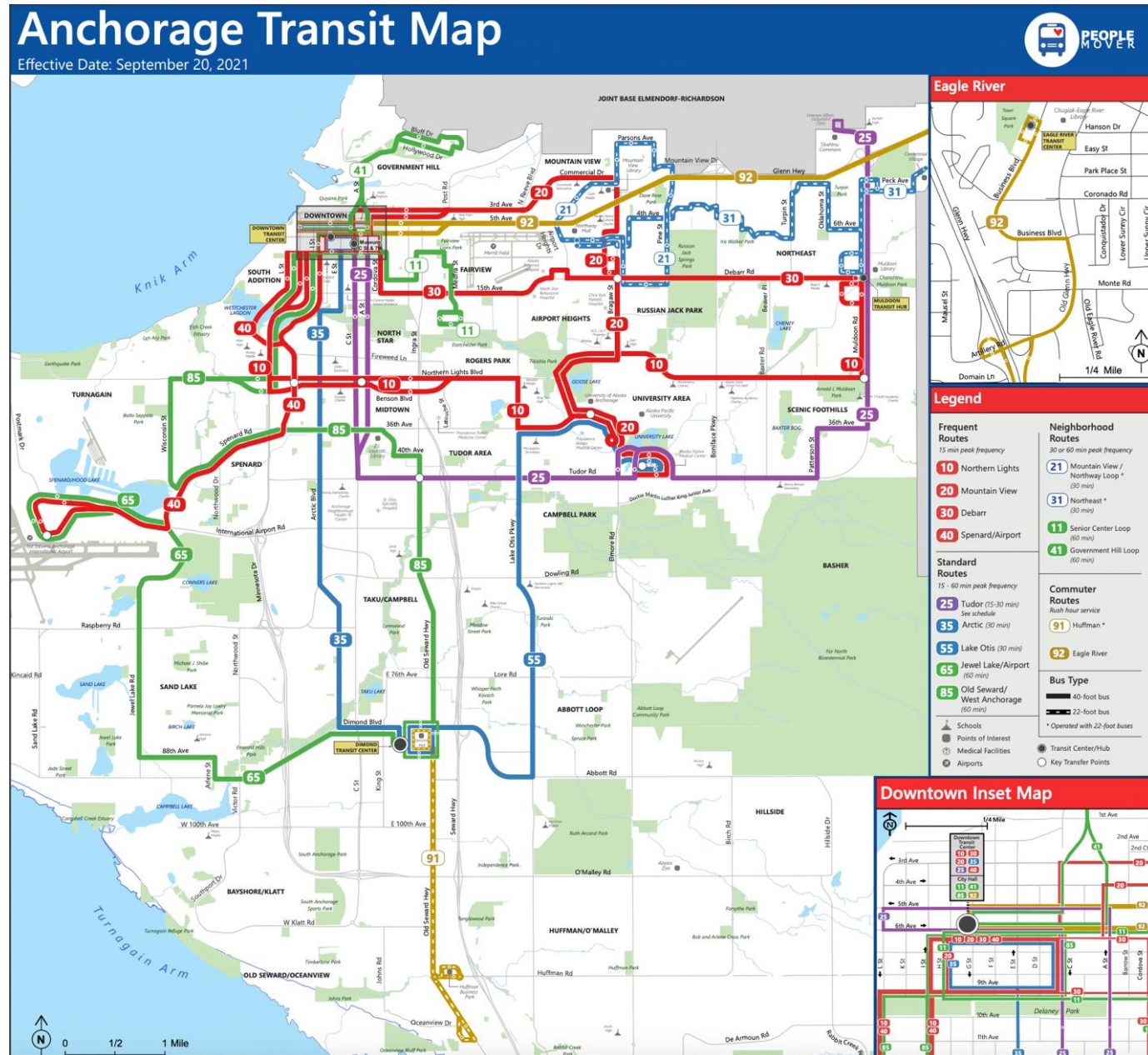
Fixed-route transit is the most understood public transportation mode. By design, fixed-route is intended to arrive and depart at predictable intervals at all its designated stops. Fixed-route transit is typically planned for maximum efficiency on public roadways.



Aside from Valley Transit’s commuter service, the closest fixed route system to the MSB is People Mover, offered through the Municipality of Anchorage. As referenced in the stakeholder interview themes, many individuals travel from the MSB into Anchorage (and vice versa) for jobs, shopping, medical, and educational purposes.

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Figure 2-17 Anchorage People Mover System Map



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In an ideal transit network, fixed-route service would be provided at frequent intervals across much of the day. However, there are limitations to realizing an ideal fixed-route network, including:

- **Financial constraints** for transit capital and operations
- **Timing of transfers** to connecting fixed routes
- **The extent to which sidewalks and bicycle facilities leading to and from transit stops are universally accessible**, in acceptable condition, and designed for short and pleasant trips
- **The availability of connecting transportation from one's front door to the transit station/stop** for circumstances in which one cannot safely or conveniently walk, roll, or bike to the transit stop

Fixed-route transit can help provide relief to coordinated and human service transit in more circumstances than before - but it will vary by trip type and origin location. For example, the People Mover system is working to better coordinate with transit services from the Borough through Valley Transit's services, but sometimes service hours, safe connection areas, and frequencies may be a limitation. For communities already served by fixed-route transit, coordination of a timed transfer or a safe walk to the bus stop is a more cost-effective option that allows resources for demand-response services to be freed up for places that are isolated from fixed-route transit.

Public and Non-Profit Fixed Route Options

Valley Transit

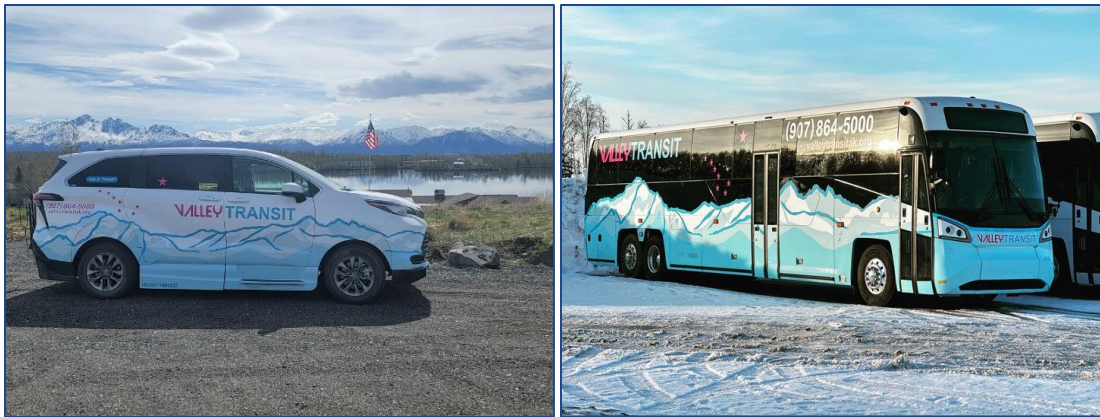
The mission of Valley Transit is to provide accessible, sustainable, reliable, efficient, and quality public transportation. In 2014, the State of Alaska mandated consolidation of Valley Mover with Mat-Su Community Transit (MASCOT), and Valley Transit has been the foremost provider of commuter and demand response services in the Borough, which includes the following services:

- **Regional commuter service** connecting Meadow Lakes, Wasilla, and Anchorage
- **Zone based demand response service** in Houston, Big Lake, Meadow Lakes, Wasilla, Knik Goose Bay, Fairview, Port MacKenzie, Palmer, and the Butte

Valley Transit's fixed route commuter service runs along Glenn Highway between the MSB and Anchorage. Many commuters rely on the service for access to employment, healthcare, and educational opportunities. Their commuter service helps to decrease congestion on the Glenn Highway during peak hours and is a critical lifeline for those individuals with zero car households.



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Valley Transit has been growing rapidly to stay in line with the rapid growth of the MSB. Supply chain demands on bus vehicle production have vastly impacted transit agencies, and Valley Transit is no exception. The agency received a new fleet of buses at the beginning of 2023 that had originally been slated to arrive fall of 2022. These buses are dedicated to their commuter service between the Borough and Anchorage. Additionally, the agency has expressed a need to provide additional demand response services within the Borough but does not have the resources, specifically vehicles, to increase services. The agency is in critical need of more funding for capital, more flexibility to plan for future needs, and for supply line issues to resolve quickly. The Borough also has land use regulation and development challenges that currently prevent Valley Transit from expanding to fixed route services. For example, connectivity between major cities like Palmer and Wasilla is not direct, increasing costs, and travel between major areas during the winter months presents challenges.

The table at right depicts the annual ridership data for Valley Transit for the last four years. While the system was impacted by the COVID-19 pandemic, ridership has already been restored on the system's demand response routes. The commuter ridership is reflective of the dire need for new buses, and now that the new fleet has arrived, they should see ridership increase closer to pre-pandemic levels.

**Valley Transit Ridership
2019-2022**

FY 2019	62,839
Demand Response	11,383
Commuter	51,456
FY 2020	53,768
Demand Response	9,767
Commuter	44,001
FY 2021	29,187
Demand Response	9,599
Commuter	19,588
FY2022	31,183
Demand Response	10,772
Commuter	20,411



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People Mover

People Mover is a division of the Municipality of Anchorage's Public Transportation Department (PTD) and is the largest public transit provider in the State of Alaska. Specifically in Anchorage Municipal area, People Mover provides the following:



- **Commuter express transit services** connecting City Hall and the Eagle River Transit Center via the Glenn Highway (on Route 92)
- **Local bus service** connecting:
 - **Anchorage Senior Center to City Hall** via Medfra Street, 9th Avenue, Hyder Street, & 13th Avenue (on Route 11)
 - **City Hall to Anchorage Museum** (on Route 41)
 - **Dimond Transit Center and the Alaska Native Medical Center** (on Route 55)
 - **Dimond Transit Center and the Downtown Transit Center** (on Route 35)
 - **Downtown Transit Center and the Ted Stevens Anchorage International Airport** via Spenard Road (on Route 40)
 - **Downtown Transit Center with the Alaska Native Medical Center** via 3rd & 4th Avenues, Northway Mall, East High School, and UMed (on Route 20)
 - **Downtown Transit Center and Muldoon Transit Hub**, and the Alaska Regional Hospital (on Route 30)
 - **Downtown Transit Center with the V.A. Clinic** and the Alaska Native Medical Center (on Route 25)
 - **Muldoon to downtown Anchorage** (via Northern Lights Blvd on Route 10)
 - **Northway Mall, Muldoon Transit Hub, Centennial Village, and Creekside Center drive** (on Route 31)
 - **Between City Hall, the Anchorage Museum and the Dimond Transit Center** via Old Seward Highway (on Route 85)
- **Local bus service circulator on Penland Parkway** at the Northway Mall looping to Mountain View Drive, Parsons Ave, Pine Street (on Route 21)
- **Circulator bus service** between the Dimond Transit Center and the Airport via Dimond Boulevard, Jewel Lake Road, and International Airport Road (on Route 65)
- **Limited stop commuter services** from Huffman/Oceanview from the Dimond Transit Center via Old Seward Highway (on Route 91)

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Demand-Response Transit

Demand-Response transportation involves the request for a specified ride by an individual, household, or another unit (e.g., coworkers) making the same trip. These pickup and drop-off points are expected by the rider to be relatively more proximate to the front door of an origin and/or destination. The extent to which the ride is **curb-to-curb**, **door-to-door**, or **door-through-door** will typically be implied in the providers' regulations and determined by several factors.



Demand-response providers are more likely to be carrying the responsibilities of **coordination** and meeting **human service needs**. The added complexities of repeatedly fulfilling demand-response trips may contribute to a higher cost to operate compared to fixed-route transit. The cost of demand-response services—and the extent to which those costs are passed on to the rider—will vary depending on the situation.

Public and Non-Profit Demand Response Options

Sunshine Transit

Sunshine Transit is a “rural” provider of demand response transit service in the Upper Susitna Valley. They strive to provide affordable reliable transit service that breaks down access barriers to healthcare, wellness, education, and employment. Sunshine Transit serves Talkeetna, Trapper Creek, Willow, Caswell, and Houston, and works to complement service provided by Valley Transit.



Sunshine operates one deviated route bus service as well as demand response bus service throughout the Upper Susitna Valley. A variety of target populations currently rely on service through Sunshine, including older adults, individuals with disabilities, and those with low incomes. Youth populations also rely on Sunshine Transit to connect to after school activities, including sports, library, and tutoring programs.

Like Valley Transit, supply-chain issues with buses remain a challenge for Sunshine. The provider reports that if the agency had additional vehicles and drivers, more service would certainly be provided in MSB. The average annual ridership for Sunshine Transit is shown in the table at right.

Sunset Transit Ridership 2019-2022

FY 2018	14,030
FY 2019	16,093
FY 2020	14,825
FY 2021	12,190
FY2022	14,442

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Native Village of Eklutna

In 1961, the Native Village of Eklutna (NVE) government office was organized to protect their land rights. At that time, the NVE had lost over 320,000 acres. In 1982, the tribe became federally recognized and was recorded under the Indian Tribal Governmental Tax Status Act. The NVE Tribal



Transportation Department provides multiple transportation services, including the implementation of the NVE Long Range Transportation Plan, sidewalk, and parking maintenance, and provides demand response ride services for the NVE Clinic, primarily for tribal elders and those members with disabilities. The NVE also implemented the Long-Range Transportation Plan, and is responsible for clearing sidewalks, maintaining village parking areas, and providing demand response ride services for the NVE Clinic. The NVE values partnerships and currently has partnerships with the municipality of Anchorage for support services, including the Anchorage Police Department. The Village has encouraged memorandums of understanding with other potential partners in the Borough for the provision of transit and planning services that mutually benefit the Village and the Borough residents.

Chickaloon Village Transportation

The mission of the Chickaloon Village's Transportation Department is "to provide the safest, most efficient transportation infrastructure for the tribe, our citizens, and the public. This includes FHWA's mission of enhancing mobility through innovation, leadership, and public service."



The goals of the transportation department are as follows:

- **Safety:** Continually improve transportation safety.
- **Mobility & Productivity:** Preserve, improve, and expand the tribal highway transportation system while enhancing the operation of transportation systems and intermodal connectors.
- **Global Connectivity:** Promote and facilitate a more efficient tribal, domestic, and global transportation system that enables economic growth.
- **Environment:** Protect and enhance the natural environment and communities affected by transportation development.
- **Organizational Excellence:** Advance the tribal ability to manage for results and innovation.

The Village offers demand response transit service through the Chickaloon Area Transit System (CATs). CATs was established through an FTA formula program, and operates weekdays from MP 70 to MP 40 of the Glenn Highway. The CATs demand response system is accessible and available to all residents in the service area. CATs offers

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connections to Valley Transit in Palmer & Wasilla with connections to Anchorage. In 2019, the annual ridership for CATs was nearly 3000. Ridership dropped to 2,654 in 2020 and 1,755 in 2021 due to Covid-19 impacts. More recent ridership data will be available at the end of 2023.

ADA Paratransit (“AnchorRIDES”)

One of the most known public demand-response options is paratransit, designed to meet a mandate set by the Americans with Disabilities Act (ADA) of 1991. ADA paratransit is intended to serve as an alternative for people who do not have the ability to safely access the fixed-route system (reasons could include deficiencies in the specific journey to a fixed-route stop or a condition experienced by the rider). As an alternative to fixed-route transit, ADA paratransit is required by law to exist within $\frac{3}{4}$ of a mile of any local fixed-route and during the same hours of operation as the fixed-routes.



Riders of these services are subject to an evaluation process that determines their eligibility to ride ADA paratransit by verifying the rider is, per the ADA, either unable to access a bus stop and lift-equipped fixed-route bus by themselves and/or has a disability prohibiting the rider from independently completing the fixed-route bus.¹ The evaluation process may include a submitted application, professional medical verification, an interview, and an assessment.

AnchorRIDES is housed within the Municipality of Anchorage’s Public Transportation Department (PTD). AnchorRIDES provides shared rides, accessible door-to-door transportation within the urbanized area. PTD administers AnchorRIDES through the state’s coordinated paratransit system. AnchorRIDES operations, customer service and vehicle maintenance are provided through a contract with MV Transportation. PTD determines customer eligibility as well as oversight of MV Transportation.

Human Services

Additional agencies in the MSB provide limited transportation services, usually solely for their own programs and clientele. Human services may encompass a variety of audiences, including those specified for medical trips, for older adults, adult daycare, dialysis services, and the like.



Mat-Su Senior Services

Mat-Su Senior Services (MSSS) was established over 40 years ago and is the largest non-profit senior facility in the MSB. MSSS serves the largest geographical area for senior services and has been a Medicaid Waiver (CHOICE) Program provider for nearly 40 years.



¹ Americans with Disabilities Act of 1990 (Section 37.123 (3))

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MSSS provides in-house demand response transportation services for older adults in the MSB.

To qualify for services, individuals must be a minimum of 60 years of age, an individual with a disability who lives with a senior who is 60 years of age or

	Total Individual Clients Served	Total Miles	Total Trips
2020	213	26,445	1,691
2021	132	25,225	1,443
2022	109	22,169	1,457

older, or an individual who is eligible for the HCBS Waiver Program or Medicaid. MSSS requests reservations to use their services, with a preference for a week's notice for trip scheduling purposes. The number of clients served by MSSS is detailed in the table above.

Wasilla Area Seniors Inc.

Wasilla Area Seniors Inc. (WASI) provides transportation to and from the WASI senior center for congregate meals, Monday - Friday, enabling seniors access to social interaction and a nutritious lunch. Rides are provided by trained drivers, and services are no cost to seniors 60 and older.



WASI's Transportation Program provides personalized, and affordable transportation for qualified seniors and disabled adults. Individuals can schedule essential transportation for doctor appointments, grocery shopping, and prescription pick-up or drop-off on a suggested donation basis or schedule non-essential transportation on a private fare basis. WASI is a Medicaid Choice Waiver and TriWest Healthcare Alliance provider for individuals 60 years of age or older, veterans, have a disability, or live with a senior that is 60 years of age or older.

WASI Transportation provided:

- July 1, 2020 to June 30, 2021 - 829 rides to 65 consumers
- July 1, 2021 to June 30, 2022 - 5,814 rides to 82 consumers
- July 1, 2022 to June 30, 2023 - 8,308 rides to 168 consumers

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Private

Taxicabs

Companies offering up taxis serve emergency, community, intercity, and charter needs based on demand. They include Grizzly Cab, A Cab, Alaska Cab Valley, Weefjuk Taxi, and Big Lake Cab companies. Not all private services are equipped to provide ADA-accessible vehicles and meet riders with all special needs; one should inquire prior to booking a ride or reach out to dedicated entities specializing in such needs (such as Mobility Transportation and Services).



While local taxi companies have created apps and other forms of electronic bookings (beyond the traditional phone and street side hailing), taxi industries are in close competition with transportation network companies (TNCs) that exclusively rely on mobile online apps to match drivers with people in search of a private ride.

Ridehailing & Transportation Network Companies (TNCs)

The act of using a TNC to complete part of a person's trip using electronic documentation and payment is known in this report as ridehailing. Vehicles used for ridehailing—which may or may not be wheelchair accessible—may shuttle private individuals, private groups, and carpools of people taking separate trips. Ridehailing trips are known to contribute to traffic congestion and other negative externalities.



These services can also pose technological and financial barriers for people with older smartphones, limited data plans, or if they are unbanked/underbanked. Currently there is no registered Uber or Lyft service in the Mat-Su Borough except for Disco Dave's which has referral codes for both Uber and Lyft. Disco Dave's is a private provider that offers taxis, bus rentals, and airport shuttle services.

A variation to the ridehailing service is a ride hailing concierge service such as GoGoGrandparent.

GoGoGrandparent turns on-demand transportation companies like Lyft and Uber into services that help families take better care of older adults—without using a smart phone. They can get a ride whenever they want in less than 15 minutes. GoGoGrandparent is offered in all 50 states and can be found in

Anchorage. GoGoGrandparent is not currently operating in the Mat-Su Borough.



Non-Emergency Medical Transportation

A subset of private demand-response transportation is known as non-emergency medical transportation (NEMT), used for transportation to publicly funded healthcare under the Alaska



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Medicaid Coordinated Care Initiative (AMCCI). Typically, NEMT is intended for Medicaid clients who have no other means of getting to and from medical appointments. The State, by extension responsible for NEMT, currently offers an information guide of Transportation Resources under the Alaska Medicaid Member page online. Currently there are no private nor non-profit providers of NEMT in the MSB; these services are provided by the local emergency medical service provider, highlighted in the next paragraph.

Emergency Medical Transportation

Emergency transportation takes multiple forms. Typically priced at an unaffordable cost, the personal choice of ambulance transportation is essential in times of life or death; however, given the limited (or no) non-emergency medical transportation options in the MSB, the EMS is often overwhelmed with “repeat offender” calls to request emergency medical transportation services to non-emergency appointments. In addition, the limitations COVID put on individuals and providers to attend preventative care appointments only increased the need for emergency transport services. First responders may be asked to determine ride destinations in coordination with law enforcement and/or social services for the safety and protection of victims.



People with emergency and non-emergency needs may occasionally take air transportation:

- LifeMed Alaska
- Medevac Alaska

It was noted by a few stakeholders that these services are sometimes a necessity for tourists to the Mat-Su region who are older adults. Many tourists overestimate their abilities (hiking and other outdoor activities) and require emergency medical transport into Anchorage when on vacation.

Other Transit

Some transit is designed for the exclusive use of a group, such as employees accessing a specific location, residents from a complex taking a shuttle, and students in need of a safe passage to their dorm or parking space. In other words, these services are exclusive because they serve a specific population and place and are not available to the general public (even if willing to pay). Examples of these include:



- Employee shuttle
- Elder care
- Veterans Affairs services

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It is challenging to track every possible service available and there is reluctance to share resources due to liability concerns. However, these services, which will require the involvement of a vehicle fleet and trained operators, carry the potential to serve as a locally based resource and partner in coordination.

RideShare (Carpool & Vanpool Service)

The Carpool/Vanpool Program provides groups of five or more riders the opportunity to “pool” rides together to places of employment. In Anchorage, free carpool matching services are provided through a contract with Commute with Enterprise. There are currently no ridesharing services readily available in the MSB. Given the growth the area has experienced, however, ride sharing may be a viable option to add to the family of services in the Borough.



Other Transportation Services

There are also services which do not directly provide transportation but are vital resources in helping people affordably and knowledgeably complete their trips.

Metropolitan Planning Organization(s) and Committees

Municipalities (the cities of Palmer, Houston, Wasilla) are responsible for the application of land use laws and policies affecting the design of locally owned streets, which have a bearing on how people use the transportation system. Each municipality has their own land use planning authority and conducts independent planning and zoning. However, despite MSB ownership of the majority of roads, and being the sole planning organization for the Region, the transportation system suffers, being limited by various city codes. The MSB does not have robust land use regulations that would guide development and support transit planning and development. Unfortunately, the region has not been investing in infrastructure like bus stops, park and rides, mobility hubs, and pedestrian facilities that would support transit. Additionally, current land use regulations within the Borough hinder development and planning for adequate transit infrastructure needed for a rapidly growing region.



A community where the placement of buildings and permitted uses containing essential needs—all within a safe walkable distance of people's homes, workplaces, and schools—can be pre-determined with a solid land use plan which aspires to a future of universally accessible multimodal transportation options for the entire population. The complementary attributes of complete streets and/or layered networks designed for the safe and comfortable enjoyment of people who walk, ride bicycles, and use their personal mobility devices (just as much as people who drive automobiles for personal and commercial reasons) can also encourage more people to use transit. Committees, such as those formed to support planning efforts, and standing committees like the Public Transit Advisory Board (PTAB) in Anchorage, help provide a public voice and feedback to planning efforts. The Borough currently has a Transportation Advisory Board (TAB); however, additional efforts must be made to consistently plan for future transit needs in the MSB. The ongoing development of the Metropolitan Planning Organization may be able to strengthen the connection between land use planning and the opportunity to build a more robust transit system for the community.

Travel Training

A major support for people to ride transit is the act of education. Riders—including those from vulnerable populations—benefit from travel training to understand available transportation options in their communities, along with how to use such services. There are currently no travel training programs in the MSB, but there are good



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lessons to be learned from the programming in Anchorage. These trainings are provided by People Mover, who employs one full-time travel trainer. The goal of the Travel Training program is to empower individuals to travel independently using the People Mover system. Travel training is free for all individuals. The travel training program offers individual and group travel training through a “train the trainer” system, with presentations for staff training, organization meetings, workshops, and materials for caregivers. Customers may also receive travel training as individuals or in a group on how to use the People Mover system.

Figure 2-18 People Mover Travel Training



Driver Training

Although professional drivers are expected to obtain the appropriate licenses, there may be additional training which can help drivers—both professional and volunteer—be more responsive and sensitive to the needs of older adults and people with disabilities. Safety training (including passenger assistance methods, disability awareness, and defensive driving) can be provided through national and state conferences. In many cases (depending on the employer), driver trainings are required. More on driver training opportunities will be discussed in the strategy section of this plan.



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Centralized Dispatch Pilot Project

This project is a high priority “implementation project” from the region’s 2018 Coordinated Public Transit-Human Services Transportation Plan, which was led by the MSB and the Mat-Su Health Foundation. The project intended to implement a centralized mobility management system in a single, online platform to coordinate the scheduling, dispatch, call-taking, fleet management, and payment functions for multiple transit and human services transportation providers in a large rural area (covering 25,000 square miles), where traditional transit service is largely cost-prohibitive. Unfortunately, with the unique service areas (urban vs. rural) and services offered by each of the participating providers, they were not able to find one software that was effective for all providers. With three dispatch systems between the four providers, the need for improved coordination between providers and consistent data collection is still a high priority.



Bike and Transit Programs

Bike and transit programs specifically target first- and last-mile issues to using transit to provide a cost-effective means of personal transportation and can be especially important and effective for low-income residents.



The MSB is in the process of wrapping up a borough-wide Bike and Pedestrian Plan (BPP) that will increase safety and connectivity. The BPP purpose is to develop priorities for bicycle and pedestrian infrastructure to support access and healthy lifestyles. The BPP will identify connections between urban and rural areas and recreational opportunities. Ultimately, the BPP will offer connections throughout the Borough so that individuals may bike from their homes to downtown Wasilla or a nearby park-and-ride to catch a commuter bus.

Youth Transportation

Youth transportation encompasses transportation to meet the following needs for youth (typically minors, or people under the age of 18, but variable depending on special needs and academic trajectories):



- School buses and transportation programs for youth
- Connections to afterschool, vocational, and remedial programs for youth
- Arrangements for carpooling to and from school among families (“schoolpools”)
- Transportation for youth transitioning to adulthood that are currently placed in unhoused programming (i.e., My House)

In the MSB, some youth transportation services are currently being provided through contract with local providers. Valley Transit works with Youth 360 to provide rides for

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their afterschool programs, as well as demand response services coordinated by parents to get kids to and from school. Sunshine Transit also works to provide services for students to afterschool activities; however, both providers discussed the financial sustainability of the services and need for more funding to be able to continue to provide them.

Transportation Needs Assessment

This section presents an overview of transportation needs and gaps in the MSB for the target population groups—older adults, youth, individuals with disabilities, individuals living in poverty, veterans, and limited-English speakers. The overview draws from four inputs:



- **Demographic Analysis:** The project team analyzed demographic characteristics and mobility and access conditions, as well as available transportation services.
- **Plan Review:** Early in the development of the Coordinated Plan, the project team collected transportation and infrastructure plans conducted in the MSB and the Anchorage Municipal area. These plans were reviewed for goals, common themes, and relevant implementation projects.
- **Stakeholder Interviews:** The project team conducted 17 stakeholder interviews between June and August 2022. Stakeholders included transit providers, human service organizations, and local, regional, and state organizations.
- **Service Inventory:** An inventory of current services was also compiled during the stakeholder interviews. The service inventory describes the type of services currently available to the region.

Five themes emerged in our assessment of needs and gaps:

1. Coordination and collaboration
2. Access to key destinations
3. Regional transportation needs
4. Education and awareness
5. Funding

These themes will help to inform the goals, recommendations, and strategies, as well as the prioritization of projects and programs discussed in future chapters and the implementation plan.

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1. Coordination and Collaboration

More coordination and collaboration came up as a need among several stakeholders. Many expressed major concerns about the lack of coordination between agencies. Most could point to *some* level of coordination with other organizations and providers. For example, some stakeholders coordinate on service functions, such as making transfers easier or otherwise more seamless. However, this type of coordination is rare, and areawide coordination is not widespread.



Gaps in transportation service were a common theme. In particular, an identified need for some type of transit (fixed-route service) were identified as a need, particularly between Wasilla and Palmer. Stakeholders noted that many marginalized populations, including people with low incomes, reside in gaps between service areas.

Stakeholders noted that it was difficult to increase available services because many agencies have issues with staffing resources, education, outreach, and service costs. Staff time and capacity were the primary concerns for all organizations and other stakeholders. For example, many agencies noted challenges with operator retention and overall driver shortages, from transit providers to school district transportation departments. Many organizations have as few as one or two staff members dedicated to operations, and sometimes even maintenance activities.

2. Access to Key Destinations

Providing transportation services for veterans and tribal elders for needed services, such as medical appointments, was a notable concern among several stakeholders. The regionalization of veterans' services means that each entity has their own ways, processes, and schedules, making coordination more difficult when changes occur. The rural nature of tribal lands means that accessing would-be client homes becomes a major issue in the winter, especially when roads are not being cleared regularly.



Access to healthcare is a major need for the region. Stakeholders expressed the need for transportation to provide more predictable access to healthcare appointments, such as dialysis and other critical services. In the 2018 Coordinated Plan, this was a significant topic of discussion and remains a challenge for the region.

Better transportation to employment is a common need among stakeholders. Large employers lack sufficient transportation options for those without access to vehicles. In addition, there is a significant need for stronger connections between population centers; from Wasilla to Palmer, from the MSB to Anchorage, etc. Many people commute between the larger population areas for employment, but in the winter,

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those commutes become challenging and even dangerous. Should an accident occur on Glenn Highway, for example, the road would be shut down until it is safely cleared.

3. Regional Transportation Needs

The service needs of urban and rural areas are vastly different. Unfortunately, these differences can be geographically complex due to service boundaries, municipal boundaries, tribal nation boundaries, and funding designations for place types.



Rapid growth can make it hard for transportation services to meet increasing demand. This can have implications on opportunities for federal funding. The MSB grew significantly since the last coordinated plan, and the pandemic only increased growth in the Borough, as a number of families nationally relocated from urban areas to more “rural” and suburban areas.

Some agencies noted geographic barriers to service such as rail lines and rivers that can cut off major routes and cause major transportation disruptions and increase unreliability. In the case of the MSB, the singular highway connecting the Borough to Anchorage can create challenges, as can two-lane roads connecting destinations within the Borough. Additionally, a number of providers noted challenges with more rural roads that are unpaved and/or unplowed in the winter months.

Stakeholders noted specific gaps in service, including more rural areas throughout the Borough, particularly on Tribal lands, where residents do not have comparable access to public transit. This reflects the need to provide more cost-effective transportation options to areas that do not meet the density required for fixed-route transit. Additionally, the pure size of the MSB is a gap for service. Many providers noted the inability to serve all areas that are requested due to long headways from point-to-point.

Stakeholders mentioned the need for a seamless regional transit system that is efficient, affordable, dependable, and safe. In areas where fixed-route transit is not feasible, stakeholders brought up the creation of park-and-ride lots as a possible solution, particularly in areas between the Borough and Anchorage. However, there was emphasis on ensuring individuals could also easily access these lots, not just by single occupancy vehicle, but by active transportation modes such as biking and walking. There is a significant concern regarding limited or no sidewalk access, particularly in the winter months.

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4. Education and Awareness

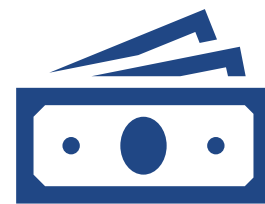
An additional concern among stakeholders was ensuring representation by target populations in feedback-gathering community engagement activities. Some agencies simply lack the resources to collect a wider, more representative sample of community feedback. In addition, there were concerns with gathering representative feedback from Tribal representatives.



Better mechanisms for disseminating information are also necessary. One stakeholder shared an example of organizations being unaware of FTA Section 5310 funding, which highlights the need to provide educational opportunities to new or existing service providers. Additionally, entities could benefit from better understanding funding options available for transit provision, especially in light of changes resulting from the decennial census.

5. Funding

Stakeholders noted that the lack of funding hinders their ability to provide needed services for their communities. However, while funding is an issue across the region, the specific needs vary considerably between organizations. Many need additional funds to purchase new vehicles as they age beyond their useful life, while others are looking to hire, retain drivers, and expand their service. Most agencies have all the needed funding for operations; however, all noted a need for more funding for capital projects.



Rural agencies have unique funding needs. Organizations in rural towns with smaller service areas face an issue where their vehicles age but do not reach the miles needed to upgrade to new vehicles. By contrast, rural organizations that serve large areas have vehicles that are driven over exceptionally long distances. They voiced concern over the way their revenue miles are calculated, stating that funding sources do not cover the extremely long deadhead miles that accrue when returning from these trips.



Chapter 3.

Review of Existing Plans, Studies, and Reports

This chapter summarizes the overarching goals identified in existing plans and policies influencing transit service funding and transit development throughout Alaska, with a focus on the MSB. The project team reviewed a diverse cross-section of documents that guide transportation and transit planning. Key findings from this analysis are shown on the following page.

The remainder of the chapter includes the following:

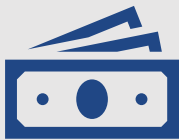
- A summary of the goals and needs (where applicable) across these plans and their relation to transit.
- Recognition of the constraints to transit access and implementation facing the MSB.
- A summary of transit-supportive strategies at the regional and state levels.
- Needs, gaps, and barriers related to transit access, service provision, and coordination.

KEY FINDINGS



Public transit does not adequately serve rural populations.

Low densities, large service areas, and extensive distances between activity centers complicate the delivery of public transit in rural areas of the MSB. Poor connectivity to regional hubs makes it difficult for residents to get their basic needs (e.g., medical care, education, shopping, and recreation) met. Opportunities exist to improve connections between rural and urban passenger travel via improved intermodal connections.



Funding remains a key barrier for transportation improvements.

There is limited dedicated funding in place to support the transit improvements needed to address the demands of a growing and aging population. Key funding sources are restrictive and different funding types may apply only to unique services for specific populations, can be used for limited purposes, or are restricted to a defined region (urban vs rural); reimbursements for non-emergency rides through Medicaid are often delayed, impacting providers. Lack of coordination between providers can also result in duplicative services and under capacity vehicles being under funded.



There is a desire to improve coordination of transportation services between transit and human service providers.

Due to limited availability of federal and state funding, it is in the best interest of transit and human service providers to coordinate transit programs and services to make the most efficient use of existing resources and to avoid duplicative efforts. The statewide long-range plan and policy references the desire to coordinate at broader scopes, stating that there is “higher demand for specialized transportation such as human service transportation, public transit, and other alternatives in various regions.” The next step is to encourage coordination at the regional level.



Lack of support to implement transportation solutions.

Several plans have been developed over the years with solutions to regional needs and growth. Transit options have yet to be implemented for various reasons, but the lack of political will is a significant factor. Nationwide, local and regional governments often support public transportation or run their

own transit operations. The Mat-Su Borough has not considered this, adding additional burden to nonprofit organizations working to provide affordable and reliable transportation options. Additionally, land use and development have yet to be guided in a way that plans for transit infrastructure, such as bus stops, or allows easy access to commercial or medical districts, employment, or government services.

STATE PLANS

Alaska Statewide Long-Range Transportation Plan, Let's Keep Moving 2036 (2016)

Let's Keep Moving, the Statewide Long-Range Transportation Plan, establishes transportation policies, goals, and implementing actions for the Alaska Department of Transportation and Public Facilities (DOT&PF) through 2036. The purpose of the plan is to set policy and investment priorities. In addition, it updates the 2030 long-range plan, "Let's Get Moving 2030", published in 2008. The 2036 Plan Vision is "to provide a transportation system that enables a robust and growing economy and meets the mobility needs of the State's residents."

The vision, including an assessment of opportunities and risks to the vision, help to shape the development of policies and implementation activities to help guide investments through the management of a statewide transportation system. Additionally, the long-range plan has multiple goals, including:



1. **Manage the Alaska Transportation System using a performance-based measurement approach** for federally funded surface transportation assets (based on federally required performance measures, focusing on safety, congestion and the condition of pavements and bridges).
2. **Prioritize investments in system preservation, modernization, and new construction** based on their impact on our transportation system performance goals and cost effectiveness.
3. **Proactively monitor trends and manage risks** to transportation system performance
4. **Monitor economic development activities and projects** so that the resulting demands for transportation infrastructure investments can be addressed
5. **Address increases in travel demand in urban areas** through MPO, corridor and area plans
6. **Improve transportation system resiliency** and add redundancy to address safety and security risks
7. **Manage and operate the system to improve operational efficiency** and reduce safety risk
8. **Incorporate livability, community, and environmental concerns** in our decisions

9. **Provide transparency** for the allocation of scarce resources and accountability for the performance of the transportation system through performance measurement and reporting

Of note, the Alaska Long Range Transportation Plan will be implemented through the development of transportation and multi-modal plans throughout the state, outlined as follows:

- Area, Corridor and Modal Plans
- MPO Plans
- Transportation Asset Management Planning
- Plan Actions
- Performance Management
- Development of the CIP and STIP

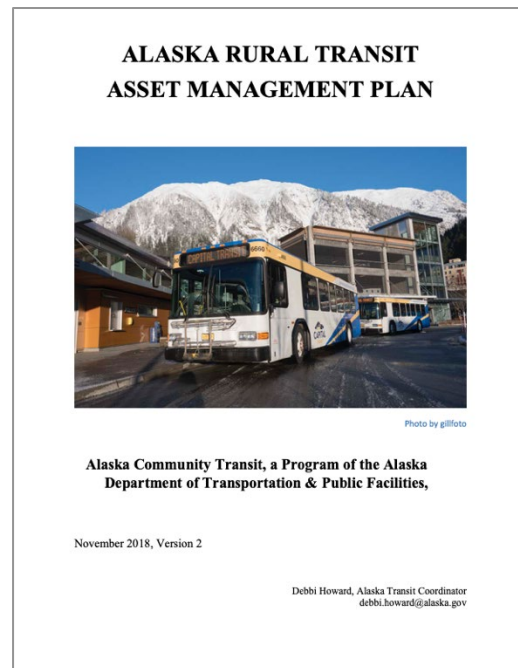
Alaska Rural Transit Asset Management (TAM) Plan (2018)

As a direct recipient of Federal Transit Administration (FTA) funding, the Alaska Department of Transportation and Public Facilities (ADOT&PF) Transit Programs Division developed this Rural Transit Asset Management Plan (TAM) to document the statewide approach to transit asset management. The TAM Plan seeks to provide guidance to Alaska's rural transit providers as they operate and maintain their capital assets to ensure reliable and safe service delivery for transit riders across the state. Additionally, the TAM is heavily focused on State of Good Repair (SGR) and providing support to rural providers to help maintain SGR for all vehicles.

The mission statement of ADOT&PF is

"To support the DOT&PF mission by providing access and mobility within the communities of Alaska, both urban and non-urban, through transit services that are safe, appealing, efficient, and easily-available to both the general public and transit-dependent populations."

The TAM plan sets out two main goals, coupled with objectives and metrics for statewide asset management. The goals of the TAM are:



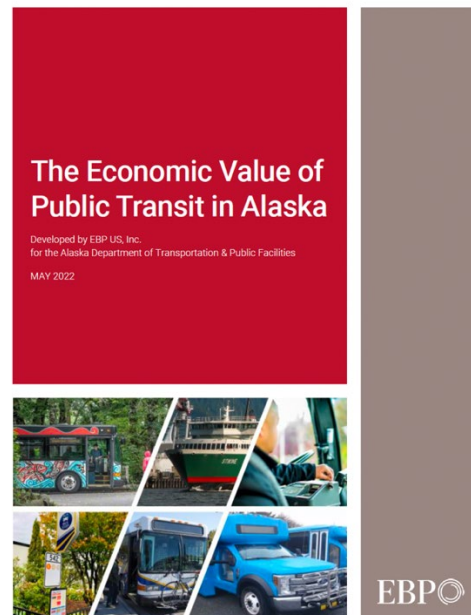
- **Goal 1: Reduce the number of vehicles that have passed their ULB life by 3% annually.** - Prioritize the replacement of vehicles that have passed their ULB.
- **Goal 2: Reduce the number of vehicles not in SGR by 3% by 2020.** - Dispose of vehicles that pose an irreparable unacceptable safety risk or provide the necessary repairs and/or refurbishment to place the vehicles back in SGR status.

The Economic Value of Public Transit in Alaska (2022)

This study highlights the economic value of public transit in Alaska and includes the providers that receive grant funding from the Alaska Community Transit Office, which includes Valley Transit and Sunshine Transit in the Mat-Su Borough. The report covers the many benefits of public transit covered through the following themes and their respective key findings:

Statewide Economic Impacts of Transit Expenditures

- 831 Jobs
- \$113.9 Million in Annual Sales (supported by transit agency expenditures)
- \$1.9 in business sales for every dollar spent on transit



Transit Commuters and the Alaskan Economy

- 5,645 employees use transit to get to work
- \$203M in annual wages brought home by transit commuters
- \$941M in annual sales facilitated by transit commuters 2% transit commuter share statewide

Transit's Role in Providing Inclusive Mobility

- 28% of Alaska transit commuters live in zero-car households
- \$24,826 median income of Alaska transit commuters
- 52% of Alaska transit commuters identify as non-white
- 24% of Alaska transit trips are by young people under the age of 16
- 34% of Alaska transit trips are by older adults 60+ years of age

Performance Benefits of Transit

- 1M trips enabled by Alaska transit agencies that would not be possible otherwise
- \$117M in annual benefits from Alaska transit compared to \$56M in average annual costs

Transit Agency Highlights

- The transit agency highlights for both Valley Transit and Sunshine Transit will be covered in the providers section of this report.

In summary, transit agencies in Alaska provide \$117 million annually in benefits to riders, visitors, and broader circles. The benefits include the system users, local communities, and those trips for individuals who would be unable to travel otherwise. Cumulatively, the benefits significantly outweigh the costs for providing transit throughout the state of Alaska.

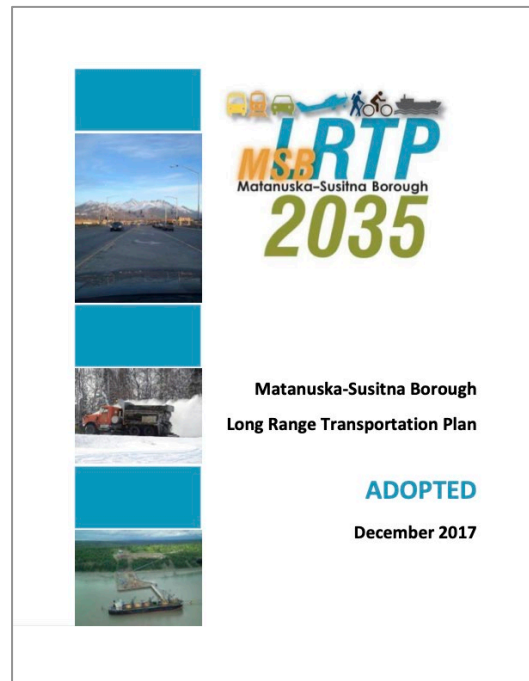
REGIONAL PLANS

Matanuska-Susitna Borough Long Range Transportation Plan 2035 (2017)

The Matanuska-Susitna Borough (MSB) worked to develop a Long-Range Transportation Plan (LRTP) in conjunction with ADOT&PF to help guide transportation solutions, improvements, funding decisions, and policy development by the MSB and the State of Alaska both in the near and long term through 2035.

The LRTP is multi-pronged; the purposes of the plan outlined as follows:

- **Establish community goals** for the MSB transportation system;
- **Plan and recommend strategies for all modes of travel**, including personal automobiles, bus/transit, bicycles, pedestrians, freight, rail, marine, and aviation;
- **Develop and analyze a range of improvements** that address identified mobility, safety, and accessibility needs;
- **Develop a prioritized, fiscally constrained list of roadway improvements** to be completed through 2035; and
- **Develop a short-term implementation strategy**



The MSB developed seven goals through input received from public workshops and meetings, as well as input from and research by the LRTP project team. Goals were developed for the future of community transportation in the Borough.

- **Goal One:** Improve Transportation and Land Use Connection
- **Goal Two:** Provide Transportation Choices
- **Goal Three:** Improve Connectivity
- **Goal Four:** Improve Mobility
- **Goal Five:** Make Transportation Safer
- **Goal Six:** Support Economic Vitality
- **Goal Seven:** Enhance Environmental Quality

The MSB's LRTP also addresses regional coordination. There was a proposal created for the establishment of a Regional Transportation Planning Organization (RTPO). An RTPO is a group of non-metropolitan local officials and transportation system operators that a state may assemble to assist in statewide and non-metropolitan transportation planning.

Additionally, the LRTP examined a formal Transportation Partnership amongst regional stakeholders to help address areas of coordination with other transportation stakeholder agencies and government structures within the MSB (DOT&PF, the cities, ADEC). The plan recommended continuing efforts to improve coordination, efficiency, and knowledge-sharing between all transportation decision-makers.

Transit on the Move (2020), Municipality of Anchorage

In the Fall of 2019, Anchorage's Public Transportation Department (PTD) began an overhaul of the People Mover bus system. As a direct result of the changes, ridership began growing after a decade of steady decline. The main changes involved service expansion into areas outside of the city core and providing more frequent service to densely populated (high ridership) areas. With any change, however, gaps still exist. The plan will continue improve the system, especially considering service changes related to the COVID-19 pandemic.

The Transit on the Move plan includes a mission statement and identifies goals and objectives for PTD to work toward and identifies performance measures and targets to track progress. The plan creates a list of priority



projects to improve the transit system, which are queued up and ready for implementation as additional funding is made available.

The mission statement of the PTD is:

Connect the community with safe, reliable transportation options, emphasizing customer service while providing economic, social and environmental benefits.

The following needs, gaps, and barriers were also identified through an extensive public engagement process:

- Concerns with planning, including route alignments, bus stops, frequency, span of service, and transit amenities.
- Comments related to reliability, including schedules, timeliness, and safety.

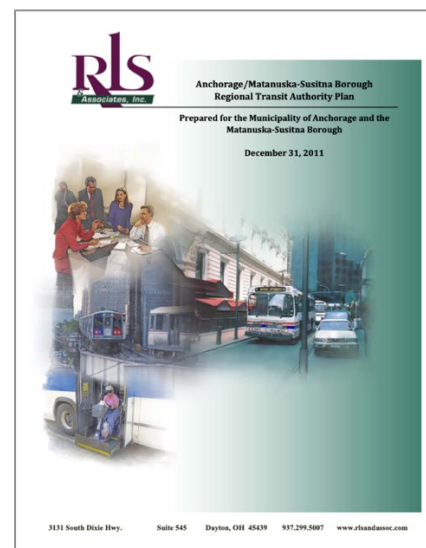
Plan goals and objectives include:

Goal	Objectives
Accessibility	<ul style="list-style-type: none"> Increase access to jobs / residents Increase seasonal accessibility of bus stops Evaluate the cost of public transit Increase our reach
Convenience	<ul style="list-style-type: none"> Decrease wait time Expand service Travel time Increase amenities at bus stops
Reliability & Safety	<ul style="list-style-type: none"> Increase vanpool participants Improve on-time performance Decrease number of missed trips Improve security at bus stops & on buses

Anchorage / Matanuska-Susitna Borough Regional Transit Authority Plan (2011)

This plan, conducted in partnership with the Municipality of Anchorage and the Mat-Su Borough focused on the development of a Regional Transit Authority (RTA) to better plan and coordinate for public transit services. The plan discussed the feasibility of establishing an RTA, including a guide for the management and organizational structure for regional public transportation services in Southcentral Alaska. The plan is relatively focused, and comprised of four main tasks:

- A review of regional transit management and governance;
- An analysis of regional transit service and operations;

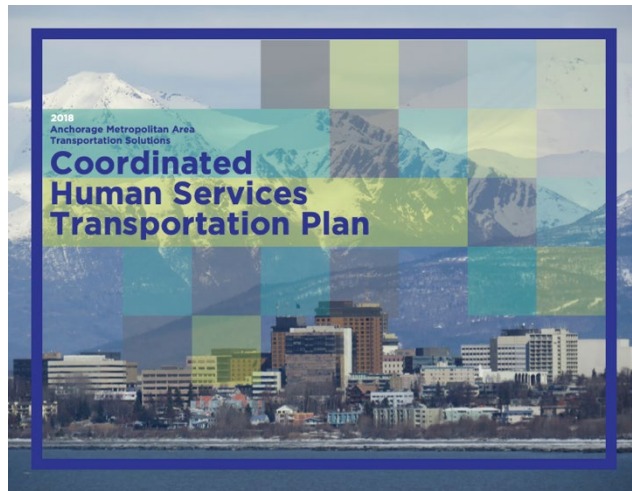


- An analysis of regional transit costs and funding; and
- The creation of a Regional Transit Authority Plan and recommendations.

The RTA plan included an overview of existing public transportation services, the recommended organizational structure of a Southcentral Alaska RTA, descriptions of potential RTA-provided transit services, a financial plan and an implementation plan.

Anchorage Metropolitan Area Coordinated Human Services Transportation Plan (2018)

The Anchorage Coordinated Human Services Transportation Plan (CHSTP) serves to improve transportation for transportation-disadvantaged populations in the Anchorage Bowl and Chugiak-Eagle River region by identifying the transportation needs of seniors, individuals with disabilities and people with low incomes. The plan included recommended strategies and actions for meeting these needs, as well as implementation. This Coordinated Plan covers fiscal years 2019-2023 and is an update to the 2009 Human Services Transportation Coordination Plan.



The Anchorage MSA Coordinated plan included a detailed needs assessment, summarized below:

- **Study /address unintentional access issues** created by PeopleMover redesign
- **Provide more transportation options** during late, weekend, and holiday hours
- **Fortify/strengthen Anchorage's transportation systems** throughout the winter
- **Connect key players** to better collaborate around human services transportation
- **Address funding limitations and barriers**
- **Provide transportation information** in more languages and non-web formats

There are three focus areas that came out of the coordinated plan associated with needs. These three areas are what the region will focus on for the next 5-year horizon:

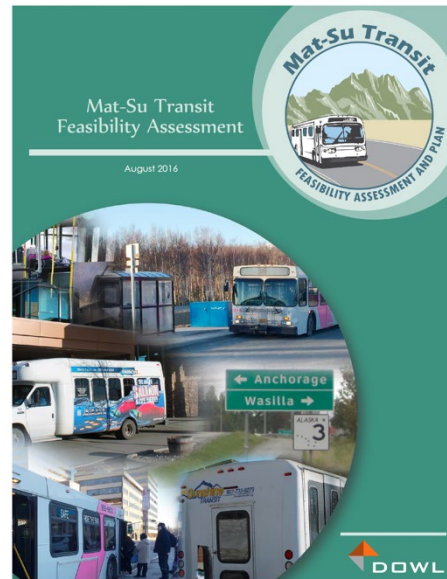
1. Working together as a community
2. Planning and building and inclusive transportation network
3. Growing and sharing funding for human services transportation

A Vision of Mobility: Practical Public Transit for the Matanuska Valley (2016)

This highly technical research report was written somewhat subjectively related to the need for transit in the Mat-Su Valley. The report discusses how not making the investment in public transit actually costs regions, and how efficient and effective planning could potentially support the needs in the Valley. The appendices go into a great deal of detail related to planning for the Valley, including a discussion of basic system design, expansion, how to organize and govern service, and whether a transit authority is appropriate. The report is a little dated, as it refers to SAFETEA-LU and MASCOT throughout. There are no clear goals in the report, and no real costs associated with the proposals being made.

Mat-Su Transit Feasibility Assessment and Plan (2016)

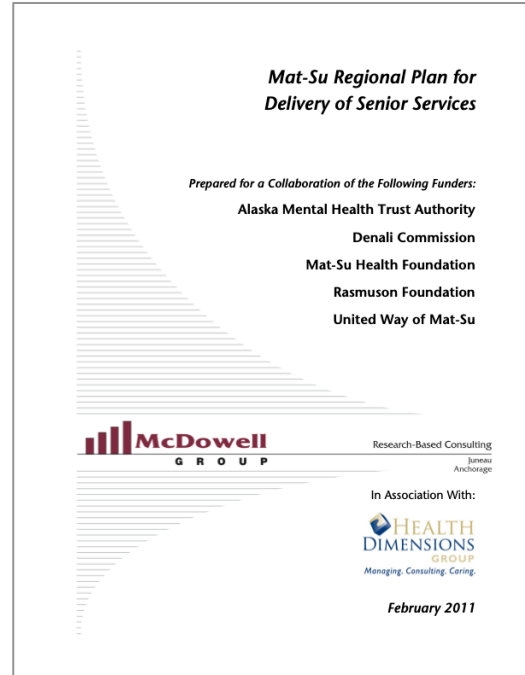
The Mat-Su Transit Feasibility Assessment consists of a detailed evaluation of existing and future transit services to identify and recommend potential restructuring of the current system. The purpose of the plan is to make recommendations to improve the current system and to simplify governance and funding. The report, like others is somewhat dated as it makes references to MASCOT, though does discuss planned consolidation of MASCOT and Valley Mover. The report details population growth and demographics, noting significant increases in older adult populations, followed by a growing youth population. Of note in this plan is a reference to coordination between the providers and ADOT&PF in order to provide bus stops on DOT owned roads. This continues to be a challenge in the Mat-Su Borough. Another important section to note is the Transit Governance Models discussion. This report clearly identified the challenges and opportunities with non-profit models versus public agency models. This topic should be resumed in the MSB related to service provision and planning for the rapidly growing region. Given the growth, it may be time to consider a public agency model to meet local needs.



HUMAN SERVICE POLICIES AND PLANS

Mat-Su Regional Plan for Delivery of Senior Services (2011)

The Alaska Mental Health Trust Authority, Denali Commission, Mat-Su Health Foundation (MSHF), Rasmuson Foundation and United Way of Mat-Su share a concern for the senior citizens of Alaska. The rate of population growth among the 65 and older demographic in Alaska is one of the highest in the nation and has not slowed down since the creation of this plan. The increase in senior citizens is putting a strain on the senior services delivery system, particularly in the MSB. To better understand the needs of Mat-Su's senior population and to match those needs with an efficient and productive delivery system that can be implemented on a regional level, this plan was developed for delivery of senior services in the Mat-Su.



The study included a demographic analysis of seniors and an assessment of senior services infrastructure, such as senior centers, senior housing, home and community-based services, senior transportation, and skilled nursing care. The plan itself is over 10 years old, so there have been some changes to the infrastructure, especially since the pandemic. In addition to documenting senior services, the plan includes a demand analysis for senior services and analyzed the current gap in services and into the future. Through the research, four overarching regional strategies were developed for the Mat-Su and were analyzed for their financial feasibility. These strategies were shaped in the form of challenges. The challenges for service delivery in the Mat-Su are:

- Lack of service coordination among providers.
- The geography offers substantial challenges to service delivery.
- State governmental infrastructure is inefficient in identifying and qualifying seniors for service
- Current service provisions are not sufficient to support future demand

At the time of publication, the range of services offered in the Mat-Su is fairly broad, and there were likely sufficient service offerings to support the population in the short-term - especially for information and referral, care coordination and case management, home health, hospice, and adult day services. The study found, however, that there was considerable duplication of service among different

providers. Lack of service coordination or an over-arching coordinating element must evolve in the Mat-Su to support seniors in the future.

The analysis conducted, in combination with anecdotal observations offered by service providers and stakeholders both in and outside the MSB, point to several areas of concern in the current infrastructure. It was recommended that the areas of concern be addressed in a regional plan that maximizes service to seniors in cost- effective and efficient manner.

These areas included:

- Unnecessary and cost-ineffective duplication of service
- Lack of sufficient service offerings to support seniors with dementia, cognitive impairment or Alzheimer's disease and related disorders
- Insufficient coordinated transportation services
- Absence of institutional long-term care or skilled nursing beds
- No formal program to manage chronic illness and support independence
- Limited-service provision outside borough "urban" centers

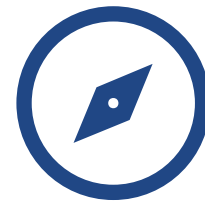
The study noted that the infrastructure gap analysis was based on the opinions of area providers, stakeholders, and observations from the study team. The plan ultimately recommends a thorough needs assessment effort to identify other areas of opportunity.



Chapter 4. Stakeholder Engagement Summary

Stakeholder Interview Summary

As part of the MSB Coordinated Plan's stakeholder outreach and engagement process, the Socius Amica team conducted a series of stakeholder interviews between the months of July and August of 2022. Stakeholders interviewed for this task included transit providers, human service organizations, and local, regional, and state agencies and organizations. The purpose of the stakeholder interviews was to:



- **Understand the roles, perspectives, and vision** of key transportation-related agencies and organizations in the study area
- **Identify transit and mobility needs and gaps**, including those related to transportation services as well as structural needs, such as organization, management, and resources.
- **Identify the key concerns, issues, and gaps related to the transportation and mobility situations of the priority populations** across the varied geographic, geopolitical, and transportation-services contexts of the regional study area.
- **Document the immediate and ongoing impacts of the COVID-19 pandemic.**

At the onset of each interview, the project team encouraged stakeholders to speak freely and assured them that any comments or ideas expressed would be anonymous. Thus, findings presented are not attributed to any individual or organization. The key takeaways are organized by the following topics:

- **Coordination** (within the MSB and with other municipalities)
- **COVID-19 Pandemic Impacts**
- **Service Needs and Gaps Planning needs**
- **Priority Populations**
- **Funding**
- **Need for more Resources**

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Participating Stakeholders

In total, 20 “Tier 1” stakeholder agencies were selected to participate in the initial interview process. Stakeholders for the coordinated plan were categorized into three different categories: Tier 1, Tier 2, and Tier 3. Tier 1 stakeholders are those being asked to participate in the project advisory committee and in the interview process, whereas Tier 2 and 3 stakeholders will participate in larger group meetings and during the public comment period of the project. The table below outlines the agencies that were interviewed as a part of the initial engagement for the coordinated plan.



Stakeholder Type	Organization Name
Public Transit Provider	<ul style="list-style-type: none"> Valley Transit Sunshine Transit AnchorRides
Tribal Nation	<ul style="list-style-type: none"> Chickaloon Area Transportation Service (CATS) Native Village of Eklutna Knik Tribal Council
State Agency	<ul style="list-style-type: none"> Alaska Department of Transportation
Human Service Organization	<ul style="list-style-type: none"> Mat-Su Senior Services My House Mat-Su Coalition on Housing and Homelessness Identity, Inc. Valley Charities Link Alaska
Health Agency(ies)	<ul style="list-style-type: none"> Mat-Su Health Foundation MSB Emergency Management Services Chickaloon Life House Clinic Community Health Center
Education Entities	<ul style="list-style-type: none"> Mat-Su Borough School District
Planning Organization	<ul style="list-style-type: none"> Mat-Su Borough Anchorage MPO (AMATS)

Coordination

Most, but not all, stakeholders identified at least some level of coordination with other organizations or providers, but instances of larger-scale area-wide coordination are rare. The most notable instances of large-scale coordination identified by stakeholders are the Central Dispatch implementation project, and some general efforts to connect service at park and rides or stops, but service connections are limited. Despite this, several stakeholders expressed major concerns about the lack of coordination, while openly noting their own lack of coordination with



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other agencies. Some even expressed that they do not really coordinate with anyone and expressed skepticism of working with others for various reasons.

Some stakeholders are coordinating on service functions, such as allowing for low conflict transfers between services. This type of coordination is not ubiquitous across the region though, and this type of coordination was identified as a need among some interviewees.

“The long-distance runs ruin the fluidity.”

There are several issues at play that are detrimental to coordination efforts:

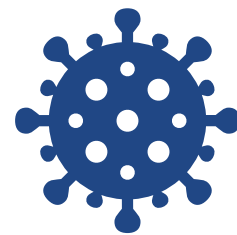
- General lack of communication amongst agencies
- Limited time and resources to work on coordination efforts
- No current incentives to coordinate (besides the potential for 5310 funding)

It should be noted, though, that any lack of coordination is not necessarily a reflection of an unwillingness to coordinate. In most cases, the relationships between organizations and key staff among are strong, and so is the desire for coordination, but the practice of coordination appears to be held back by lack of communication and lack of regional leadership.

“All things being equal, if centralized dispatch gets off the ground, it would make it easier to coordinate transportation between the providers.”

COVID-19 Impacts

While the initial impact of the COVID-19 pandemic of 2020-2021 was very similar across all organizations, the lack of coordination between services and organizations in the region quickly sent many of the service providers and organizations in very different directions. While some continued to run services at regular service levels, others reduced services and even stopped services for periods of time, while offering alternative services, such as grocery runs. Services provided by tribal nations are still quite limited. With pandemic-related emergency measures for federal programs, providers also had opportunities to use 5310 funds for vouchers, but they now must be able to track the voucher to do so. Whatever the tactic, a chief concern among stakeholders was retaining staff; however, some were forced to furlough drivers and/or dispatchers and have yet to recover from those losses.



When shutdowns began in March of 2020, nearly every organization experienced a rapid, steep decline in ridership for every service type. However, the pandemic also revealed the level to which certain communities rely on transit, as some services

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maintained relatively high levels of ridership (such as Sunshine Transit), suggesting that there were always very few “choice” riders. The pandemic also impacted how users move around spatially.

The recovery is also varied for regions and providers, as some have returned to near pre-pandemic levels, while others are still running services at 25% or less of the pre-pandemic ridership levels. The reasons for this appear to vary, but notably include lingering fears of sharing enclosed spaces, ongoing cultural conflicts regarding the wearing of masks, and reduced capacity that resulted from formal and informal mandates.

Needs, Gaps, and Barriers

The needs, gaps, and barriers discussion was quite broad, but was addressed often enough during stakeholder interviews for it to be a major theme with several subthemes, including organizational leadership, the need for a broader regional plan, school transportation, and potential census changes.



Organizational Structure & Geographic Barriers

Stakeholders expressed an absence of any organizational structure to address legislative issues. The providers also find themselves hindered by what they see as inconsistent expectations from ADOT&PF with regard to service planning and funding that prevent them from implementing much-needed service in the MSB. Lastly, planning for and measuring service performance is often challenging when, as is often the case, measures of performance are esoteric and non-specific.



The service needs of urban and rural areas are vastly different, and unfortunately these differences can be extremely spatially complex due to service and municipal boundaries and funding designations for place types. Some agencies also noted geographic barriers to service such as limited access highways (and frontage roads) that can cut off routes and cause significant disruptions to providing reliable transportation. Additionally, weather in Alaska during the winter months can present a massive challenge, especially for those areas that are rural or on tribal lands that may not have consistent snowplowing. The Borough’s lack of road powers is currently the biggest barrier related to infrastructure and maintenance. This barrier is compounded by a disconnect between land use and transportation infrastructure. The Mat-Su Borough’s lack of road powers means the entity cannot use area wide tax revenues to build transportation infrastructure, limiting its ability to plan for all modes. When projects go to the voters for permission to bond for infrastructure, the highest priority road projects typically receive funding, leaving transit infrastructure with no voice.

An additional issue, and one of the most significant, is the lack of non-emergency medical transportation (NEMT) services in the Borough. NEMT services are offered

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through a variety of means, whether by contract through a local transit provider or through a contract with a national private company (for example, Logisticare or Acadian). However, previous efforts to find a private provider for the Borough have been unsuccessful, and no local transit provider is currently providing NEMT service. As such, would-be NEMT trips fall to the local emergency service provider, holding up call lines and requiring precious emergency service provider resources. Currently, the Mat-Su Borough does not possess health powers. As such, support for NEMT type of services is quite limited from the planning organization.

Planning Needs

Planning is tied closely to rapid regional growth and potential changes related to the 2020 decennial census, outlined later in the section. A number of entities expressed the desire to plan for more long-term service needs; however, either don't have sufficient resources (i.e. time & staff), or are unclear on a regional vision for the Mat-Su Borough. Throughout the course of the conversations, there is clearly a need for strategic planning for transit service, whether for connectivity amongst various areas within the Borough or connectivity between the Borough and Anchorage. All entities expressed a continued interest in the ability to plan ahead and work with other providers to coordinate service. This is especially true of the tribal nations and health and human service providers. It is recommended that the region's providers work together to develop a 5-10-year strategic service plan.



During multiple interviews, there was quite a bit of discussion around the development of a fixed route system. Since the previous Coordinated Plan in 2018, the MSB experienced rapid growth, warranting the need for stronger public transportation services. Several social service providers expressed a need for a fixed route system in Wasilla that also connects to the government offices in Palmer. Additionally, commuter service was a major topic of discussion, particularly between the Anchorage Metropolitan Area and the MSB. Several entities expressed a need for customers to travel between both areas for employment, higher education, and medical appointments.

School Transportation

School transportation was brought up by multiple agencies as a need, whether to and from school, or for after school activities. There are several new charter schools that are starting or planned, and the charter schools do not currently have access to MSB school district transportation services. Currently, transportation to these schools is provided by parents; in some cases, adding congestion to already cramped neighborhoods. There are also planned schools on tribal lands with no transportation services associated with them. There's a great need for transportation to after school activities. Some services are currently being



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provided by local operators; however, other organizations do not help support the costs of these services, meaning a greater financial burden on the provider(s). It is recommended that this plan includes a strategy for the school district, charter schools, tribal nations, and providers work together to plan for school transportation and access.

“We’ve tried to fill a lot of the gaps, but there’s no funding to back it. Without enough funding the school programs and routes are in jeopardy.”

Decennial Census Changes

With the rapid growth in the MSB, the Borough is in the planning stages to develop the region’s Metropolitan Planning Organization (MPO), which will help further coordination and planning efforts. The results of the 2020 decennial census have determined the Palmer-Wasilla region will be urbanized as of December 2022. Several stakeholders expressed curiosity regarding the process and the desire to stay informed as changes happen.



“No kid wants to be homeless. They want to matter.”

Priority Populations

Stakeholders were asked to identify service and mobility characteristics, gaps, needs, or concerns for priority populations that they serve. Common themes that emerged include the following:

- **Several outlying areas of the region are not served by transit, and rural/tribal services do not cover enough of the whole area to compensate.** Stakeholders noted there are a lot of at-risk populations that reside within the gap between Valley Transit and Sunshine Transit service areas.
- **Ensuring priority populations are represented in community engagement activities is of particular concern,** and tribal nation representatives expressed concern over tribal elders being “left behind”, specifically. In general, there is a need to collect a wider, more representative sample of community feedback across the board in the MSB.
- **Income insecure populations of all types are at risk of service gaps.** Stakeholders noted the difficulties in providing affordable transportation because several providers have issues with staffing, education, capital, and service costs.
- **Providing services for school aged children and at-risk youth was a notable concern among several stakeholders.** They noted that the expansion of schools, particularly charter schools, is a challenge. Additionally, connectivity

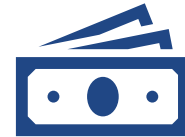


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for at-risk youth to social services, employment, and education is a critical issue.

Funding

Stakeholders noted that the lack of funding is a hinderance to providing the services needed for their respective communities. While funding is an issue across the region, the specific needs vary between organizations and providers.



Some agencies need additional funds to purchase new vehicles as they age beyond their useful life, while others are looking for funding to hire and retain drivers and expand their service. Other entities are looking for funding for service expansion and new capital projects. Agencies in rural areas with smaller service areas face an issue where their vehicles age but do not reach the miles needed to upgrade to new vehicles. On the contrary, rural agencies that serve large areas have vehicles that are driven very long distances. The stakeholders from these agencies voiced their concern over the way their revenue miles are calculated, stating that funding sources do not cover the extremely long deadhead miles that accrue when returning from these trips.

Lastly, there is the issue of local match. Currently local match is being provided by the Mat-Su Health Foundation for the service providers. With the rapid regional growth, local match should be planned for and collected other ways, especially if funding streams for transit service change. Stakeholders should consider partnerships with local municipalities and the possibility of medical service provision to serve as possible channels for local match. 5310 funding can also be used to provide match for federal funds, as long as they did not originate from DOT.

Interview Talking Points Guide

The following provides a high-level overview of the topics and questions that were covered in stakeholder interview discussions:



Getting to Know You

- Tell me about your organization and the services you provide:
- Do you currently work with other organizations (e.g., Valley Metro, AnchorRides, Sunshine, etc., to coordinate services)?



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Existing Conditions

- How transportation services within the region currently support individual or organizational interests?
- How is the transportation system working for priority populations (i.e., youth, Veterans, older adults, individuals with disabilities)?



Needs and Gaps

- Concerns of priority populations
- Barriers to improving the system and services for priority populations
- Markets that are not well served by the existing transportation system that are particularly important to serve
- Inefficiencies in public transportation or other mobility options operating in the region



Recommendations/Opportunities/Gaps

- Key players to successful transit service planning and development and/or service provision
- Opportunities to improve access to transit and mobility options
- Opportunities to make it easier and safer to access transit & stops in your community
- What could be done differently for regional (transfer) trips?
- What specific service improvements would you like to see funded?
- How would you suggest those improvements best get funded?
- Additional examples of programs, policies, or improvements which MSB should consider?
- Besides funding, what prevents transportation and mobility improvements
- Your vision for transportation in your community



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Workshop

In October 2022, the plan Advisory Committee convened at an in-person workshop facilitated by the consulting team to identify project goals and strategies. The workshop agenda included:



- **A review of the state of the region**, including demographic maps and service inventories.
- **Stakeholder feedback to date**—interview themes, and a discussion of needs and gaps.
- **A detailed discussion of proposed goals and associated strategies for the region.** The attendees agreed on the proposed strategies and subsequently gave initial prioritization rankings to the strategies.

Upon conclusion of the workshop, the consulting team summed up themes, comments, and documented the stakeholders' preferred goals and strategies. The advisory committee met two more times in November and December, respectively, to provide comments and feedback on the strategies and further prioritize the final fleshed-out strategies in November. These goals and strategies can be found in Chapter 5.

Chapter 5.

Goals and Strategies

This chapter showcase the region’s vision and mission statements, respectively, as well as “clearly articulated” goals and strategies for future implementation. Before diving into the specificities of those items, it’s important to discuss their purpose.

Vision and Mission Statements

Vision statements focus on the future. The horizon of vision statements is typically long-term; capturing what the agency(ies) would like to become over time. As such, vision statements may only need updating every 10+ years, if at all, meaning that there is little need to update the vision statement for a plan with a 5-year horizon.

Mission statements highlight how the Mat-Su Borough currently functions. Like vision statements, mission statements may only need be updated every 10+ years; unless the work of the region is so dynamic it would warrant more frequent updates to the mission statement. Mission statements typically capture the things a group or region is accomplishing to achieve their goals.

Previously, stakeholders in the Borough worked to establish agreed-upon vision and mission statements with the development of their 2018 coordinated plan update. The group reviewed the vision statement as a part of the 2022 update and determined the statements still held true for the future.

Vision Statement

A sustainable, multi-modal transportation network that effectively meets the transportation needs of Mat-Su Borough residents of all ages and abilities.

Mission Statement

To enhance mobility for senior citizens, individuals with disabilities, individuals with low incomes, and other groups lacking adequate transportation in the Mat-Su Borough through improved public transit and human service transportation coordination.

Goals and Objectives

Goals and objectives are statements that describe what the coordinated plan will accomplish as well as the overall value that coordination contributes to transportation in region. Goals are a critical component to coordinated planning, providing overall context for what the plan is working to accomplish from a regional perspective, while operating as guideposts for strategy implementation and the activities of the future coordinating committee (to be established based on strategy recommendations in the next section).

The project Advisory Committee took part in an in-person workshop in October 2022 to update and develop goals and strategies for the coordinated plan. The Advisory Committee was first asked to identify common themes around a Strengths, Challenges, Opportunities, and Threats (SCOT) analysis for the MSB, as well as a discussion of how they see coordination currently and what the stakeholders would like to see from concerted coordination efforts. Based on the themes and discussion, the team was able to flesh out five goals for the next 5-year plan horizon. The themes captured from the workshop are:

- Access
- Safety
- Collaboration
- Needs
- Education & Awareness
- Funding/Resources
- Data
- Affordability

The project team used these themes to develop goals. The goals and their descriptions are captured in the following section. The goals are listed as follows, *in no particular order, though they are numbered for easy reference*.



Goal 1: Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough

The MSB is not only growing at a rapid pace but is as large as the state of West Virginia. As the area is growing at such a rapid pace, there's a need to continually educate regional partners on coordination efforts made to date and how the partners (stakeholders) work together to problem solve Borough-wide. Additionally, there is a need to educate the public on services (transportation and human service) available to them and how to access those services.



Goal 2: Strengthen and Sustain Financial Opportunities

Sustainable funding streams will always be a challenge for providers; however, with the onset of CARES and CRSSA act funding, providers have more options, though not sustainable. Additionally, rapid regional growth emphasizes need for service planning to meet the needs of the population. Services in the Borough have been utilizing Mat-Su Health Foundation grant funding for local match, but the time

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has come for providers to plan for long-term solutions as cities in the MSB plan to transition to “urbanized” with decennial census designations.



Goal 3: Establish a Data Collection and Management Plan to Inform Future Planning Efforts

Data collection is important to all operations, but is critical to transit planning and operation, especially in a world where critics would like to equate “empty buses” to lack of success. Ironically, highways and roads are also empty during non-peak hours, but the public transportation industry always has more to prove. Data helps agencies make informed decisions, identify problems, develop strategic approaches, and makes the argument for additional funding. Data is power in a world that very much relies on information to back up plans. As such this goal captures the need to build on the current data sharing network, and develop a new, strategic plan for data collection and management.



Goal 4: Define and Address Regional Transportation Needs

Needs in the MSB include those for the providers: ongoing maintenance, operations, and capital planning; and subsequent needs of the public. Providers have worked together for over a decade to develop services and programs in probable “gaps” throughout the region; however, with rapid growth, the gaps not only grow and change, but some may move. Additionally, partners should work together to clearly understand where service gaps may be within agency service areas.



Goal 5: Support Ongoing Coordination and Collaboration, while Creating New Partnerships

Links and connections must be made throughout the region regardless of the “invisible barriers” that Borough boundaries, city limits, and transit service areas make up. Mat-Su partners have made huge strides to address connectivity, but with rapid growth, will need to continue to work together to close gaps in service for target populations, both within the Borough and between the Borough and the Anchorage Metropolitan Statistical Area.



Goal 6: Design Safe, Accessible, and Affordable Services for Borough Residents

Given the wide geographic expanse of the MSB, providers and stakeholders need to work together to plan for services that are safe, accessible, and affordable for residents. Considerations should be made for safety and accessibility at stops (shelters, walking paths, lighting, etc.), and programs should be offered for target populations for whom cost is a barrier in using public transportation services.

The development of strategies happened organically prior to the goals development at the advisory committee workshop. The consulting team first presented a list of

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strategies from the previous plan, and the group discussed which ones had been implemented, with the opportunity to determine if a strategy should be retained from the previous plan. The stakeholder group was then asked to discuss ideas and strategies they would like to implement if money were no object, with consideration for those strategies needed related to the COVID-19 pandemic.

Strategies from the 2018 Coordinated Human Services Transportation Plan

There were nine main strategies with implementation detail established in the 2018 plan. The strategies were grouped in two main categories: 1) those that would improve coordination, and 2) those that would improve services. Many of the strategies are still relevant for the 2023 plan update as well.

1. Centralize Mobility Management Services
2. Reduce Operations Costs while Maintaining Service Levels
3. Determine the Appropriate Combination of Transportation Services
4. Generate New Revenue
5. Improve Information Access & Quality
6. Improve Medicaid Approval Process for Providers & Recipients
7. Improve Affordability for Residents
8. Improve Service Availability
9. Improve Marketing

Strategies for the 2023 Coordinated Plan

Based on the previous strategies, the SCOT analysis, stakeholder interviews, and advisory committee meetings, the group worked to develop detailed strategies that would be associated with each goal. The following section outlines the goals and their respective strategies for 2023.



Goal 1: Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough

Strategy 1a: Design and Develop a Travel Training Program

Travel training programs are designed to teach people with disabilities, older adults, youth, veterans, and/or low-income populations to travel safely and independently on the range of services available within a given area.

- Travel training can include information on communicating with drivers, technology training and a review of eligibility requirements for different services.

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- Teaching people to use transportation services safely and independently can reduce the barrier of personal transportation to access resources as well as reduce congestion and traffic safety risk on the roadways.
- Travel training can dispel misconceptions and concerns about public transportation services among youth and caregivers of youth, including youth with special needs and/or that speak languages other than English, and build confidence in how independent travel can increase youth access to opportunity such as before/after school programs and employment.

Strategy 1b: Engagement Planning for Local Governments

Elected officials can be helpful advocates for public transit plans and funding; however, these positions often turn over frequently, as do positions within municipalities. There is a continual need to educate municipal staff, elected officials, and other official positions on the purpose and value of public transportation options.

- MSB stakeholders identified the need to develop messaging on how the local community and its citizens benefit from public transportation.
- This strategy will include an additional objective that stakeholders identified during the workshop: “compiling return on investment information for elected officials and strategy makers.” Stakeholders feel it’s important to pull together data related to return on investment in transit. Conveniently, the group will be able to use the recently released report that discusses the economic impacts of transit in Alaska, that was highlighted in the plans section of this document.

Strategy 1c: Borough Listening Sessions/Town Halls

Stakeholders identified a need for the community to have an outlet to discuss needs related to transportation and mobility.

- “Listening sessions”, and/or town halls allow community members the opportunity to hear about upcoming plans related to public transit in the Borough, and comment on those upcoming plans. Additionally, town halls allow these same community members to address topics and concerns related to transportation in the region.
- These type of outreach events are mutually beneficial, wherein stakeholders may listen and learn from the community, and citizens may learn about upcoming plans. These events also allow for public trust and transparency to be established.

Strategy 1d: Establish a Formal Marketing Campaign on Transportation Resources in the Mat-Su Borough

People who reside in, work in, and visit the MSB may be unaware of both what transit services are available and for which services they may be eligible. The distribution of consumer-friendly, accessible educational materials can help to increase public awareness of services.

- Some of the service providers already distribute educational materials through various forms of media, but a next step may involve a coordinated public awareness campaign targeting at-risk populations in the region.

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Strategy 1e: Develop Consistent Online Resources

This strategy is comprised of two objectives highlighted in the stakeholder workshop: ensuring an online (one-click) resource for transportation and mobility is developed for the MSB, and compiling a “map viewer”, wherein individuals can search and find available transit services, walking paths, accessible paths to bus stops, and bike routes

- Ensuring a consistent online resource will allow individuals to further educate themselves on available services and will allow providers to develop a keen understanding on true “gaps” in mobility and access.

Strategy 1f: Support Access to Existing Community Services (libraries, drug testing, food pantries, etc.) by Hosting Informational Webinars, Meetings, and Providing Leave-behind Materials Informing the Community how to Use Public Transportation

This strategy can be an off shoot of the formal marketing campaign, detailed above, but is called out because it can be implemented immediately, with or without a specific marketing campaign.

- Stakeholders can collaborate with partner organizations, non-profits, and social services to identify opportunities for coordinated engagement and production of educational materials.
- Online and paper surveys can be regularly distributed to gauge public awareness and interest in transportation services to identify opportunities for improvement.

Strategy 1g: Develop Educational Materials on all Mobility Options in the Region (not only public transportation)

The citizens of the MSB would benefit from general information on all transportation options available to them.

- While taxi and for-profit providers are limited in the region, these services do exist; albeit not widely advertised. Additionally, individuals may benefit from information related to airport and commuter services, as well as options for tourists.



Goal 2. Strengthen and Sustain Financial Opportunities

Strategy 2a: Increase Resources for Local Match

As the region grows and potentially develops into a more “urbanized” area, providers will not only need to re-evaluate the funding they are eligible for, but how local match is allocated.

- In larger and growing urban areas, local match for federal funding streams is typically contributed through the municipality that is being served, and sometimes with a vote for a portion of tax (sales, gas, or other).
- Transit providers in the MSB are currently being supported with grants through the Mat-Su Health Foundation but need to expand local match options as the region changes and grows.
- 5310 funding can also be used as match funding in specific cases, including federal grant funding, as long as the original funding does not originate from DOT

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Strategy 2b: Develop a System to Identify and Promote Funding Opportunities for Regional Providers and Programs

Stakeholders expressed a need to better understand funding opportunities available to them. It would be helpful to have a lead agency, like the MSB (or future Metropolitan Planning Organization), establish a database for available funding streams, application processes, and timelines for securing funding for current and new public transit and mobility projects.

Strategy 2c: Grant Writing Assistance Program

Grant writing is a skilled activity that requires technical knowledge as well as storytelling ability. It is a time-intensive activity that generally occurs in cycles. It is harder for under-resourced agencies to respond to grant opportunities or submit competitive applications because of the investment of time and resources needed to do so.

- The MSB can develop a grant writing technical assistance program for under-resourced agencies to support the development of competitive applications, such as support with crash analysis, mapping/GIS analysis, graphics, and proposal narratives.
- The MSB can also help build (and/or support) agency capacity to respond to grant opportunities themselves and through offering trainings on grant program requirements, statutes, cycles, and analysis processes for local partners.

Strategy 2d: Continue to Support the Mat-Su Borough's Development of a Regional Metropolitan Planning Organization

During plan implementation, lead and support organizations will use the proposed performance measures associated with each strategy—and/or new ones that emerge—to establish a baseline.

- The baseline may be as simple as a “yes, this item was completed” or “no, the item was not completed” or may be a number or percentage associated with the strategy itself. At this time, the lead and support agencies will be responsible for measuring performance, with MSB oversight (and eventually MPO oversight) for final reporting purposes.



Goal 3: Establish a Data Collection and Management Plan to Inform Future Planning Efforts

Strategy 3a: Expand and Utilize Current Data Sharing Plan

The service providers in the region currently have a more “informal” data sharing plan based on need. The providers share information (trip needs, ridership information) to loosely track service demand.

- The providers would like to expand the tools and methods they use to share data currently to make better informed decisions for the region.

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Strategy 3b: Analyze Travel Patterns and Regional Demographics to Better Understand Gaps in Service Areas

There is a wide range of travel patterns in the MSB by virtue of the region being so expansive.

- Collecting information related to travel and trip patterns would help providers better plan for (and provide) much needed service and assist in “making the case” for more service and funding streams.
- Understanding travel patterns also benefits the local community so that transit and planning agencies can ensure the right type of service is offered in the right places.

Strategy 3c: Develop a Regional Data Management Plan

Stakeholders expressed a need to develop a more formalized data management plan for the MSB. The plan would help providers with consistent data collection, provide guidelines for updating and managing Borough-wide data, set reporting requirements, including regular timelines for uploading and sharing information.

- A data management plan may also be used to develop a more extensive transportation database, with an up-to-date vehicle inventory, that could pave the way for a Borough-wide asset management plan.



Goal 4: Define and Address Regional Transportation Needs

Strategy 4a: Develop a Borough-wide Transit Development Plan

Stakeholders have collectively expressed a need for a transit development plan for the MSB. Transit Development Plans (TDPs) can help regions plan for medium- and long-term transit needs.

- TDPs can include passenger needs surveys, detailed ridership data collection and analysis, trip patterns and analysis.
- In the case of the MSB, a TDP can help navigate the potential impending changes to the urbanized area and prevent lapses in funding for providers by setting forth a detailed plan.

Strategy 4b: Mat-Su Borough Leverages Agency Leadership to Emphasize Transit Needs

This strategy, which is a component of the formal marketing plan, relies on agency leadership (assembly members and other elected officials) to advocate for transit needs in the Borough. Funding for transit and transit projects is highly competitive at the state level and having advocacy through local leadership will help advance transit projects and much needed funding for transit in the Borough.

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Strategy 4c: Identify “Need” to Determine if the Need can be Fulfilled by Existing Service or Whether the “Need” Requires New Service through Formula 5310 Funding

This strategy drills down into services currently provided in the region overlaid with the needs of the target populations for 5310 funding (older adults, individuals with disabilities, other politically and socially marginalized populations) to determine various transit needs in the Borough and whether those needs can be met with a current service provider.

- If the need is truly a “gap”, in that there is no service to service that newly identified “need”, then the Borough should have a plan in place for ranking order of magnitude needs for new 5310 service. Having this needs strategy in place is a good means to start, especially in the strategy prioritization section.

Strategy 4d: Develop Driver Training and Retention Programs

As noted earlier, the MSB is facing a shortage of drivers. Beyond the steps already taken by school districts and providers to incentivize recruitment, other considerations, such as cash referral bonuses, paid Commercial Driver’s License (CDL) training, and time retention bonuses may help support longevity.

- Transit operators can convene with other agencies and unions in the Borough and State to review existing pay, benefits, and licensing requirements to identify opportunities for improvement.
- Agencies and unions should also collaborate to find opportunities to add incentive pay not just for newly hired drivers, but for drivers willing and able to take on more challenging assignments and routes or those who have been resilient and continued to drive throughout the pandemic.
- Programs which recognize drivers, dispatchers, and other customer-facing personnel will help improve the sense of community ownership and morale on board the coordinated transit system. All transportation providers – public, private, and non-profit – can incorporate driver recognition as part of marketing and public-facing materials through calling attention to individual drivers’ stories and establishing an email address or hotline requesting individual commendations from riders. February is Love the Bus / School Bus Driver Appreciation Month, so it is a good month to target for appreciation activities.



Goal 5: Support Ongoing Coordination and Collaboration, while Creating New Partnerships

Strategy 5a: Implement Borough-wide Mobility Management Program

Mobility management can be broadly defined as creating and managing mobility options, at both the systemic and system-to-customer levels, to improve the reach, efficiency, and affordability of public transportation services.

- Transportation impacts every piece of individual’s daily lives, and through the development of a mobility management network, regions and providers can increase efficiency and effectiveness in service operations and further coordination efforts.
- The development of a mobility management network starts with bringing together transportation and mobility providers, other health and human service agency staff, decision-

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makers, community members, and riders/customers, which will allow for the exchange of ideas and development of solutions and strategies that will address transportation-related barriers.

- It is important to note here that Connect Mat-Su (www.connectmatsu.org) currently operates as the only mobility management-type operation in the Borough. Connect Mat-Su is a new organization, and according to their information can assist with paying for transportation, exploring public transportation options, navigating medical transportation and help with school transportation options. Connect Mat-Su was established by the Mat-Su Health Foundation with support from community partners to help with mobility options for the residents of the MSB.

Strategy 5b: Formalize Agreements and Processes for Leveraging Funding Services and Planning

Tribal communities emphasized interest in better coordinating agreements between entities (i.e. tribal nations and the Alaska DOT&PF, tribal nations and the Borough, tribal nations and providers, and between the providers and the Borough).

- These more formal agreements not only signify a willingness to coordinate and collaborate but allow for exchange of ideas and agency resources (and potentially funding) for transit planning and services.

Strategy 5c: Develop Borough-wide Coordinating Committee

Most Coordinated Human Service Transportation Plans are “successful”, because upon adoption, the agencies involved in plan development continue to regularly meet for implementation updates.

- Coordinating Committees typically consist of the same stakeholders involved in the advisory committee for the plan and meet regularly (monthly, bi-monthly, quarterly) to discuss strategy implementation champions and status updates for all the providers.
- Some Coordinating Committees have “working groups” that meet at higher frequencies and consist of those agency representatives that are champions for a particular strategy(ies), or with those agencies who have decision-making powers. Example working groups include executive committee, marketing committee, funding and grants committee, etc.

Strategy 5d: Develop Partnerships for those Non-profit Agencies who may Need Support with Vehicle Maintenance

This is especially apparent as the region works to recover from the COVID-19 pandemic as there are not enough resources needed to perform daily functions as they existed prior to March 2020. Limited resources (drivers, funding, vehicles, etc.) affect the continuation of a diverse array of services, including commuter service, the development of more demand response service, and special education transportation.

- Non-profit organizations that provide public transportation services may consider partnering on vehicle maintenance services, whether through a singular location or sharing of mechanics willing to travel throughout the Borough.

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Strategy 5e: Coordinate with Critical Health and Social Services to Better Provide Consistent Transportation for those who Rely on the Service(s)

Due to the lack of non-emergency transportation service (NEMT) in the MSB, at-risk community members may struggle with how to access critical services, including dialysis, mental health services, drug testing facilities, etc.)

- Coordinating with the operators of these health and social services, as well as those agencies involved in the prisoner re-entry programming, would vastly help ensure that community members have the transportation needed to access critical care.
- Once a clearer picture is developed of the demand for health and social services, providers can work to establish more consistent transportation service offerings and/or consistent service times.



Goal 6: Design Safe, Accessible, and Affordable Services for Borough Residents

Strategy 6a: Develop a Program for Discounted Fares for Older Adults and Individuals with Disabilities

Various programs may be developed to expand the affordability of transit for target populations. In other areas, “ride free” programs may be developed as a pilot with grant funding. In some cases, funding is provided with local sales tax and grant funding with municipalities eventually taking over long-term funding.

- In other areas, transit providers partner with non-profit organizations to fund monthly passes that are distributed directly to the individuals and families that need them.
- Multiple conversations with youth groups expressed interest in expanding or maintaining affordable public transportation, given the other burdens families face in the rising regional costs of housing.

Strategy 6b: Upgrade Facilities and Bus Stops and Transfer Stations

It goes without saying that the climate in Alaska can be harsh, particularly during the winter. Providing consistent shelters, lighting, and accessible ramps at bus stops and transfer points would help passengers feel safer riding local public transit services.

- Providers could start by documenting stop locations and the “amenities” (bench, shelter, flag sign) at those stops, targeting higher ridership locations as priority for upgraded amenities.

Strategy 6c: Further Identify Public Transportation Infrastructure Needs in the Borough

This strategy is a recommendation for a larger infrastructure plan in the MSB that looks at all transportation infrastructure (bus shelters, accessible ramps to bus stops and transfer points, bike racks, lighting, benches, etc.).

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- Having an infrastructure plan would help prepare the region to receive funding for infrastructure, should it become available for “shovel ready” projects, and would help the providers allocate funding based on order of magnitude.
- Infrastructure plans can also detail the balance of amenities in urban versus rural areas, as rural needs have been identified multiple times throughout the course of the coordinated planning project.

Strategy 6d: Work with the Alaska DOT&PF to support the Borough’s Level of Autonomy over Road Clearing During Winter Months

All transit providers, and the Borough, emphasized a great need to have better road clearing during the winter months. The challenge is that roads in the Borough (like so many other places) have various jurisdictions: cities, Borough, state, tribal nations, etc.), and the standards for clearing the roads are inconsistent.

- Providers, like Valley Transit, provide commuter service between the Borough and Anchorage; however, several of the stops are on roads managed by the state, and those transfer points are inconsistently cleared, meaning access to the bus stop is a challenge.
- Providing the Borough with road-clearing powers, and the funding to clear the roads for additional resources, would mean that the roads transit providers operate on are more consistently cleared for customers.

Strategy 6e: Develop and Support Borough-wide Technology Measures for Customers and Providers

Stakeholders felt the need to capture a separate strategy to address lack of technology throughout the Borough; as such, this strategy was added after thoughtful discussion and consideration. Since this strategy was not originally included in prioritization, it will be given an “unranked” qualifier. It will be at the discretion of the advisory committee to determine where this strategy later ranks upon plan adoption and implementation.

- Transit providers in rural areas, particularly Sunshine Transit and Chickaloon, expressed a need for customers to have better access to technology to book rides. Stories were shared of multiple customers that don’t have access to mobile phones, meaning that if they want to book a ride, they have to go to a local social services center or community center to use a phone to book a ride.
- Internet is spotty and limited in certain areas of the Borough, particularly rural areas. Several states are working on 5G initiatives wherein mobile service and cellular towers will be improved statewide.
- Providers also expressed an interest in next bus information at stops and transfer locations. Next bus technology is possible when services operate on a more consistent schedule. This would allow individuals who have limited or no access to technology to better understand wait times for their ride(s).

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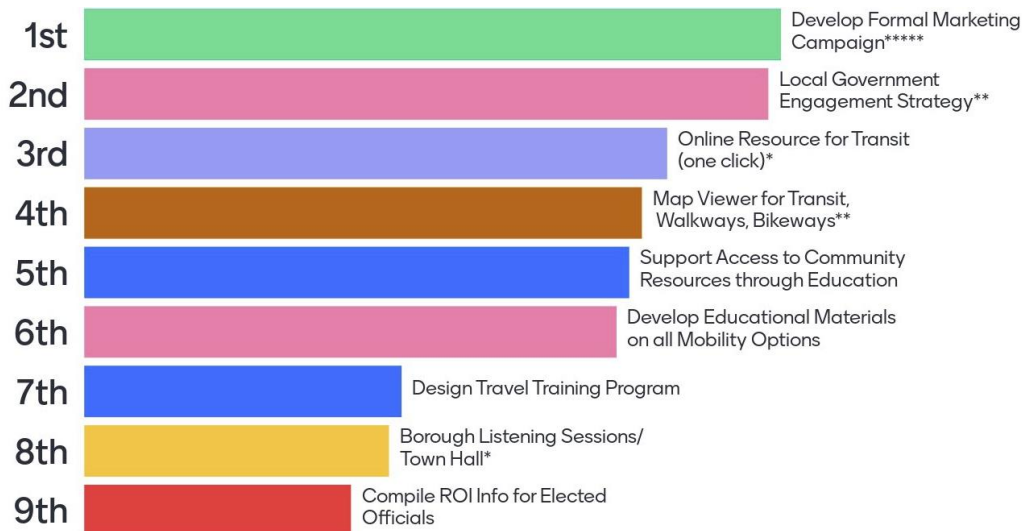
Strategy Prioritization and Plan Implementation

In November 2022, the Advisory Committee met to prioritize the strategies that had been aligned with each goal. The strategy prioritization aided the project team in assigning timelines to the individual strategies as well as potential funding sources. For each goal, the committee ranked the strategies from highest to lowest priority. It is important to note that just because a strategy received a lower ranking does not mean that it is less important than the other strategies within that goal. Higher prioritization was given to those strategies that the committee wished to focus on first. This next section provides a review of the strategy prioritization that took place in real-time at the November advisory committee meeting. *(Note: the wording (but not the meaning) of some strategies has changed slightly since they were ranked as illustrated below.)*



Goal 1 Strategy Prioritization

Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough



Goal 2 Strategy Prioritization

Strengthen and Sustain Financial Opportunities



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Goal 3 Strategy Prioritization

Establish a Data Collection and Management Plan to Inform Future Planning Efforts

- 1st Analyze Travel Patterns & Demographics to Better Plan for Gaps
- 2nd Develop Regional Data Management Plan For Sharing & Collection
- 3rd Expand & Utilize Data Sharing Plan****



Goal 4 Strategy Prioritization

Define and Address Regional Transportation Needs

- 1st Develop Borough-Wide Transit Development Plan
- 2nd Identify "Need" to Determine if it Can Be Fulfilled with Current Service or If New Funding is Needed
- 3rd MSB Leverages Agency Leadership to Emphasize Transit Needs*****
- 4th Develop Driver Training and Retention Programs



Goal 5 Strategy Prioritization

Support Ongoing Coordination and Collaboration, while Creating New Partnerships

- 1st Develop Coordinating Committee*****
- 2nd Formalize Agreements & Processes for Sharing Funding & Planning Services****
- 3rd Implement Mobility Management Program****
- 4th Partnerships & Support for Administrative Staff Shortages
- 5th Develop Partnerships for Non-Profits who Need Vehicle Maintenance Support
- 6th Coordination with Dialysis Centers and other Critical Services

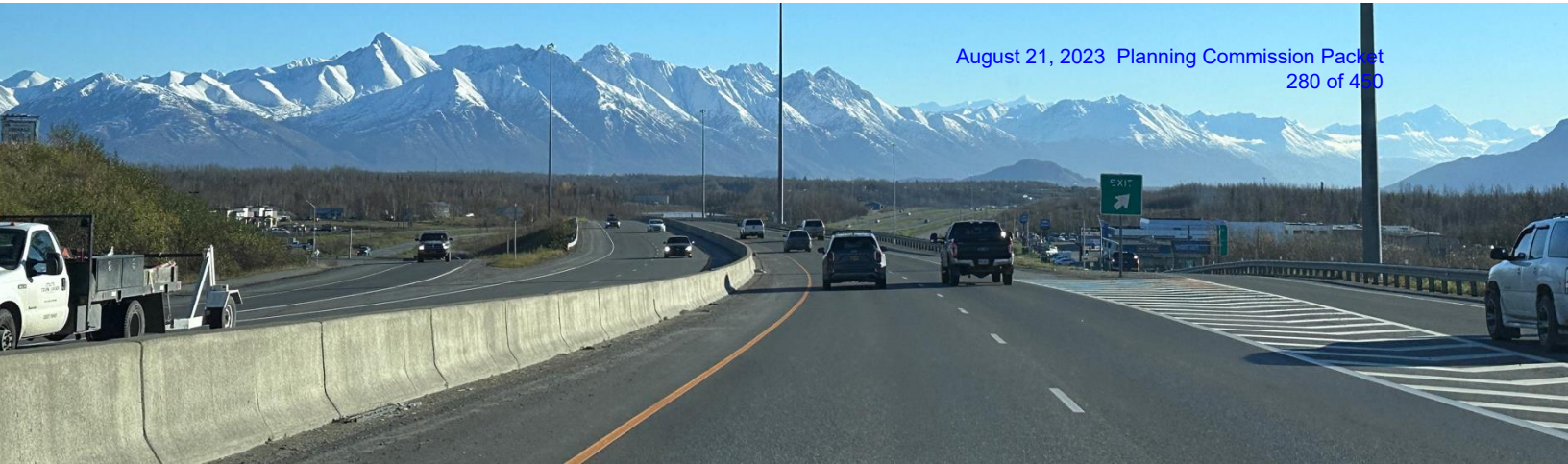
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Goal 6 Strategy Prioritization

Design Safe, Accessible, and Affordable Services for Borough Residents

- | | | |
|-----|-------------|---|
| 1st | <div></div> | Further Identify Public Transportation Infrastructure Needs for Rural Areas |
| 2nd | <div></div> | Discounted Fare Program for Older Adults & Individuals w/ Disabilities*** |
| 3rd | <div></div> | Shelter & Facilities for Bus Stops* |
| 4th | <div></div> | Work with DOT to Ensure Borough has Autonomy over Road Clearing in Winter |



Chapter 6.

Plan Implementation and Funding Source

The proposed strategies presented in Chapter 5 of this plan are intended to meet a series of needs uncovered throughout the engagement process; they also include guidance on how they can be implemented. The proposed strategies in this chapter are categorized by goals and then are split further into their prioritization tiers.

After listening to feedback from both the Advisory Committee and stakeholders, the proposed strategies in this plan are prioritized by placement on one of the following order of magnitude categories. Implementation timelines will be associated with each strategy as well. Some strategies may be ready for immediate implementation, whether ranked high or low priority. Other strategies, while ranked “high priority” may take longer to implement. Project timelines range from short (1-2 years) to medium (3-4 years) to long (5+ years).

- **HIGH PRIORITY** - To begin meeting project goals and closing needs, the Mat-Su Borough should consider prioritizing several basic investments and programs in coordination with regional stakeholders. The measures included in the high priority tier are those which have been deemed important by the Advisory Committee in the next couple of years.
- **MEDIUM PRIORITY** - The impacts of these strategies are also consequential, but they are not the highest priority. Some strategies under the Medium Priority tier may also benefit from High Priority strategies being implemented. For example, a pilot flexible transit route may be more successful if there is already an understanding of which communities would be most likely to ride such a service, along with more direct in-person marketing of transit options.
- **LONG-TERM PRIORITY** - Included in the proposed strategies are proposed policies that address larger ongoing challenges, for example, the impacts of census changes and the legacy of systemic discrimination. These proposed policies are given a long-term timeframe, as they will require consideration in the context of all future transportation decisions.

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The project team used the strategy rankings to create the implementation plan for the region. The implementation plan depicts strategies with their goals, proposed implementation agency (or team), proposed time to implement, and the priority assigned to the strategy. Based on the prioritization activity captured in Chapter 5, strategies that were listed in the top two spaces are “high” priority, strategies in the 3rd and 4th spaces are “medium” priority, and strategies in the 5th ranked spot and greater are “low” priority.

Using the advisory committee rankings, the proposed timeline for implementation, and the ease of implementation, each strategy is then given an “overall ranking”, in numerical order, for implementation. Other factors, such as funding availability, may impact a strategy’s overall ranking and whether it gets implemented sooner rather than later.

Figure 6-1 Proposed Implementation Timeline

Goal	Strategy	Timeline	Priority	Overall Ranking
	GOAL 1: Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough			
	1.a Design and Develop a Travel Training Program	1-2 years	Low	7
	1.b Engagement Planning for Local Governments	1-2 years	High	2
	1.c Borough Listening Sessions/Town Halls	1-2 years	Low	5
	1.d Establish a Formal Marketing Campaign on Transportation Resources in the Mat-Su Borough	1-2 years	High	1
	1.e Develop Consistent Online Resources	3-4 years	Medium	4
	1.f Support Access to Existing Community Services (libraries, drug testing, food pantries, etc.) by Hosting Informational Webinars, Meetings, and Providing Leave-behind Materials Informing the Community how to Use Public Transportation	1 year or less	Medium	3
	1.g Develop Educational Materials on all Mobility Options in the Region (not just public transportation)	1-2 years	Low	6
	GOAL 2: Strengthen and Sustain Financial Opportunities			
	2.a Increase Resources for Local Match	3-4 years	High	2
	2.b Develop a System to Identify and Promote Funding Opportunities for Regional Providers and Programs	1-2 years	High	1
	2.c Grant Writing Assistance Program	3-4 years	Low	4
	2.d Continue to Support the Mat-Su Borough’s Development of a Regional Metropolitan Planning Organization	3-4 years	Medium	3

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Goal	Strategy	Timeline	Priority	Overall Ranking
	GOAL 3: Establish a Data Collection and Management Plan to Inform Future Planning Efforts			
3.a	Expand and Utilize Current Data Sharing Plan	1-2 years	Low	3
3.b	Analyze Travel Patterns and Regional Demographics to Better Understand Gaps in Service Areas	3-4 years	High	1
3.c	Develop a Regional Data Management Plan	3-4 years	Medium	2
	GOAL 4: Define and Address Regional Transportation Needs			
4.a	Develop a Borough-wide Transit Development Plan	1-2 years	High	1
4.b	Mat-Su Borough Leverages Agency Leadership to Emphasize Transit Needs	1-2 years	Medium	2
4.c	Identify “Need” to Determine if the need can be Fulfilled by Existing Service or Whether the “Need” Requires New Service through Formula 5310 Funding	3-4 years	Medium	4
4.d	Develop Driver Training and Retention Programs	3-4 years	Low	3
	GOAL 5: Support Ongoing Coordination and Collaboration, while Creating New Partnerships			
5.a	Implement Borough-wide Mobility Management Program	3-4 years	Medium	3
5.b	Formalize Agreements and Processes for Leveraging Funding Services and Planning	1-2 years	High	2
5.c	Develop Borough-wide Coordinating Committee	1-2 years	High	1
5.d	Develop Partnerships for those Non-profit Agencies who may Need Support with Vehicle Maintenance	5+ years	Low	5
5.e	Coordinate with Critical Health and Social Services to Better Provide Consistent Transportation for those who Rely on the Service(s)	3-4 years	Low	4
	GOAL 6: Design Safe, Accessible, and Affordable Services for Borough Residents			
6.a	Develop a Program for Discounted Fares for Older Adults and Individuals with Disabilities	1-2 years	High	1
6.b	Upgrade Facilities at Bus Stops and Transfer Stations	5+ years	Medium	4
6.c	Further Identify Public Transportation Infrastructure Needs in the Borough	3-4 years	High	2
6.d	Work with the Alaska DOT&PF to support the Borough’s level of Autonomy over Road Clearing During the Winter Months	1-2 years	Low	3
6e.	Develop and Support Borough-wide Technology Measures for Customers and Providers	3-4 years	Unranked	—

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Based on the strategy rankings, the highest priority strategies for the region to tackle upon adoption of this Coordinated Plan are as follows:

- 1.4 Establish a Formal Marketing Campaign on Transportation Resources in the Mat-Su Borough**
- 2.2 Develop a System to Identify and Promote Funding Opportunities for Regional Providers and Programs**
- 3.2 Analyze Travel Patterns and Regional Demographics to Better Understand Gaps in Service Areas**
- 4.1 Develop a Borough-wide Transit Development Plan**
- 5.3 Develop Borough-wide Coordinating Committee**
- 6.1 Develop a Program for Discounted Fares for Older Adults and Individuals with Disabilities**

It is recommended that the newly formed coordinating committee implement strategies according to their priority rankings. These first six strategies are simply the highest-ranked initial steps the stakeholders in the Borough must take to become better coordinated.

The project team worked with the advisory committee to discuss which agencies would support the implementation of each strategy once the plan is approved. Some agencies volunteered to lead the implementation of strategies; others offered general support toward the implementation of the strategy. The team created a table with a live online link so that agencies could “volunteer” by signing up to be a strategy champion. The stakeholders did not finish the sign-ups prior to the completion of this plan, so the Strategy Champions document will continue to be a working document on a shared drive for the Mat-Su Borough and the coordinating committee to continue to use for tracking and amendment purposes.

Potential Funding Sources for Strategy Implementation

It is generally understood with Coordinated Human Service Public Transportation Plans that the purpose of developing and prioritizing strategies is to rank those coordination strategies that should receive Federal Formula 5310 funding for older adults and individuals with disabilities. However, coordinated plans are often written in such a way to benefit more marginalized populations than just older adults and individuals with disabilities, and there’s still a good deal of coordination that needs to occur and limited 5310 funding. As such, it is important to point out that there are multiple possible funding sources entities could tap into for strategy implementation. This next section highlights various types of funding sources at a high level, with a full listing of possible funding sources in the appendix of this plan.

Federal Transportation Funding

On March 15th, 2022, President Joe Biden signed a \$1.5 trillion spending bill to fund the federal government for the remainder of fiscal year 2022, ending on Sept. 30. The following information was taken from the National Conference on State Legislatures; the below amounts have been appropriated for transportation programs:

Just over \$100 billion for federal transportation programs—a total of \$140 billion, a 60% increase, when adding the FY 2022 appropriation provisions contained within the Infrastructure Investment and Jobs Act (IIJA). The omnibus fully implements program authorized levels in the infrastructure bill.

- \$61 billion for federal highway investments, along with \$9.5 billion from the infrastructure bill for an FY 2022 total of \$70.5 billion, a 44% increase over 2021.
- \$16.3 billion for public transit, an increase of \$3.3 billion from FY 2021; when combined with the infrastructure bill, public transit funding totals \$20.5 billion in FY 2022, an increase of \$7.6 billion (58%).

The Federal Transportation Administration (FTA) has allocated a significant amount of funding to public transportation planning, service, and operations. Some of those funding categories are outlined in the state transportation funding section, below; however, it is important to note that these funding allocations should be tracked accordingly.

State Transportation Funding

The Division of Statewide Planning and Program Development of the Alaska Department of Transportation and Public Facilities (ADOT&PF) administers federal transit funding to those providers and operators within the legal requirements of the FTA. DOTs typically align their funding allocations with the preparation of Transportation Improvement Plans (TIPs) and State Transportation Improvement Plans (STIPs), approved by planning organizations, such as MPOs. The following federal formula transportation definitions are adapted from the FTA:

- **FTA Section 5307** - Mass transit apportionment to urbanized areas based on population, population density and operating performance. The department has authority over the distribution of funds to urbanized areas with a population of less than 200,000. Once an area becomes “urbanized”, the DOT may allocate funding through a local designated recipient, which would be responsible for reporting to the FTA.
- **FTA Section 5309** - Mass transit discretionary funds for capital projects only. The presence of an identifier number in the project description indicates the transit agency has received the funds requested. Otherwise, the numbers shown in each fiscal year simply reflect needs as perceived by the requesting agencies and operators. Funding for the following programs is constrained to the FTA’s published estimates of future funding levels.

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- **FTA Section 5310** - Provides federal funds to public and private nonprofit entities for the transportation of elderly individuals and/or individuals with disabilities. Allocation of funding is determined by locally determined strategy prioritization in an adopted coordinated plan.
- **FTA Section 5311** - Provides funds for Rural Transit Programs. ADOT&PF currently does not receive enough 5311 funding to support all the would-be rural transit operators in the state. As such, a provider must be registered with the state in order to receive some share of 5311 funding.

Other Funding Sources

NADTC Funding and Community Grants

There are other funding sources available for transportation planning and services for marginalized populations at a national level. The National Aging and Disability Transportation Center (NADTC) issues annual RFPs for a variety of transportation grants for older adults and individuals with disabilities. In 2021, NADTC announced a new funding opportunity, Equity and Accessibility: Transportation Planning Grant Program. Grant opportunities through NADTC are typically announced in late Spring and awarded in August-September each year.

NADTC also offers community grants that are designed to help communities assess transportation needs. The grants can assist with the development and implementation of innovations and new models for increasing the availability of accessible transportation services for older adults and individuals with disabilities. Grants may also help make effective use of Section 5310 Enhanced Mobility of Seniors and Individuals with Disabilities funds.







National Center for Mobility Management

The National Center for Mobility Management (NCMM) is funded by the FTA and housed at the Community Transportation Association of America (CTAA). The NCMM operates as a national technical assistance center and provides grant opportunities to support partnerships. The entity has provided multiple community planning grants to further the goals of the Transit and Health Access Initiative.

The following table, organized by goals for the MSBs Coordinated Plan, highlight some proposed funding streams that could be utilized for strategy implementation. This list is by no means comprehensive and is subject to change through funding allocations. Additionally, the list should not be limited to the sources suggested. Other funding opportunities may be available, such as emergency planning and preparedness, and transportation funding for special populations, such as Veterans and Tribal Transit Programming.

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Figure 6-2 Proposed Funding Streams

	GOAL 1 Develop a Comprehensive Plan for Communication, Education, and Awareness Throughout the Borough
Potential Funding Sources	<ul style="list-style-type: none"> • In-kind agency assistance • 5310 funding • NCMM Grants
	GOAL 2 Strengthen and Sustain Financial Opportunities
Potential Funding Sources	<ul style="list-style-type: none"> • 5310 funding • In-kind agency assistance • 5303/5304 planning assistance
	GOAL 3 Establish a Data Collection and Management Plan to Inform Future Planning Efforts
Potential Funding Sources	<ul style="list-style-type: none"> • 5310 funding • 5311 funding • In-kind agency assistance • NADTC planning assistance
	GOAL 4 Define and Address Regional Transportation Needs
Potential Funding Sources	<ul style="list-style-type: none"> • 5303 funding • 5310 funding • NADTC planning assistance • NCMM community grants
	GOAL 5 Support Ongoing Coordination, Collaboration, while Creating New Partnerships
Potential Funding Sources	<ul style="list-style-type: none"> • 5307 funding • NADTC planning assistance • NCMM community grants
	GOAL 6 Design Safe, Accessible, and Affordable Services for Borough Residents
Potential Funding Sources	<ul style="list-style-type: none"> • NADTC planning assistance • 5310 funding • Emergency management funding (ARPA, etc.) • 5303 funding

Chapter 7.

Looking Ahead/Conclusions

This chapter looks ahead to key considerations that will likely have an impact on this Coordinated Plan in the future, and public engagement planning for the Borough.

Chapter 7 focuses on future considerations:

- **Annual Reporting on the Coordinated Plan:** How can the advisory committee and the MSB provide regular updates on Coordinated Plan progress?
- **Lead Agency(ies) for Implementation:** Who will report on strategy progress? How will this information be communicated?
- **U.S. Census Updates:** Urbanized area census determinations came out in December 2022. The U.S. Census Bureau determined that, due to rapid growth around the cities of Palmer and Wasilla, the area should be designated as urbanized.
- **Transit Development Planning:** While future transit planning is captured in the strategy section, it goes without saying that a detailed transit plan for the Borough is long overdue. The high percentages of marginalized populations compared to regional peers puts a fine point on the need for more local transit investment and planning.
- **Linking all mobility options in the Borough:** The MSB is currently working on the development of a Borough-wide Bike & Pedestrian Plan. Linking this plan with future transit planning is critical for Borough residents, particularly those who need better access to bus stops and transfer centers.
- **Engagement Planning:** As the Borough continues to grow and the stakeholders begin implementing elements of the Coordinated Plan, engagement planning should be a consideration. This Chapter includes an engagement plan to get things started.



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Annual Reporting on the Coordinated Plan: State of Coordination in the Mat-Su Borough

Provide regular Coordinated Plan updates to stakeholders.

Regular progress updates on the Coordinated Plan are important for stakeholders, such as boards of directors, city councils, Borough assembly, and health and human services leadership.

These updates should occur on an annual basis and provide a sort of “state of coordination” report for the Borough. The update can present dashboards showing initial baseline performance metrics for the priority strategies for year one; followed by performance metric reporting each year that follows. This will help keep stakeholders apprised and build rapport and trust, leading to greater future investments.



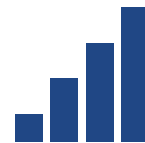
Lead Agencies for Implementation

Lead and support organizations will be responsible for measuring performance.

The project advisory committee met in two subsequent meetings in November and December of 2022 to discuss final strategy development and proposed lead organizations for each strategy.

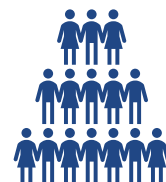
The project team worked with the committee to create an online matrix for keeping track of the proposed lead and support organizations that would help with strategy implementation. The matrix is a live, working document, so it is not included as a part of this report. Committee members, in conjunction with the Borough, will continue to have access to the matrix as they establish implementation working groups.

During implementation, lead and support organizations can utilize performance measures to establish a baseline. The baseline may be as simple as a “yes, this item was completed” or “no, the item was not completed” or may be a number or percentage associated with the strategy itself. At this time, the lead and support agencies will be responsible for measuring performance, with MSB oversight for final reporting purposes.



Updates from the U.S. Census Bureau

The MSB has a population of more than 108,000, and the population of Wasilla is over 10,000 residents and is the largest city in the Borough. Palmer’s population is over 7,000 residents. The region has experienced significant growth over the last decade and the U.S. Census Bureau has determined (as of December 2022) that the rapid growth of the Borough has pushed areas in the region into an ‘urbanized’ designation from a ‘rural’ one, which will impact



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funding for public transit. As such, the Borough is currently undertaking the process to develop a stand-alone Metropolitan Planning Organization (MPO) for the area, which will help further coordination and planning efforts.

Transit Development Planning

As evidenced in Chapter 3, there have been multiple plans developed for the MSB and surrounding areas over the last decade. However, none of these plans are specific to transit planning and development. The region needs a plan that addresses specific, realistic transit planning needs, as well as a market analysis, and transit implementation scenarios coupled with costs. The MSB is currently in the process of putting together funding for such a plan, and the need is certainly greater with rapid regional growth and the potential for census changes on the horizon.



Linking Borough-Wide Mobility Options

According to the Borough website, the purpose of the MSB Bike and Pedestrian Plan (BPP) is to develop a detailed and prioritized list of bike and pedestrian infrastructure throughout the MSB that, when built, will increase safety and connectivity while promoting a healthier and more active lifestyle for Borough residents. The BPP will identify connectors between urbanized areas, recreation areas, and the backcountry so that MSB residents can safely and efficiently enjoy all that the region has to offer. In addition to these advantages, bike and pedestrian routes can effectively connect individuals to transit stops and centers, particularly ensuring accessibility and safety. The goals of the BPP are as follows:



The goals of the BPP are:

- **Inventory and document the bike and pedestrian network** to identify gaps and deficiencies
- **Review MSB Code, the MSB Subdivision Construction Manual, and MSB policy** to identify potential changes that will help implement the plan's recommendations
- **Create a prioritized list of projects** to start building out the bike and pedestrian network
- **Educate the public** on the vision and goals of the BPP
- **Solicit public input** on the plan's gap analysis and other findings
- **Identify funding mechanisms** to help implement the plan's recommendations

As the plan is rolled out, it is important to ensure linkages between the BPP and the any future transit plans.

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Engagement Planning

Part of this coordinated plan is to take a closer look at engagement planning efforts, both for implementation of recommendations and for future transit plans. The following engagement plan supports these planning and implementation efforts.



Public and Stakeholder Engagement Considerations

Overall Project Summary and Talking Points

The Coordinated Human Services Transportation Plan Update—or “Coordinated Plan”—aims to make transportation more seamless for older adults, individuals with disabilities, and other people facing mobility challenges in the Matanuska-Susitna Borough.



Coordinated plans aim to improve transportation services for older adults, people with disabilities, and other marginalized populations. They are more formally known as coordinated public transit-human service transportation plans, and have a specific legal context at the federal, state, and regional levels.

The primary focus of this Coordinated Plan is to improve transportation and access for the following populations, who tend to experience more mobility challenges. These populations were identified by the lead agency to be included in this plan:

- Older adults
- Individuals with disabilities
- Tribal nations
- Youth (under age 18)
- Veterans
- Low-income individuals and families
- Unhoused populations
- Households with no vehicles

Community Engagement Objectives

- **Establish a Coordinating Committee** to guide plan implementation and provide oversight throughout the implementation process. The Coordinating Committee will be comprised of the project advisory committee, and will be responsible for internal coordination, planning and detailed guidance throughout the plan process, with monthly or quarterly check-in meetings.
- **Participate in relevant ADOT&PF, MSB, and other relevant stakeholder meetings** as appropriate. This will include meetings of the future MPO.
- **Provide Legislative/Assembly Updates** as appropriate to educate and inform.
- **Develop a Project Landing Page** to provide frequent updates, widely distribute information, and act as an informational project clearinghouse. The landing page could be housed in the interim at the MSB until the MPO is launched.



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- **Conduct a survey** to gauge local perceptions of need and actual regional transportation and mobility needs.
- **Hold Public Comment Periods** and Hearings to ensure public feedback into implementation planning.
- **Host Regional Meetings** throughout the region as appropriate.
- **Conduct transit provider assessments** to determine service availability and future service needs.

Target Audiences

- Community members
- Transit/Transportation service consumers
- Alaska Department of Transportation and Public Facilities
- Statewide (legislature), City, and assembly elected officials
- 5307, 5310, 5311, and 5311c recipients
- Matanuska-Susitna Borough
- Health and Human Services
- Commerce & Economic Development Organization(s)
- Social Service(s), including Workforce Development
- Tourism and Recreation Divisions
- Advocacy groups/organizations



Primary Message

The MSB Coordinated Human Service Transportation Plan is the update to the 2018 Coordinated Plan for the Borough. The plan is being developed with the MSB as the lead agency and with the participation of multiple stakeholder agencies. The purpose of the plan is to:

- Improve transportation services for older adults, people with disabilities, and other marginalized populations.
- Aid in the allocation of future funding for transit projects for these marginalized populations.
- Identify and prioritize strategies for future implementation in the Borough.

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Coordinating Committee

There are 21 stakeholder agencies represented on the coordinated plan's Advisory Committee, listed below. Most agencies have more than one representative to ensure regular attendance at meetings throughout the course of the plan and to help support stakeholder education and participation. These entities will be represented on the future Coordinating Committee for the MSB, with the addition of Wasilla Area Seniors, Inc. (WASI).



- Mat-Su Borough
- Mat-Su Health Foundation
- Chickaloon Area Transit Service
- Valley Transit
- Mat-Su Senior Services
- Sunshine Transit
- People Mover
- Knik Tribe
- Mat-Su School District
- Alaska DOT&PF
- AMATS (Anchorage MPO)
- My House
- Mat-Su Housing
- Identity, Inc. (LGBTQIA+ Advocacy)
- Emergency Management Services
- Sunshine Clinic
- Eklutna Tribe
- Valley Charities
- Links Resource Center
- Mat-Su Regional Health Center
- Benteh Nuutah (Tribal Medical Facilities)

Engagement Tactics and Toolkit

Key Engagement Strategies

Successful engagement requires tailored approaches to meet the assorted needs and priorities of key audiences and partners. Individuals and organizations within the key audience groups will vary in their understanding of coordinated planning and transit modes. To that end, it is recommended that the Borough use several core engagement strategies to connect with audiences around their priorities and concerns.



Community Meetings

Community meetings, forums, or public meetings are a method of collecting feedback from community members and neighborhood residents in an informal format. Typically, attendees are self-selected, having received an invitation, or notice for the meeting in advance. The agency soliciting input will usually provide posters, handouts, and other information in addition to a presentation regarding the information or proposed change the agency is seeking feedback on. Discussions in community meetings are highly interactive, wherein attendees can speak to one or more staff representatives from the agency holding the meeting.



Focus Groups / Affinity Groups

Focus groups are smaller in number (usually between 3-15 people; typically, 8) that are convened with a moderator or facilitator to discuss a specific topic. Focus groups are usually highly scientific, and participants are usually compensated for their time. The discussions from the focus group are clearly documented through notes or responses to specific questions. The discussions may be used to reflect ideas and opinions of the larger general population. This type of engagement is most often used in market studies with new consumer products.



Like focus groups, affinity groups pair like-minded, or individuals of a similar background together in an intense, small group setting for feedback purposes. For example, an agency may group developers together in one group, neighborhood associations together in another group, and city representatives together in a third group. Affinity groups are particularly helpful if the subject matter is high-conflict, or if the team simply wants to get direct feedback from specific groups of representatives before convening the discussion on a larger scale with a full audience.

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Public Hearings

A public hearing is an official meeting, usually held at agency offices, such as a transit agency board room or city hall, where individuals from the public have up to three minutes each to speak on specific items which are being “heard” before the council or board. In lieu of speaking, citizens may also vote for or against a proposed change or may simply leave a comment for the council to review. All the meetings are documented, and public hearings are typically one of the last engagement steps in a planning process.



Surveys

Surveys are an incredibly useful tool for garnering public and stakeholder feedback. Surveys may be conducted in person or online, and the parameters may be infinite. Shorter surveys can be offered as the public attends various meetings, and can be delivered via paper, or electronically, with “clickers” that allow meeting attendees to vote on various ideas and concepts. Surveys can also be offered through the mail, at transit stops, and on agency websites. They are an ideal tool due to their flexibility. Survey designers need to ensure that questions are clear and concise, written in such a way to garner responses that are easy to understand.



Virtual and Remote Engagement

During the time of COVID-19, virtual and remote engagement will be the primary means of engaging the broader community. Below are the study’s main talking points for both virtual and remote engagement to reach key audiences and partners.



Tribal Engagement

There are multiple benefits to engaging tribal nations when in the planning processes for transit projects. Tribal engagement is particularly important to coordinated planning efforts, as tribal nations are considered one of the target populations for whom the plan is being implemented. The National Rural Transit Assistance Program (RTAP) outlined several important considerations for tribal engagement, including the following:



- **Access.** Information sharing regarding current transit services helps individuals access transportation services to health care, employment, educational opportunities, and other needs.
- **Equity.** Engaging tribal nations can help improve transportation equity and social justice within the current system.

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- **Collaboration.** Creates an opportunity for cross-jurisdictional coordination and engagement for a more collaborative system, with the possibility for shared future investments.
- **Cooperative Understanding.** Potential to administer the tribal portion of a rural transit system; experience better understanding tribal transit needs and funding with cooperative agreements.
- **Cross-cultural, historic, and political exchange** allows for relationship development and better intergovernmental communication.
- **Funding.** Possible increased access to grant funding by collaborating between tribal nations and rural systems.
- **Provides a firm foundation** for additional future intergovernmental partnerships to achieve implementation of plan recommendations.

In November, the Biden-Harris Administration announced new actions to support Indian Country and Native Communities ahead of the Administration's Second Tribal Nations Summit. Since becoming President, Biden has prioritized relationships with Tribal Nations, and has subsequently prioritized investments for Tribal Nations. President Biden advanced an economic agenda including funding for Tribal communities and Native people, including \$32 billion in the American Rescue Plan (ARP), \$13 billion in the Bipartisan Infrastructure Law (BIL), and \$700 million in the Inflation Reduction Act (IRA).

Appendix

Federal Funding Streams for Public Transportation

	Federal Program	Eligibility	Application
1)	<p>Accelerating Innovative Mobility (AIM) (Link)</p> <p>Program Goals:</p> <ul style="list-style-type: none"> Identify, test, and prove out new approaches, technologies and service models Promote the most promising mobility innovations that can be implemented more broadly through FTA's capital programs Establish a national network of transit stakeholders that are incorporating innovative approaches and business models to improve mobility <p>The federal share of project costs under this program is limited to 80 percent. Proposers may seek a lower federal contribution. The applicant must provide the local share of the net project cost in cash, or in-kind, and must document in its application the source of the local match.</p>	<p>Eligible activities include all activities leading to the development and testing of innovative mobility, such as:</p> <ul style="list-style-type: none"> Planning and developing business models Obtaining equipment and service Acquiring or developing software and hardware interfaces to implement the project Operating or implementing the new service model Evaluating project results. 	<p>Application opportunities are posted in the form of a Notice of Funding Opportunity (NOFO) (link to March 2020 NOFO)</p> <p>In 2020, 25 public transit projects were selected across 24 states and 1 territory to receive \$14 million in funding. Funding amounts ranged from \$40,000 to \$2.3 million.</p>
2)	<p>American Rescue Plan Act of 2021 (link) (fact sheet)</p> <p>Includes \$30.5 billion in supplemental appropriations allocated to support the transit industry during the COVID-19 public health emergency.</p>	<p>Appropriations include:</p> <ul style="list-style-type: none"> \$26.6 billion allocated by statutory formulas to urbanized and rural areas and tribal governments. Eligible activities for urbanized areas include <ul style="list-style-type: none"> Planning, engineering, design and evaluation of transit projects and other technical transportation-related studies Capital investments in bus and bus-related activities such as replacement, overhaul and rebuilding of buses, crime prevention and security equipment and construction of maintenance and passenger facilities 	<p>Applications are competitive and submitted online (2021 form link)</p> <p>FTA will send notification when funding is available for obligation through the Transit Award Management System (TrAMS).</p> <p>FTA most recently announced Notice of Funding Opportunity September 7, 2021 (link)</p>

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	Federal Program	Eligibility	Application
		<ul style="list-style-type: none"> – Capital investments in new and existing fixed guideway systems including rolling stock, overhaul and rebuilding of vehicles, track, signals, communications, and computer hardware and software. – Associated transit improvements and certain expenses associated with mobility management programs – Preventive maintenance and some Americans with Disabilities Act complementary paratransit service costs • \$2.2 billion to FTA grant recipients in communities that demonstrate additional pandemic-associated needs. 	
3)	<p>Better Utilizing Investments to Leverage Development (BUILD) Transportation Grants Program (formerly TIGER) (link) (press release) (fact sheet)</p> <p>Funds investments in transportation infrastructure, including transit. Overall, USDOT has awarded \$9.9 billion to more than 700 projects.</p>	<p>RAISE projects are rigorously reviewed and selected on merit based on statutory criteria of:</p> <ul style="list-style-type: none"> • Safety • Environmental sustainability • Quality of life • Economic competitiveness and opportunity • State of good repair • Partnership and innovation 	<p>Current Notice of Funding Opportunity (link) for \$1.5 billion in total funding, representing a 50% increase in available funds compared to last year, when applicants requested \$10 in funding for every \$1 available.</p> <p>In 2021, 63 funded projects received funding amounts ranging between \$2 million and \$25 million (fact sheet)</p> <p>Deadline of April 14, 2022.</p> <p>Selections announced by August 12, 2022</p>
4)	<p>Capital Investment Grants (CIG) – 5309 (link)</p> <p>Discretionary grant program funds transit capital investments, including heavy rail, commuter rail, light rail, streetcars and bus rapid transit.</p> <p>Fiscal years 2022-26 each have \$3 billion in authorized funding subject to appropriation, with additional \$1.6 billion per year in advanced appropriations. (funding info link)</p>	<p>The Fixing America's Surface Transportation Act (FAST) requires projects fall under 1 of 3 categories (detailed guidance link):</p> <p>New Starts</p> <ul style="list-style-type: none"> • Total project cost is equal to or greater than \$300 million or total New Starts funding sought equals or exceeds \$100 million • New fixed guideway system (light rail, commuter rail etc.) • Extension to existing system • Fixed guideway BRT system <p>Small Starts</p> <ul style="list-style-type: none"> • Total project cost is less than \$300 million and total Small Starts funding sought is less than \$100 million • New fixed guideway systems (light rail, commuter rail etc.) • Extension to existing system • Fixed guideway BRT system • Corridor-based BRT system <p>Core Capacity projects are substantial corridor-based investment in existing fixed guideway system, which must:</p> <ul style="list-style-type: none"> • Be located in a corridor that is at or over capacity or will be in five years 	<p>Federal transit law requires transit agencies seeking CIG funding to complete a series of steps over several years</p> <p>New Starts and Core Capacity projects require completion of two phases in advance of receipt of a construction grant agreement – Project Development and Engineering.</p> <p>Small Starts projects require completion of one phase in advance of receipt of a construction grant agreement – Project Development.</p> <p>Projects must also be rated by FTA at various points in the process according to statutory criteria evaluating project justification and local financial commitment.</p>

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	Federal Program	Eligibility	Application
		<ul style="list-style-type: none"> • Increase capacity by 10% • "not include project elements designated to maintain a state of good repair" 	
5)	<p>Enhancing Mobility Innovation (link)</p> <p>Promotes technology projects that center the passenger experience and encourage people to get on board, such as integrated fare payment systems and user-friendly software for demand-response public transportation.</p> <p>The federal share of project costs under this program is limited to 80%.</p>	<p>Eligible projects fit under one of two topical areas:</p> <ol style="list-style-type: none"> 1. Develop novel operational concepts and/or demonstrate innovations that improve mobility and enhance the rider experience, focused on innovative service delivery models, creative financing, novel partnerships, and integrated payment solutions, or other innovative solutions. <ul style="list-style-type: none"> • This includes all activities leading to uncovering the next iteration of promising technologies, practices and strategies that accelerate innovations in mobility for transit, including, but not limited to, technology scanning and feasibility analysis, stakeholder engagement and outreach, planning, acquiring essential equipment or services, project implementation, modeling forecast of climate and equity impacts of proposed novel concepts and evaluating project results. 2. Develops software to facilitate demand-response public transportation that dispatches transit vehicles through riders' mobile devices or other means. <ul style="list-style-type: none"> • Eligible activities may include establishing user needs; defining system requirements; development, validation and verification of the software; modeling and simulation; and/or pilot implementation, with a software solution. 	<ul style="list-style-type: none"> • On November 12, 2021, FTA released a Notice of Funding Opportunity (NOFO) to solicit project proposals for <u>the Enhancing Mobility Innovation Competitive Funding Opportunity</u>. The NOFO made available \$2 million in Fiscal Year 2021 funds. • Project proposals were due January 11, 2022
6)	<p>Grants for Buses and Bus Facilities Program (link)</p> <p>To assist in the financing of buses and bus facilities capital projects, including replacing, rehabilitating, purchasing or leasing buses or related equipment, and rehabilitating, purchasing, constructing or leasing bus-related facilities.</p>	<p>Eligible Activities</p> <ul style="list-style-type: none"> • Capital projects to replace, rehabilitate and purchase buses, vans, and related equipment, • Capital projects to construct bus-related facilities, including technological changes or innovations to modify low or no emission vehicles or facilities. 	<p>Competitive allocation provides funding for major improvements to bus transit systems that would not be achievable through formula allocations.</p> <p>Supplemental Form link</p> <p>FTA last announced a Notice of Funding Opportunity due November 19, 2021 (link)</p>
7)	<p>Innovative Coordinated Access and Mobility Grants (ICAM) (link)</p> <p>To improve access to public transportation by building partnerships among health, transportation and other service providers</p>	<p>Eligible Activities</p> <ul style="list-style-type: none"> • Innovative projects for the transportation disadvantaged that will improve the coordination of transportation services and non-emergency medical transportation services. 	<p>In 2018, there were two funding opportunities under the initiative: the Innovative Coordinated Access and Mobility (ICAM) Pilot Program and Human Services Coordination Research (HSCR) grants.</p> <p>In 2021, only the ICAM funding is available. FTA last announced a Notice of Funding Opportunity due December 6, 2021 (link)</p>

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	Federal Program	Eligibility	Application
8)	Integrated Mobility Innovation (IMI) (link) Program goals are: <ul style="list-style-type: none"> • Enhance transit industry preparedness for IMI • Assist the transit industry to develop the ability to integrate IMI practices with existing public transit service • Validate the technical and institutional feasibility of IMI business models, and document IMI best practices that may emerge from the demonstrations • Measure the impacts of IMI on travelers and transportation systems • Examine relevant public sector and Federal requirements, regulations, and policies that may support or hamper the public transit sector's adoption of IMI 	Eligible Activities fall under three research focus areas: <ul style="list-style-type: none"> • Mobility on Demand • Transit Automation • Mobility Payment Integration Activities can include: <ul style="list-style-type: none"> • Planning and developing business models • Obtaining equipment and service • Acquiring or developing software and hardware interfaces to implement the project • Operating the demonstration • Providing data to support performance measurement and evaluation 	In 2020, \$20.3 million in funding was granted to 25 projects in 23 states (press release link) FTA last announced a Notice of Funding Opportunity due December 6, 2021 (link)
9)	Metropolitan & Statewide Planning and Non-Metropolitan Transportation Planning - 5303, 5304, 5305 (link) Provides funding and procedural requirements for multimodal transportation planning in metropolitan areas and states. Planning needs to be cooperative, continuous, and comprehensive, resulting in long-range plans and short-range programs reflecting transportation investment priorities.	Eligible planning activities: <ul style="list-style-type: none"> • Support the economic vitality of the metropolitan area, especially by enabling global competitiveness, productivity, and efficiency • Increase the safety of the transportation system for motorized and nonmotorized users • Increase the security of the transportation system for motorized and nonmotorized users • Increase the accessibility and mobility of people and for freight • Protect and enhance the environment, promote energy conservation, improve the quality of life, and promote consistency between transportation improvements and State and local planned growth and economic development patterns • Enhance the integration and connectivity of the transportation system, across and between modes, for people and freight • Promote efficient system management and operation • Emphasize the preservation of the existing transportation system 	Funds are apportioned to states by a formula that includes each state's urbanized area population in proportion to the total urbanized area population for the nation, as well as other factors. States can receive no less than .5 percent of the amount apportioned. These funds, in turn, are sub-allocated by states to MPOs by a formula that considers each MPO's urbanized area population, their individual planning needs, and a minimum distribution.
10)	Public Transportation Innovation – 5312 (link) Provides funding to develop innovative products and services assisting transit agencies in better meeting the needs of their customers.	Eligible Activities <ul style="list-style-type: none"> • Research • Development • Demonstration • Deployment projects 	Funds may be allocated on a discretionary basis. No recent NOFAs available

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	Federal Program	Eligibility	Application
		<ul style="list-style-type: none"> Evaluation of technology of national significance to public transportation 	
11)	<p>Real-Time Transit Infrastructure and Rolling Stock Condition Assessment Research and Demonstration Program</p> <p>Funds cooperative agreements to engage in demonstrations to assess and identify infrastructure deficiencies in public transportation rolling stock via innovative technologies to keep public transit assets in a state of good repair.</p> <p>Intended to help transit agencies:</p> <ul style="list-style-type: none"> Explore advanced cutting-edge technologies that can provide real-time condition assessment of transit capital and facilities Allow a more effective way for transit agencies to assess, detect, monitor and track deficiencies and defects related to infrastructure and rolling stock Evaluate the cost-effectiveness and the practicality of proposed state-of-the art solutions 	<p>This program is a research demonstration program and not a capital procurement program. The project proposals must include a research/synthesis phase, a development phase, and a demonstration phase. All phases are critical to project selection.</p> <p>To ensure proposed demonstration projects address the needs of transit agencies, FTA requires that applicants identify partnerships with at least one transit agency. FTA will assess the strength of those partnerships as part of its evaluation of applications.</p>	<p>Funding availability depending on FTA's Research, Development, Demonstration and Deployment Program. No recent NOFAs available.</p>

CODE ORDINANCE

Sponsored by:
Introduced:
Public Hearing:
Action:

**MATANUSKA-SUSITNA BOROUGH
ORDINANCE SERIAL NO. 23-074**

AN ORDINANCE OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY AMENDING MSB 15.24.030(B)(44) AND ADOPTING THE 2023 COORDINATED HUMAN SERVICES TRANSPORTATION PLAN.

WHEREAS, all the information regarding this ordinance can be found in Information Memorandum No. 23-149.

BE IT ENACTED:

Section 1. Classification. This ordinance is of a general and permanent nature and shall become a part of the Borough Code.

Section 2. Amendment of paragraph. MSB 15.24.030(B)(44) is hereby amended to read as follows:

(44) [2018-2022]2023 Coordinated Human Services Transportation Plan, [ADOPTED NOVEMBER 2018]; adopted September 2023.

Section 3. Effective date. This ordinance shall take effect upon adoption.

ADOPTED by the Matanuska-Susitna Borough Assembly this - day
of -, 2023.

EDNA DeVRIES, Borough Mayor

ATTEST:

LONNIE R. McKECHNIE, CMC, Borough Clerk

(SEAL)

DRAFT

By:
Introduced:
Public Hearing:
Action:

MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION RESOLUTION NO. 23-20

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH PLANNING COMMISSION
RECOMMENDING MATANUSKA-SUSITNA BOROUGH ASSEMBLY ADOPTION OF THE
2023 COORDINATED HUMAN SERVICES TRANSPORTATION PLAN UPDATE.

WHEREAS, the Matanuska-Susitna Borough Planning Commission
advises the Assembly on issues and activities related to planning
land use regulation and community development; and

WHEREAS, the primary focus of the Matanuska-Susitna Borough's
Coordinated Human Services Transportation Plan (CHSTP) Update is
to evaluate and improve transportation options and access to
services for priority populations such as older adults, youth,
indigenous populations, veterans, individuals with disabilities,
individuals living in poverty, and individuals with limited
English proficiency; and

WHEREAS, the Borough-wide Comprehensive Plan identifies
completing functional plans that address each mode of
transportation as a transportation priority; and

WHEREAS, historically, research indicates that priority
populations have higher rates of transit dependency and lower
access to vehicles; and

WHEREAS, the 2020 U.S. Census identified a higher rate of priority populations in the Matanuska-Susitna Borough (MSB) when compared to other coordinated planning regions, underscoring the need for improved transportation services and provider coordination; and

WHEREAS, the most recent update to the MSB's CHSTP was adopted in 2018; and

WHEREAS, Federal Transit Administration (FTA) Circular 5310 funding through the Fixing America's Surface Transportation (FAST) Act and Alaska Mental Health Trust (AMHT) funding through the Alaska Department of Transportation and Public Facilities (ADOT&PF) Alaska Community Transit office focus on the transportation needs of disadvantaged persons and those with special transportation needs that cannot be met through traditional personal automobile or public transportation means; and

WHEREAS, in order for local providers to be eligible for FTA 5310 or AMHT funding, projects must be derived from a locally developed, coordinated plan that is updated at least every five years; and

WHEREAS, the ADOT&PF must certify to FTA that a CHSTP includes all of the following requirements:

1. Be locally developed, evidenced by public participation that must include seniors, individuals with disabilities, representatives of public, private, nonprofit, and human services transportation providers, and other members of the public.
2. Include information on the community background, an inventory of local resources and services, a needs assessment, gaps in service, strategies, priority of projects, and a signature page of participating agencies.
3. A resolution from the local governing body adopting the plan.
4. Must be updated every five years.

WHEREAS, the CHSTP Update gathered feedback from interviews, advisory committee meetings, and a workshop, all involving transit providers, human service organizations, and local, regional, and state agencies and organizations, and also included a 30 day public comment period; and

WHEREAS, the CHSTP Update includes a list of prioritized goals and strategy recommendations to implement future improvements that effectively meet the transportation needs of priority populations throughout the Borough.

NOW, THEREFORE, BE IT RESOLVED, that the Matanuska-Susitna Borough Planning Commission hereby recommends Assembly adoption of the 2023 Coordinated Human Services Transportation Plan Update.

ADOPTED by the Matanuska-Susitna Borough Planning Commission
this -- day of --, 2023.

C.J. KOAN, CHAIR

ATTEST

KAROL RIESE, Planning Clerk

(SEAL)

YES:

NO:

INTRODUCTION FOR PUBLIC HEARING LEGISLATIVE

Resolution No. 23-30

Bike and Pedestrian Plan

(Pages _ - _)

INTRODUCTION FOR PUBLIC HEARING

MATANUSKA-SUSITNA BOROUGH INFORMATION MEMORANDUM IM No. 23-145

SUBJECT: AN ORDINANCE OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY ADOPTING MSB 15.24.030 (B) (12) (a) MATANUSKA-SUSITNA BOROUGH BICYCLE AND PEDESTRIAN PLAN.

AGENDA OF: September 5, 2023

ASSEMBLY ACTION:

AGENDA ACTION REQUESTED: Introduce and set for public hearing.

Route To	Signatures
Originator	<input checked="" type="checkbox"/>
Department Director	<input checked="" type="checkbox"/>
Finance Director	<input checked="" type="checkbox"/>
Borough Attorney	<input checked="" type="checkbox"/>
Borough Manager	<input checked="" type="checkbox"/>
Borough Clerk	<input checked="" type="checkbox"/>

ATTACHMENT (S) : Fiscal Note

Local Road Service Area Advisory Board Resolution
NO. 23-03 (2 pp)

Transportation Advisory Board Resolution 23-06
(2 pp)

Parks, Recreation, and Trails Advisory Board
Resolution 23-03 (2 pp)

Planning Commission Resolution No. 23-30 (3 pp)

Matanuska-Susitna Borough Bicycle and Pedestrian
Plan Executive Summary (2 pp)

Matanuska-Susitna Borough Bicycle and Pedestrian
Plan (98 pp)

Ordinance Serial No. 23-068 (1 pp)

SUMMARY STATEMENT:

In 2017, the Matanuska-Susitna Borough Assembly adopted the Long-Range Transportation Plan (LRTP). The LRTP consists of goals and strategies to build out a better-connected and safer transportation network and includes goals to increase mobility and transportation choices, to name a few. One of the strategies in the LRTP is to develop an active transportation master plan that creates a vision for the bicycle and pedestrian system and identifies recommendations to implement that vision. The Planning Department has developed the Borough's first Bicycle and Pedestrian Plan (BPP) to implement the strategies and goals pertaining to nonmotorized transportation borough-wide.

Staff worked with a local consulting company, RESPEC, to develop an Existing Conditions Report and an Initial Recommendations Report. RESPEC also subcontracted McKinley Group to provide an Economic Analysis Report, which showed that investing in nonmotorized infrastructure increases public health. It also showed that homes that are in proximity to a connected nonmotorized network can have a realized value increase of 3%-5%.

The team also conducted extensive outreach to identify where new or improved facilities are needed, according to MSB residents using stakeholder interviews and a public comment map. The BPP gathered feedback from community councils, MSB advisory boards, transportation planners and engineers, local governments, bike & pedestrian advocate groups, developers and real estate agents, and the general public through steering committee meetings, public meetings and events, and a 45-day public comment period.

Public engagement highlights:

- 30 new projects added to the Implementation Plan.
- 72 comments received.
- Overwhelming support for the plan from the public and three resolutions of support from MSB advisory boards.

The BPP was developed by understanding the existing system of sidewalks, pathways, roads, and development and incorporated an economic analysis showing that increased transportation choices positively impact the social, physical, economic, and environmental qualities of life that MSB residents enjoy. The BPP provides a clear path forward for the Matanuska-Susitna Borough to begin implementing the recommended policies, infrastructure, and programs outlined in the BPP.

RECOMMENDATION OF ADMINISTRATION: Staff recommends the Assembly adopt the Matanuska-Susitna Borough Bicycle and Pedestrian Plan.

MATANUSKA-SUSITNA BOROUGH

August 21, 2023 Planning Commission Packet

311 of 450

FISCAL NOTE

Agenda Date: _____

SUBJECT: ADOPTION OF THE MATANUSKA-SUSITNA BOROUGH BICYCLE AND PEDESTRIAN PLAN

FISCAL ACTION (TO BE COMPLETED BY FINANCE)	FISCAL IMPACT YES NO
AMOUNT REQUESTED	FUNDING SOURCE
FROM ACCOUNT #	PROJECT
TO ACCOUNT :	PROJECT #
VERIFIED BY: _____	CERTIFIED BY:
DATE:	DATE:

EXPENDITURES/REVENUES:

(Thousands of Dollars)

OPERATING	FY2023	FY2024	FY2025	FY2026	FY2027	FY2028
Personnel Services						
Travel						
Contractual						
Supplies						
Equipment						
Land/Structures						
Grants, Claims						
Miscellaneous						
TOTAL OPERATING						

CAPITAL						
---------	--	--	--	--	--	--

REVENUE						
---------	--	--	--	--	--	--

FUNDING:

(Thousands of Dollars)

General Fund						
State/Federal Funds						
Other						
TOTAL						

POSITIONS:

Full-Time						
Part-Time						
Temporary						

ANALYSIS: (Attach a separate page if necessary)

PREPARED BY: _____ PHONE: _____

DEPARTMENT: _____ DATE: _____

APPROVED BY: _____ DATE: _____

**MATANUSKA-SUSITNA BOROUGH
LOCAL ROAD SERVICE AREA ADVISORY BOARD
RESOLUTION SERIAL NO. 23-03**

A RESOLUTION OF THE MATANSUKA-SUSITNA BOROUGH LOCAL ROAD SERVICE AREA ADVISORY BOARD RECOMMENDING THE ASSEMBLY APPROVAL OF THE MATANSUSKA-SUSITNA BOROUGH'S BICYCLE AND PEDESTRIAN PLAN.

WHEREAS, the Assembly adopted the Long-Range Transportation Plan (LRTP) in 2017; and

WHEREAS, the LRTP consists of goals and strategies to build out a better-connected and safer transportation network, including goals to increase mobility and transportation choices; and

WHEREAS, one of the strategies in the LRTP is to develop an active transportation master plan that creates a vision for the bicycle and pedestrian system and identifies recommendations to implement that vision; and

WHEREAS, the Matanuska-Susitna Borough Planning Department has developed the Borough's first Bike and Pedestrian Plan to implement LRTP strategies and goals pertaining to nonmotorized transportation; and

WHEREAS, the Bike and Pedestrian Plan (BPP) was developed by understanding the existing system of sidewalks, pathways, roads, and development, and incorporates an economic analysis that shows increased transportation choices have positive impacts on the social, physical, economic, and environmental qualities of life that MSB residents enjoy; and

WHEREAS, the BPP gathered feedback from community councils, MSB advisory boards, transportation planners and engineers, local governments, bike & pedestrian advocate groups, developers and real estate agents, and the general public through steering committee meetings, public meetings and events, and a 45-day public comment period; and

WHEREAS, the public process allowed for timely comment and discussion on possible routes, neighborhood concerns, increasing safe routes to school, development standards, and other pertinent information; and

WHEREAS, public comments were incorporated into the BPP where appropriate; and

WHEREAS, the BPP provides a clear path forward for the Matanuska-Susitna Borough to begin implementing the recommended policies, infrastructure, and programs outlined in the BPP.

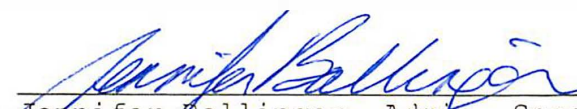
NOW, THEREFORE, BE IT RESOLVED,

ADOPTED by the Matanuska-Susitna Borough Local Road Service Area Advisory Board this - day of -, 2023.


Stephen Edwards, Chairperson

ATTEST:

Approved Unanimously 5/18/23


Jennifer Ballinger, Admin. Specialist

**MATANUSKA-SUSITNA BOROUGH
TRANSPORTATION ADVISORY BOARD
RESOLUTION SERIAL NO. 23-06**

A RESOLUTION OF THE MATANSUKA-SUSITNA BOROUGH TRANSPORTATION ADVISORY BOARD RECOMMENDING THE ASSEMBLY APPROVAL OF THE MATANSUSKA-SUSITNA BOROUGH BICYCLE AND PEDESTRIAN PLAN.

WHEREAS, the Assembly adopted the Long-Range Transportation Plan (LRTP) in 2017; and

WHEREAS, the LRTP consists of goals and strategies to build out a better-connected and safer transportation network, including goals to increase mobility, transportation choices, and safety of non-motorized users; and

WHEREAS, one of the strategies in the LRTP is to develop an active transportation master plan that creates a vision for the bicycle and pedestrian system and identifies recommendations to implement that vision; and

WHEREAS, the Matanuska-Susitna Borough Planning Department has developed the Borough's first Bike and Pedestrian Plan to implement LRTP strategies and goals pertaining to nonmotorized transportation; and

WHEREAS, the Bike and Pedestrian Plan (BPP) was developed by understanding the existing system of sidewalks, pathways, roads, and development, and incorporates an economic analysis that shows increased transportation choices have positive impacts on the social, physical, economic, and environmental qualities of life

that MSB residents enjoy; and

WHEREAS, the BPP gathered feedback from community councils, MSB advisory boards, transportation planners and engineers, local governments, bike & pedestrian advocate groups, developers and real estate agents, and the general public through steering committee meetings, public meetings and events, and a 45-day public comment period; and

WHEREAS, the public process allowed for timely comment and discussion on possible routes, neighborhood concerns, increasing safe routes to school, development standards, and other pertinent information; and

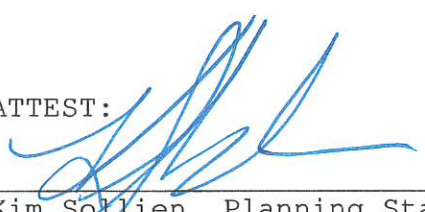
WHEREAS, public comments were incorporated into the BPP where appropriate; and

WHEREAS, the BPP provides a clear path forward for the Matanuska-Susitna Borough to begin implementing the recommended policies, infrastructure, and programs outlined in the BPP.

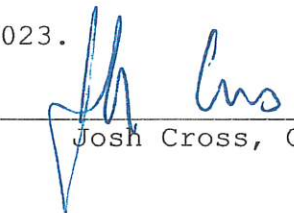
NOW, THEREFORE, BE IT RESOLVED, the Matanuska-Susitna Borough Transportation Advisory Board recommends the Assembly adopt the Matanuska-Susitna Borough Bicycle and Pedestrian Plan.

ADOPTED by the Matanuska-Susitna Borough Transportation Advisory Board this 23rd day of June 2023.

ATTEST:



Kim Sollien, Planning Staff



Josh Cross, Chairperson

**MATANUSKA-SUSITNA BOROUGH
PARKS, RECREATION, AND TRAILS ADVISORY BOARD
RESOLUTION SERIAL NO. 23-03**

A RESOLUTION OF THE MATANSUKA-SUSITNA BOROUGH PARKS, RECREATION, AND TRAILS ADVISORY BOARD RECOMMENDING THE ASSEMBLY APPROVAL OF THE MATANSUSKA-SUSITNA BOROUGH'S BIKE AND PEDESTRIAN PLAN.

WHEREAS, the Assembly adopted the Long-Range Transportation Plan (LRTP) in 2017; and

WHEREAS, the LRTP consists of goals and strategies to build out a better-connected and safer transportation network, including goals to increase mobility and transportation choices; and

WHEREAS, one of the strategies in the LRTP is to develop an active transportation master plan that creates a vision for the bicycle and pedestrian system and identifies recommendations to implement that vision; and

WHEREAS, the Matanuska-Susitna Borough Planning Department has developed the Borough's first Bike and Pedestrian Plan to implement LRTP strategies and goals pertaining to nonmotorized transportation; and

WHEREAS, the Bike and Pedestrian Plan (BPP) was developed by understanding the existing system of sidewalks, pathways, roads, and development, and incorporates an economic analysis that shows increased transportation choices have positive impacts on the social, physical, economic, and environmental qualities of life that MSB residents enjoy; and

WHEREAS, the BPP gathered feedback from community councils, MSB advisory boards, transportation planners and engineers, local governments, bike & pedestrian advocate groups, developers and real estate agents, and the general public through steering committee meetings, public meetings and events, and a 45-day public comment period; and

WHEREAS, the public process allowed for timely comment and discussion on possible routes, neighborhood concerns, increasing safe routes to school, development standards, and other pertinent information; and

WHEREAS, public comments were incorporated into the BPP where appropriate; and

WHEREAS, the BPP provides a clear path forward for the Matanuska-Susitna Borough to begin implementing the recommended policies, infrastructure, and programs outlined in the BPP.

NOW, THEREFORE, BE IT RESOLVED,

The Matanuska-Susitna Borough Parks, Recreation, and Trails Advisory Board recommends that this plan be adopted by the Borough Assembly, this 26th day of June, 2023.


Chris Wilson, Chairperson

ATTEST: 
Alyssia Jones, Rec. Admin. Specialist

Executive Summary

The Matanuska-Susitna Borough (MSB) Bicycle and Pedestrian Plan (BPP) was developed to improve the nonmotorized transportation network in the borough. RESPEC, a firm contracted by the MSB, performed an existing conditions analysis, sought input from the public, and worked with the steering committee to develop the project recommendations in this plan.

The Process

- 1 The project team reviewed existing planning documents, analyzed bicycle and pedestrian crash data, and conducted interviews with stakeholders and user groups to develop an Existing Conditions & Issues report. Outreach and research showed that there is a strong desire for a robust bike and pedestrian network in the MSB and a clear need to make nonmotorized transportation safer for all users. Two key challenges with developing a borough-wide transportation network were identified: population centers are widely dispersed and several entities are responsible for constructing and maintaining nonmotorized facilities.
- 2 The team conducted extensive outreach to identify where new or improved facilities are needed according to the folks who know the area best – MSB residents. In addition to interviews and an interactive comment map, MSB staff attended several community events to share information and gather public input.
- 3 The BPP steering committee has provided guidance and feedback throughout the entire process. The committee has 10 members, each of whom has familiarity and expertise with the bike and pedestrian network in the MSB. The members of the committee provided recommendations, identified important user groups, and reviewed multiple drafts of the BPP.

Implementation Plan

The BPP contains 111 policy, infrastructure, and program recommendations. The infrastructure recommendations are listed as near-term, mid-term, or

long-term projects, though the timeframes for individual projects may vary based on future studies or the need to obtain right-of-way.

➤ Policy:

- ❖ Facility design standards
- ❖ Complete Streets policy
- ❖ Snow clearing policy
- ❖ Maintenance policy
- ❖ Subdivision regulations
- ❖ Vision Zero program
- ❖ Bike and pedestrian projects in the TIP

➤ Infrastructure:

- ❖ Separated, shared-use paths
- ❖ Sidewalks
- ❖ Roadway crossing treatments
- ❖ Connections between or extensions of existing pathways

➤ Programs:

- ❖ Nonmotorized taskforce
- ❖ Annual bicycle/pedestrian counts
- ❖ Level of service assessment
- ❖ Bicycle and pedestrian map
- ❖ ADA assessment
- ❖ User conflict guide
- ❖ Wayfinding plan
- ❖ Greenbelt pathway reconnaissance and engineering study
- ❖ Interpretive bicycle and pedestrian path

The BPP also outlines what funding sources are available based on what entity or entities will be taking ownership of the project and provides a list of potential grant funding opportunities.

Recreation connections and resources

Another goal of the BPP is to connect the nonmotorized transportation network to recreational trail networks and parks. To accomplish this, many of the recommended infrastructure projects link these two networks. The plan also contains a map of trailheads and public parks that shows existing or planned network connections and compiled resources for residents and visitors to learn more about recreational trails in the MSB!

Matanuska-Susitna Borough **Bicycle and Pedestrian Plan**

June 19, 2023



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Letter from the Mayor

The Mat-Su Borough's vision statement is to be 'The greatest community in Alaska to live, work and play'; a vital part of our mission statement is 'To efficiently and effectively deliver responsive services to the public'. These two statements are more than just words, they are action statements. It is our goal here at the Borough to not only provide a great place to live, work and raise families, but also provide the essential services that will ensure a safe community to live, work and play.



The Mat-Su Borough's Bike and Pedestrian Plan (BPP) plan is one of the ways that we are working to provide a safe and effective way for residents to safely enjoy our beautiful Valley. The implementation of the BPP includes programs, policies, and infrastructure that will improve our public health by making it easier to ride a bike, or walk the pathways; strengthen our public safety by reducing the number of cyclist and pedestrian injuries and fatalities; increase home values by building up the trails and pathway networks around neighborhoods; and add to the local economy by developing connections that bring people into business districts and provide for tourism opportunities.

The BPP is a chapter of the Borough's Long-Range Transportation Plan and will help us reach our goal of increasing transportation choices, improving connectivity, and supporting economic vitality here in the Mat-Su Valley. Tourism and recreation opportunities are some of the Mat-Su Valley's greatest strengths; a safe and functional road, pathway, and trail system will improve the quality of life for residents and visitors alike.

We appreciate your input and participation in the planning process, because working together we can make even better decisions.

Mayor Edna DeVries

Acknowledgments

MSB Planning Commission

Doug Glenn *District 1*

Rick Allen *District 2*

CJ Koan *District 3*

Michael Rubeo *District 4*

William Kendig *District 5*

Wilfred Fernandez *District 6*

Curt Scoggin *District 7*

MSB Assembly

Tim Hale *District 1*

Stephanie Nowers *District 2*

Dee McKee *District 3*

Rob Yundt *District 4*

Mokie Tew *District 5*

Dmitri Fonov *District 6*

Ron Bernier *District 7*

Consultant Team

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Elise Blocker *RESPEC*

Megan Flory *RESPEC*

Donna Logan *McKinley Research Group*

Marie Schmidt *RESPEC*

Casey Witt *RESPEC*

MSB Staff

Alex Strawn *Planning & Land Use Director*

Kelsey Anderson *Planner III*

Kim Sollien *Planning Services Manager*

Steering Committee

Adam Bradway *MSB Planner*

Jenny Willardson *Elevate Alaska Realty Commercial Sales & Leasing*

Jim Beck *Mat-Su Health Foundation*

Joe Metzger *MSB Land Management*

Joshua Shaver *Alaska Pioneer Homes*

Julius Adolfsson *DOT&PF Statewide Bike and Pedestrian Coordinator, Rural Transit Planner*

Mike Campfield *MSB Public Works*

Patricia Owens *Parks Rec, and Trails Advisory Board*

Tracy Kalytiak *Mat-Su Health Foundation*

Wes Hoskins *Mat-Su Trails and Parks Foundation*

And everyone who participated in our public outreach activities!

—Executive Summary—

The Matanuska-Susitna Borough (MSB) Bicycle and Pedestrian Plan (BPP) was developed to improve the nonmotorized transportation network in the borough. RESPEC, a firm contracted by the MSB, performed an existing conditions analysis, sought input from the public, and worked with the steering committee to develop the project recommendations in this plan.

1

The Process

The project team reviewed existing planning documents, analyzed bicycle and pedestrian crash data, and conducted interviews with stakeholders and user groups to develop an Existing Conditions & Issues report. Outreach and research showed that there is a strong desire for a robust bike and pedestrian network in the MSB and a clear need to make nonmotorized transportation safer for all users. Two key challenges with developing a borough-wide transportation network were identified: population centers are widely dispersed and several entities are responsible for constructing and maintaining nonmotorized facilities.

2

The team conducted extensive outreach to identify where new or improved facilities are needed according to the folks who know the area best – MSB residents. In addition to interviews and an interactive comment map, MSB staff attended several community events to share information and gather public input.

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The BPP steering committee has provided guidance and feedback throughout the entire process. The committee has 10 members, each of whom has familiarity and expertise with the bike and pedestrian network in the MSB. The members of the committee provided recommendations, identified important user groups, and reviewed multiple drafts of the BPP.

Implementation Plan

The BPP contains 111 policy, infrastructure, and program recommendations. The infrastructure recommendations are listed as near-term, mid-term, or long-term projects, though the timeframes for individual projects may vary based on future studies or the need to obtain right-of-way.

Funding sources are identified based on which entity will be taking ownership of the project. A list of potential grant opportunities is also provided in Appendix F.

Policy

- » Facility design standards
- » Complete Streets
- » Snow clearing
- » Maintenance
- » Subdivision regulations
- » Vision Zero
- » Bike and pedestrian projects in the TIP

Infrastructure

- » Separated, shared-use paths
- » Sidewalks
- » Roadway crossing treatments



Programs

- » Nonmotorized taskforce
- » Annual bike/pedestrian counts
- » Level of service assessment
- » Bike and pedestrian map
- » ADA assessment
- » User conflict guide
- » Wayfinding plan
- » Greenbelt pathway reconnaissance and engineering study

Recreation

Another goal of the BPP is to connect the nonmotorized transportation network to recreational trail networks and parks. To accomplish this, many of the recommended infrastructure projects link these two networks.

The plan also contains a map of trailheads and public parks that shows existing or planned network connections and compiled resources for residents and visitors to learn more about recreational trails in the MSB!

Acronyms and Abbreviations

ADA Americans with Disabilities Act

BIL Bipartisan Infrastructure Law

BPP Bicycle and Pedestrian Plan

CIP Capital Improvement Plan

Comp Plan Matanuska-Susitna Borough Comprehensive Development Plan — 2005 Update

DOT&PF Alaska Department of Transportation & Public Facilities

EPA U.S. Environmental Protection Agency

ESRI Environmental Systems Research Institute

FAO Food and Agriculture Organization

FHWA Federal Highway Administration

GIS Geographic Information System

GPRA Government Peak Recreation Area

HERE Here Technologies

IJA Infrastructure Investment and Jobs Act

LRTP Long-Range Transportation Plan

Mat-Su Matanuska-Susitna

METI/NASA Ministry of Economic, Trade, and Industry/
National Aeronautics and Space Administration

MPO Metropolitan Planning Organization

MSB Matanuska-Susitna Borough

MVP Matanuska-Susitna Valley Planning for Transportation

RSA Road Service Area

SRTS Safe Routes to School

STBG Surface Transportation Block Grant

STIP Statewide Transportation Improvement Program

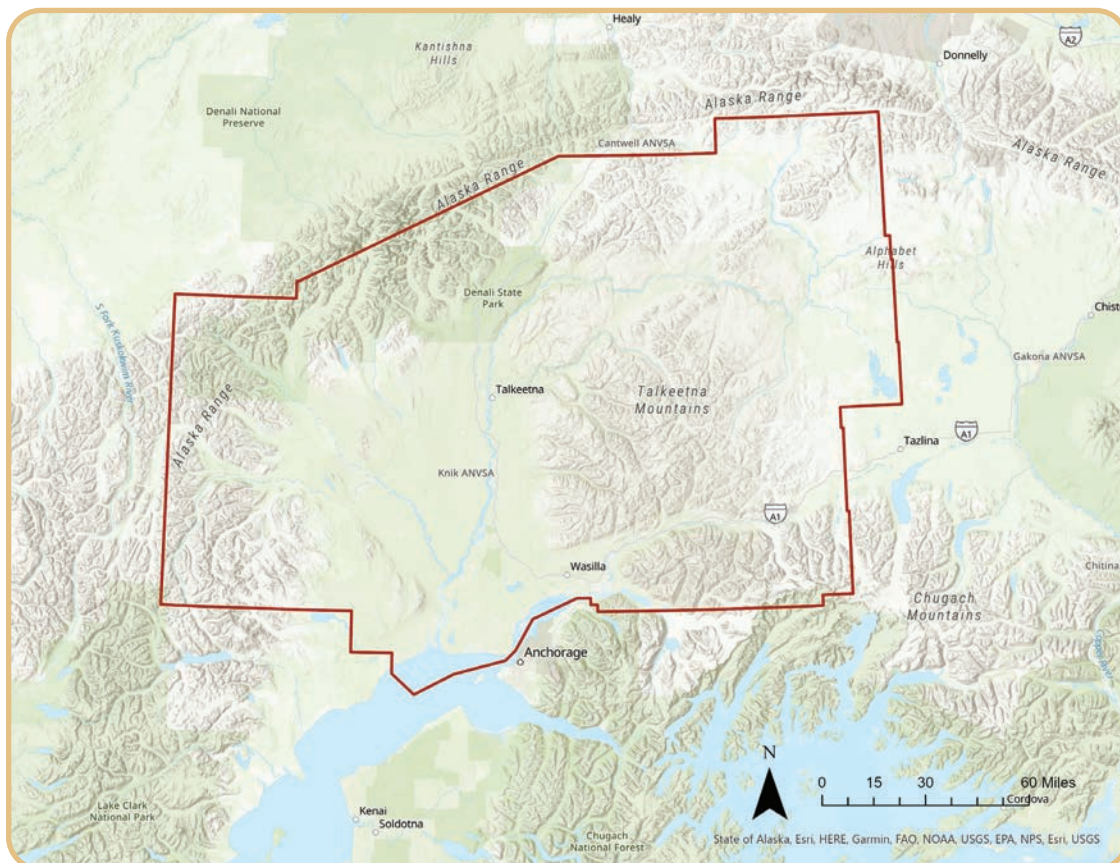
TIP Transportation Improvement Program

USGS United States Geological Survey

- Introduction -

The Matanuska-Susitna (Mat-Su) geographical area includes approximately 25,000 square miles of land and approximately 2,000 miles of trails (MSB, 2022). The Mat-Su has been inhabited for millennia, beginning with the Dena'ina and Ahtna Athabascans, and expanding to settlers throughout the 19th and 20th centuries. Transportation routes were developed and built upon by each group for subsistence, trade, and commerce. The Mat-Su encompasses glaciers, rivers, wetlands, hills, mountains, lakes, farmland, and forests, and hosts a vast array of natural resources that continue to bring people to the region to live, work, and play.

The Mat-Su Borough (MSB) consists of 25 communities, including three cities—Houston (second-class city), Palmer (home-rule city), and Wasilla (first-class city). The remaining 22 communities are classified as Census Designated Places. The MSB is a second-class borough with a seven-member Assembly and Mayor. The MSB also includes two tribal governments: Chickaloon Native Village and Knik Tribe.



Why does the MSB need a bicycle and pedestrian plan?

The MSB's vision is to be the greatest community in Alaska to live, work, and play. This vision means something different to every resident, but for the borough, it means protecting and enhancing those qualities of life that residents value most. The Mat-Su continues to be the fastest-growing community in the state and vehicle travel will remain the dominant and, in many areas, only option for transportation. As a result, bike and pedestrian planning has been an afterthought in planning for how people move throughout the region. The MSB has a lot of work to do to make up for gaps in the existing network, while looking forward

to meet the future needs of its dynamic and ever-changing population. The Bicycle and Pedestrian Plan (BPP) was developed to help launch programs, start infrastructure projects, and put policies in place. The plan supports a nonmotorized transportation network that is safe, connected, and easy to use for residents and visitors of all ages and abilities.



The MSB's population has **grown by 20%** in the last decade (Census, 2020) and is expected to reach **130,000 by 2027** (MSB, 2022).

According to an MSB Smart Growth survey from January 2022, borough residents agree that improving and expanding transportation infrastructure and widening shoulders on existing roads should be top priorities of the MSB government. Residents also agreed that the top three measures of good quality of life in the Mat-Su are (1) access to public lands and recreation areas, (2) a great place to raise children, and (3) access to grocery stores and local food options. The BPP makes the connection between how MSB residents measure quality of life through the lens of transportation choices. It is a policy document that will help MSB staff and elected officials consider how an expanded and safer nonmotorized network can support economic diversity and growth through tourism and transportation infrastructure, decrease serious injuries and fatal crashes, and increase Safe Routes to School (SRTS) connections, to name a few.

The BPP's priority is the safety of all travelers within the MSB. Data show that the more local governments plan for compatible uses and shared

spaces in transportation, the safer drivers, bikers, and pedestrians are. Incorporating bike and pedestrian facilities throughout the MSB does not have to be limited to separated pathways along the road system. The MSB has many existing urban and rural trails, separated pathways, sidewalks, and backcountry trails that could become part of an all-encompassing bike and pedestrian system. By improving existing facilities and adding new connections, the MSB can support the health and safety of all its residents and visitors.

103

reported
bicycle and
pedestrian
incidents

33%

resulted in
a serious
injury or
fatality

(DOT&PF, 2010-2019)

7

fatalities
occurred
and all were
pedestrians

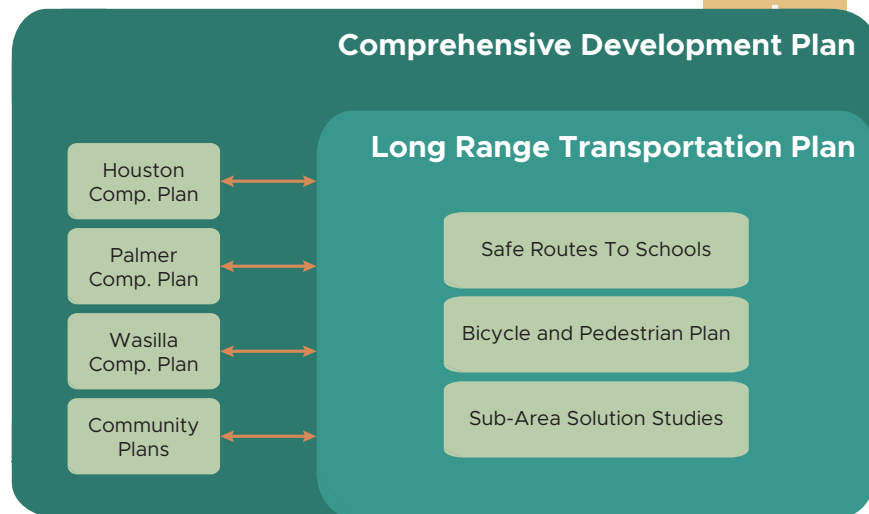


What is a bicycle and pedestrian plan?

A BPP is a comprehensive strategy that evaluates the current bike and pedestrian network, examines gaps and hazard areas, and provides solutions to develop an all-encompassing nonmotorized transportation network that creates additional connections outside the road system and is safe for all user types.

Comprehensive Development Plan

The MSB Comprehensive Development Plan – 2005 Update (Comp Plan), originally adopted in 1970, offers goals and policy recommendations for future development for a 20-year period. Within the Comp Plan are transportation goals that include supporting a multi-modal transportation plan that encourages street and trail connectivity at a regional and local level. The BPP is taking these goals and turning them into a list of implementable projects.



Long Range Transportation Plan

The 2035 Long Range Transportation Plan (LRTP) provided a road map of planned future infrastructure that the BPP used to identify locations where more safety and connectivity could be provided. In turn, the development of this plan contributes to many of the active transportation goals and strategies within the LRTP.

- » Goal Two: Provide Transportation Choices
 - » The BPP satisfies the strategy “Development of an Active Transportation Plan” and will be adopted as a chapter of the LRTP.
- » Goal Three: Improve Connectivity
- » Goal Four: Improve Mobility
- » Goal Five: Safety — Make Transportation Safer

Benefits of Nonmotorized Transportation

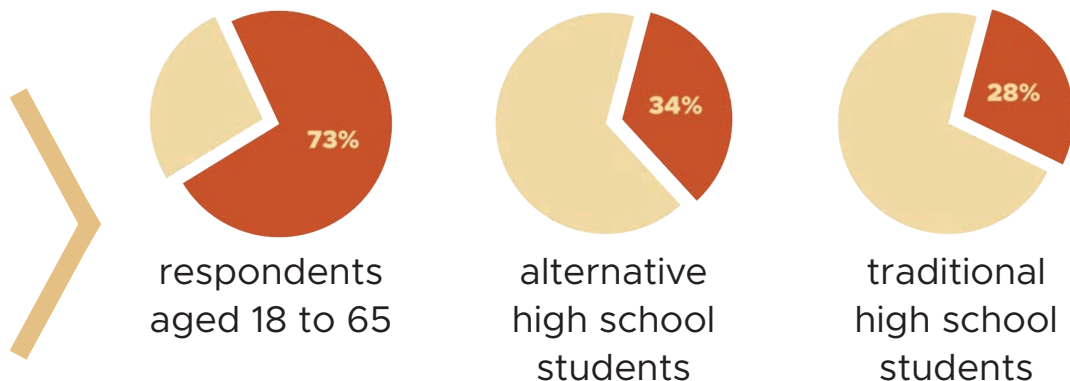


“The economic impact from trails is highest when a trail is connected to local businesses that cater to trail user needs, including restaurants, grocery stores, camping, and hotels, guiding services, and gear stores.”
(*Economic Impact Report*)

A well-connected and well-maintained bicycle and pedestrian network does more than just provide alternative transportation opportunities. It creates a greater sense of belonging and community among residents and entices visitors to stay in the area longer. The alternative modes of transportation that require bike and pedestrian trails improve both physical and mental health and can help prevent long-term health concerns such as heart disease, strokes, and diabetes. By

creating a safe and complete bicycle and pedestrian transportation network, we can uplift the whole MSB. Read the full *Economic Impact Report* in Appendix A.

**Self-reported
as overweight
or obese**



(*Mat-Su Health Foundation, 2019*)



Vision

The Matanuska-Susitna Borough envisions equitable access to a safe bike and pedestrian transportation network where residents and visitors of all ages and abilities enjoy an improved quality of life through **healthier, better-connected communities.**

Goals

- » Inventory and document the bike and pedestrian network to identify gaps and deficiencies.
- » Review MSB Code, the MSB Subdivision Construction Manual, and MSB Policy to identify potential changes that will help implement the plan's recommendations.
- » Create a prioritized list of projects to start building out the bike and pedestrian network.
- » Educate the public on the vision and goals of the BPP.
- » Solicit public input on the BPP's gap analysis and other findings.
- » Identify funding mechanisms to help implement the BPP's recommendations.

How was the BPP developed?

Outreach

Due to the nature of the BPP, much of its development was generated through community outreach and feedback. It was important that users of the network provide their experiences while using current plans to create a more robust system that allows for more connections, recreational opportunities, and safe routes.

Steering Committee

The Steering Committee was created in the first quarter of 2022 and held its first meeting in March. The committee consisted of the following 10 community members, who represent a wide range of backgrounds and familiarity with the bike and pedestrian network:

Adam Bradway *MSB Planner*

Jenny Willardson *Elevate Alaska Realty, Commercial Sales & Leasing*

Jim Beck *Mat-Su Health Foundation*

Joe Metzger *MSB Land Management*

Joshua Shaver *Alaska Pioneer Homes*

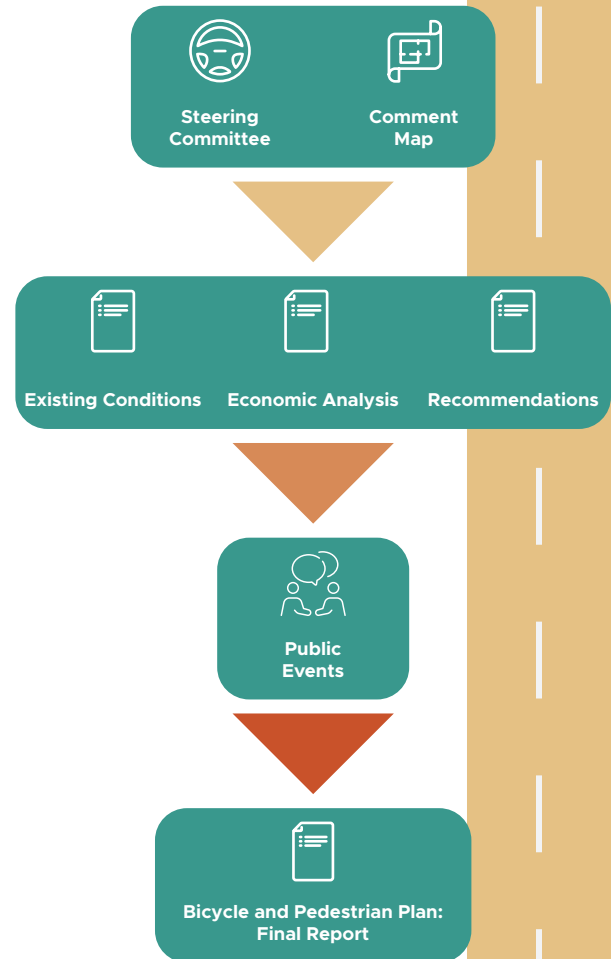
Julius Adolfsson *DOT&PF Statewide Bike and Pedestrian Coordinator, Rural Transit Planner*

Mike Campfield *MSB Public Works*

Patricia Owens *Parks, Recreation, and Trails Advisory Board*

Tracy Kalytiak *Mat-Su Health Foundation*

Wes Hoskins *Mat-Su Trails and Parks Foundation*



The Steering Committee met on a semi-monthly basis, with a long holiday break. The following highlights are from the Steering Committee meetings:

- » Assistance in providing information on problem areas
- » Recommendations of events to spread the word of the BPP
- » Feedback and edits on reports generated through the project

Comment Map

The comment map was made available to the public in May 2022. The map was an online interactive outlet for participants to comment on specific areas that needed the attention of the BPP. The comments were viewable by others that used the map and users were able to “like” or “dislike” the comments, which made the feedback more compelling. In total, the comment map acquired 137 unique comments, 1204 likes, and 10 dislikes before it was closed in September 2022.



Comments per Community

Big Lake	1
Butte	15
Farm Loop	5
Fishhook	3
Gateway	5
Greater Palmer	13
Houston	0
Knik-Fairview	11
Lazy Mountain	1
Meadow Lakes	3
North Lake	16
Palmer	20
South Lake	6
Sutton	5
Talkeetna	2
Tanaina	10
Trapper Creek	1
Wasilla	19
Willow	1

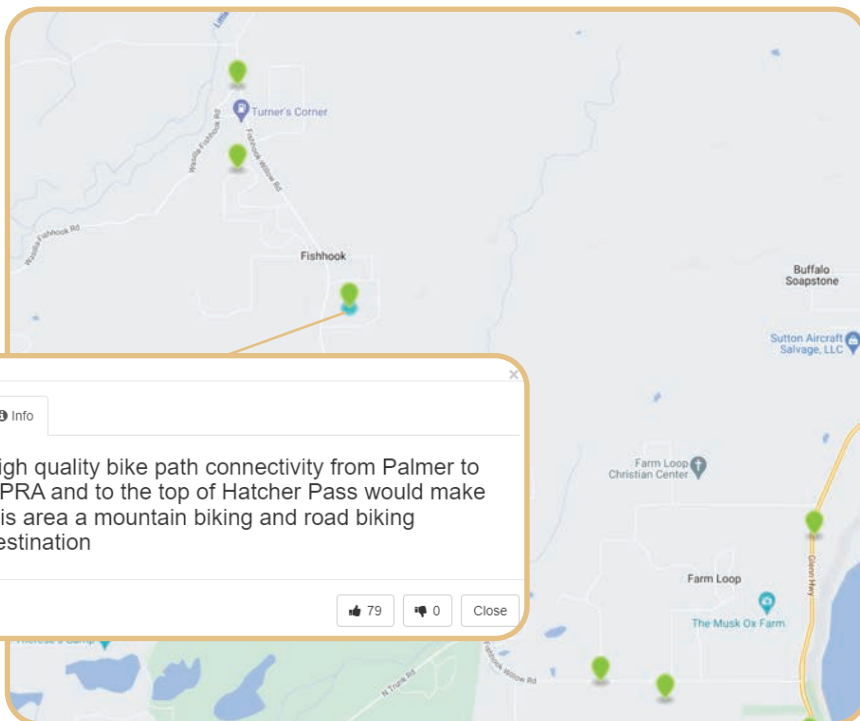




Photo credit: MSB Public Affairs Office

Public Engagement

Because of the size and varying terrain of the Mat-Su, residents use the bike and pedestrian network in many different ways, including for recreating, exercising, and commuting. The strategy behind the public outreach process for the BPP was aimed at capturing the full picture of where, when, and how users are on the roads, pathways, and trails. The lived experience is the driving force for this plan, and the recommendations are connected to real-life comments and known safety issues. For example, residents of Talkeetna frequently use the separated pathway along Talkeetna Spur Road, but they cannot cross the road safely to get to their public library from the pathway. The BPP is meant to provide recommended projects that bring value to communities, and

the public outreach process is focused on finding out what residents envision for the future of their bike and pedestrian system.

The BPP public outreach consisted of small group presentations, community council workshops, an interactive web map, a project website, frequent posts to the Planning Department's Facebook page, three interviews with Big Cabbage



Staff were present or provided information materials at the following community

events: State of Alaska Transportation Fair, Valley Mountain Bikers and Hikers Spring Bike and Gear Swap, Colony Days, Skeetawk Blueberry Ball, and the Palmer Spring Classic bike race.

Radio, several features in the Planning Department's newsletter (the Planner Platform), and stakeholder interviews. Planning staff reached out to 51 stakeholder groups, including all active community councils in the MSB, local businesses and chamber groups, tribal councils, and advisory boards.

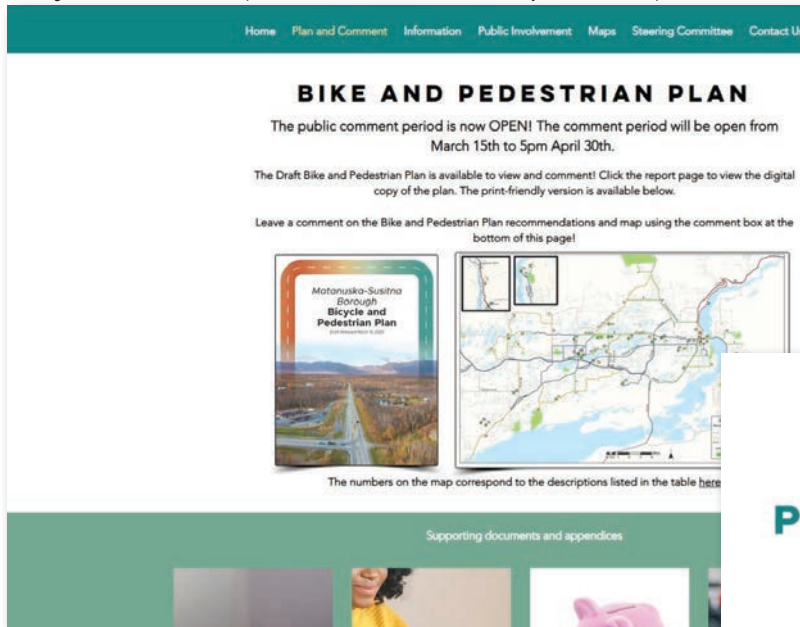
The following groups requested and received a presentation or written update on the BPP (see Appendix B for a full list of stakeholders who were contacted for comment):

- » Chickaloon Village Traditional Council
- » Knik Tribe
- » Talkeetna Community Council
- » Tanaina Community Council
- » Lazy Mountain Community Council
- » South Knik Community Council
- » North Lakes Community Council
- » Sutton Community Council
- » City of Wasilla
- » Backcountry Bike and Ski
- » Mat-Su Valley Board of Realtors
- » All About Herbs
- » Valley Mayors' & Managers' Meeting
- » Greater Wasilla Chamber of Commerce
- » MSB Parks, Recreation, & Trails Advisory Board
- » MSB Transportation Advisory Board
- » MSB Safe Routes to School Committee
- » MSB Health and Human Services Board
- » Valley Transit
- » Sunshine Transit
- » Chickaloon Area Transit Systems
- » Mat-Su Coalition on Housing and Homelessness
- » Alaska Climate Action Network
- » Valley Interfaith Action Group

Public Comment Period

The draft Bike and Pedestrian Plan was available for public comment between March 15 and April 30, 2023. Information about the public comment period was shared via Facebook, emails to community councils and key stakeholders, an email to the MSB School District mailing list, physical flyers in libraries, an announcement on Big Cabbage Radio, and an article in the Frontiersman. Backcountry Bike and Ski also included the link to the comment page on their Palmer Spring Classic entry form!

Project website (www.matsubikeandped.com)



Very excited to see this! Some sections are ones I use frequently as I [use] my bicycles as forms of transportation.

Public comment period flyer



As a regular user of some of the new and also long-existing cycle paths around the Palmer area, I plan to use the paths in the plans for both recreation and as able, as an alternative to driving for work commutes and running errands.

A total of 72 comments were received through the project website, Facebook, and emails to the BPP project team. The team was also forwarded a letter to the Alaska Department of Transportation & Public Facilities (DOT&PF) regarding the crosswalk near the Talkeetna Library.

Comments were reviewed to identify suggested routes for new facilities and alterations to projects already listed in the Implementation Plan. The list of new recommendations was evaluated by the Steering Committee to determine which suggestions would be incorporated into the final BPP.

30

new projects in the
Implementation
Plan

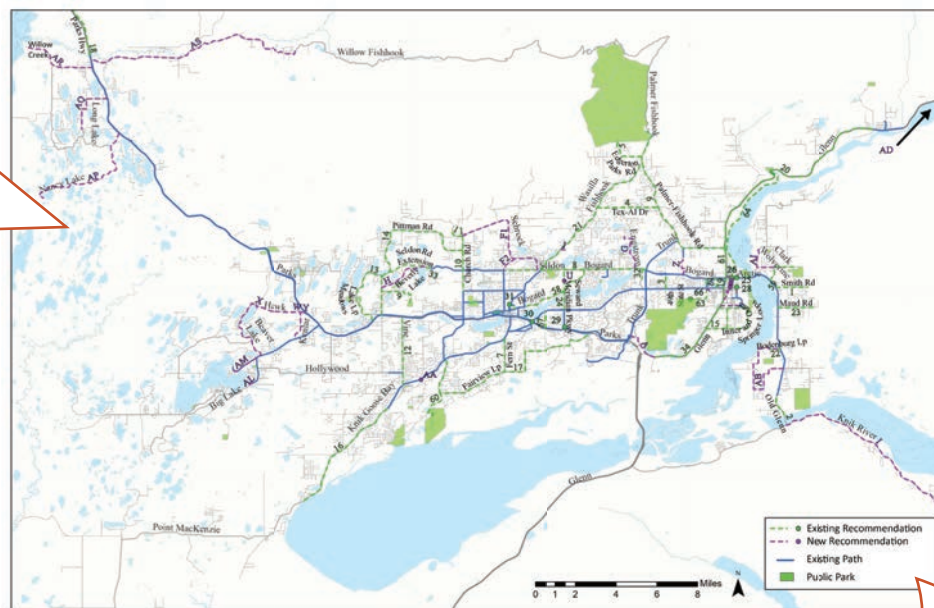
12

comments supporting
the crosswalk near the
Talkeetna Library - the
most received for any
one project

28

comments
expressing
support for
the BPP as a
whole

I am mainly
concerned
about kids'
safety.



I also fully support an ADA sidewalk assessment. When my children were in a stroller, I often found it difficult to navigate steep or nonexistent curb cutdowns across Palmer. Even now, it can be especially challenging with young kids to navigate soft shoulders and nonexistent sidewalks.

How do I use the BPP?



As a resident

- » Advocate for the bike and pedestrian facilities you want to see and the programs you want to make happen.
- » Leverage the BPP for grant applications and community funding opportunities.
- » Nominate projects from the BPP to the Transportation Improvement Program (TIP) and the Capital Improvement Program (CIP).



As an MSB employee

- » Evaluate and include bike and pedestrian infrastructure in transportation bond packages.
- » Set department, organization, and legislative priorities.
- » Advocate for local, state, and federal funds.
- » Implement the policies, programs, and infrastructure projects outlined on pages 36 to 48.



As a private sector developer or investor

- » Learn more about community-supported network connections and bike and pedestrian infrastructure design and standards.
- » Leverage the *Economic Impact Report* (Appendix A) and *Existing Conditions and Issues Report* (Appendix C) to inform your development decisions.



A house in the MSB valued at \$285,000 could have a realized value **increase of \$8,550 to \$14,250** if it were located near a trail (*Economic Impact Report*, Appendix A).



As a business owner

- » Advocate for changes and improvements that will make nonmotorized access to your local business easier and safer.



As a City or MSB official

- » Inform your policy decisions regarding transportation and land-use issues within the borough. The BPP reflects community priorities that were built on extensive public outreach and expert analyses of the borough-wide transportation network, so it provides useful insights for policy makers.

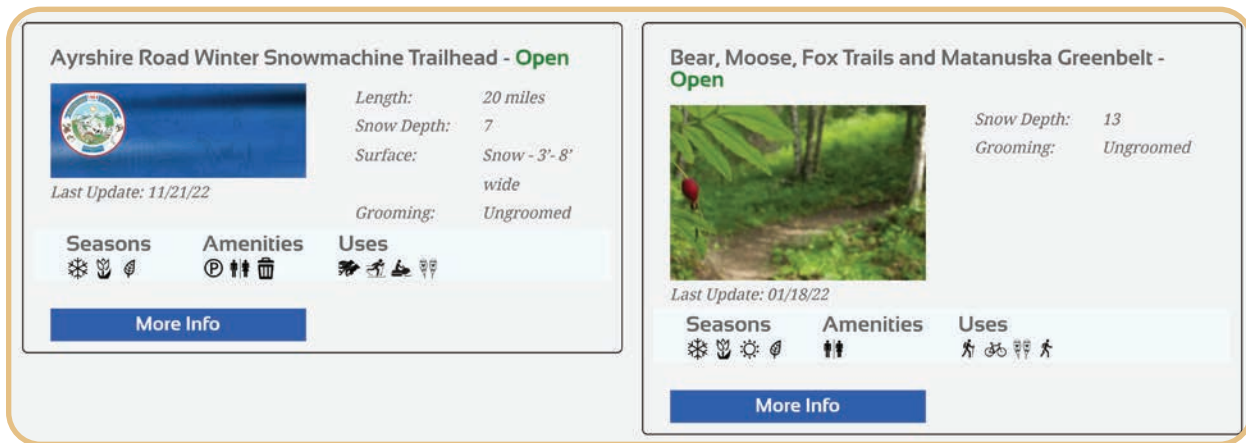
Recreation Trails

One of the goals of the BPP is to connect people to recreation opportunities throughout the MSB. The BPP does not contain recommendations for creating new recreation trails, but the resources below provide information about existing trails throughout the borough.

MSB Trails Page

See which trails are open, what amenities they have, and more!

<https://matsugov.us/trails>



Mat-Su Trails and Parks Foundation Winter Trails and Parks Guide

This guide includes a map of parks and recreation areas, sledding adventures, and “can’t miss” winter trails! This guide also provides links to more resources.

<https://matsutrails.org/>

Matanuska-Susitna Borough Recreation Trails Plan

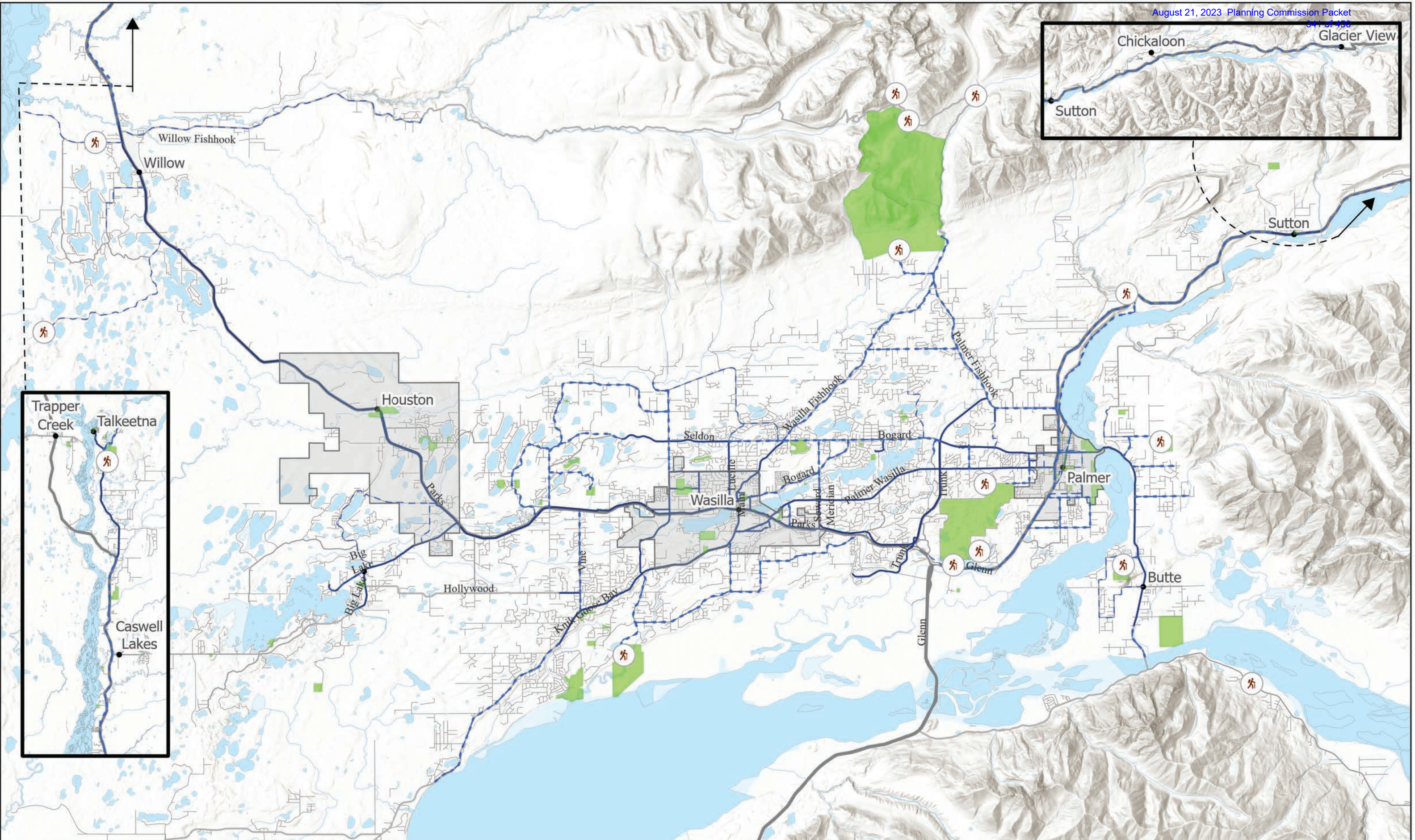
This plan is the official guiding document for recreation trails owned by the MSB.

<https://matsugov.us/plans>

Trails and Parks Master Plan for the Mat-Su Area

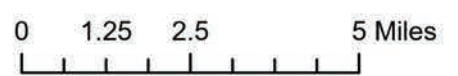
Learn more about recreation trails and parks planning in the region from the Mat-Su Trails and Parks Foundation.

<https://matsutrails.org/master-plan-trail-building-resources/>



Trailheads and Parks in the MSB

- Existing Separated Path
- Recommended Path
- Local Park
- Trailhead
- City Boundary



-Existing Conditions and Issues-

The following is a summary of the 2022 *Existing Conditions and Issues Report* (Appendix C).

Bike and Pedestrian Facilities

The nonmotorized transportation network in the MSB contains the following types of bike and pedestrian facilities:

Separated Pathways: Separated pathways are made of asphalt and connect larger populated areas that run parallel to high-volume roadways.

Bike Lane: Bike lanes are intended to be exclusively for bicycle traffic. The City of Palmer has bike lanes.

Paved Shoulder: Paved shoulders can function similarly to bike lanes. They are shared use and can include rumble strips.

Sidewalk: Sidewalks are typically used for pedestrian traffic in urban areas. The Cities of Palmer and Wasilla have sidewalks in parts of their respective city centers. Typically, bicycles are not allowed on sidewalks. MSB subdivision code does not require the construction of sidewalks when a residential subdivision is developed.

Shared Roadways: Shared roadways have no delineation between vehicle, bike, and pedestrian use and are common throughout the MSB.

Transit: The majority of transit services in the MSB are currently provided through a demand-response model instead of the fixed-route services typical of many transit systems. Bike and pedestrian facilities are frequently used to create connectivity within transit networks. However, with the gaps created by a demand-response service, developing a route-based, scheduled service is needed to fully support the community's needs. The MSB is in the beginning stages of developing a Transit Development Plan

that will address the issue of connectivity on a broader scale throughout the borough.

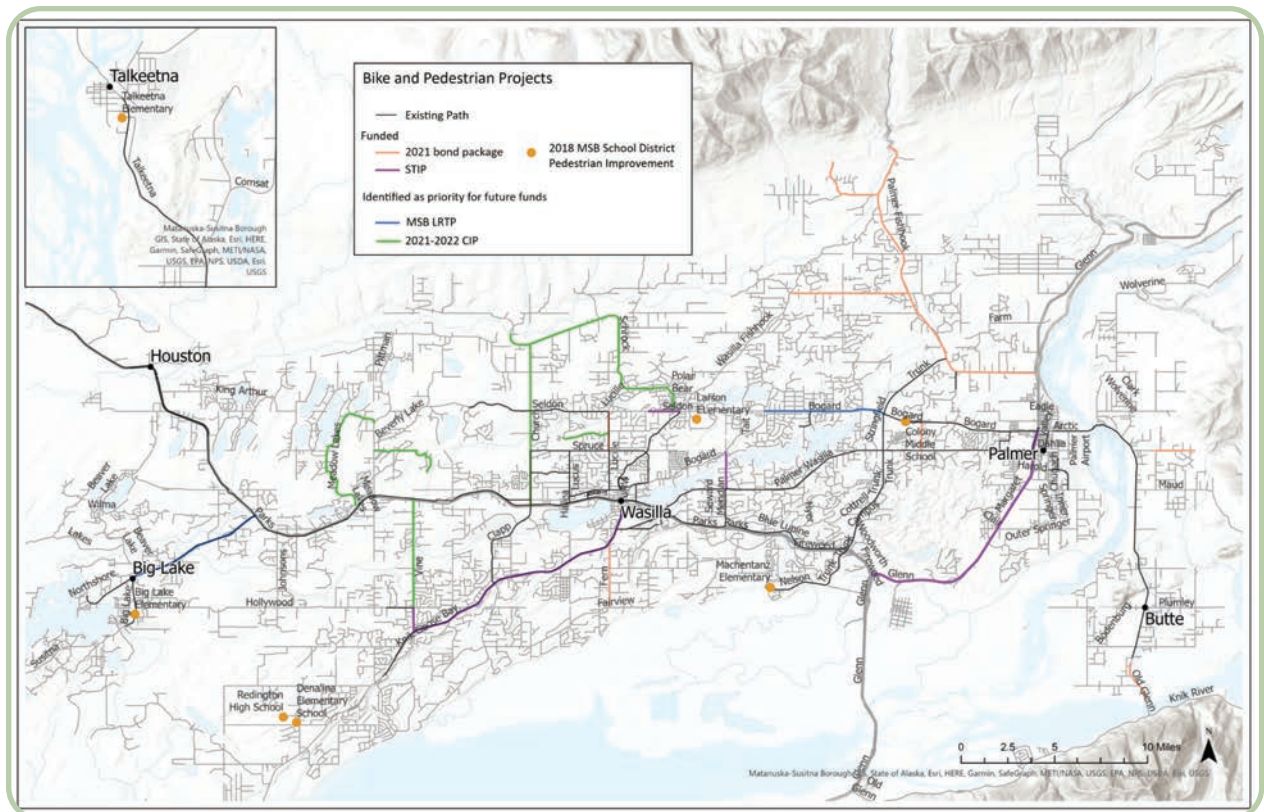
Related Plans, Projects, and Policies

Several plans at the state, borough, and community levels were reviewed in the development of this plan. The goals and strategies of each plan were evaluated to determine how they relate to one another and how they could be used to support the BPP. Key plans included the MSB Official Streets and Highways Plan, the MSB LRTP, the MSB Trails Plan, and Safe Routes to School Plan.

For the full list of plans, please see *Section 3.1 Plans* in the 2022 *Existing Conditions and Issues Report* (Appendix C).

Current and Planned Projects

Through cooperative efforts between communities, cities, MSB, and the state, several projects are planned and several projects that are included in the BPP are already designed and funded. See Appendix E for a full-page version of the map below.

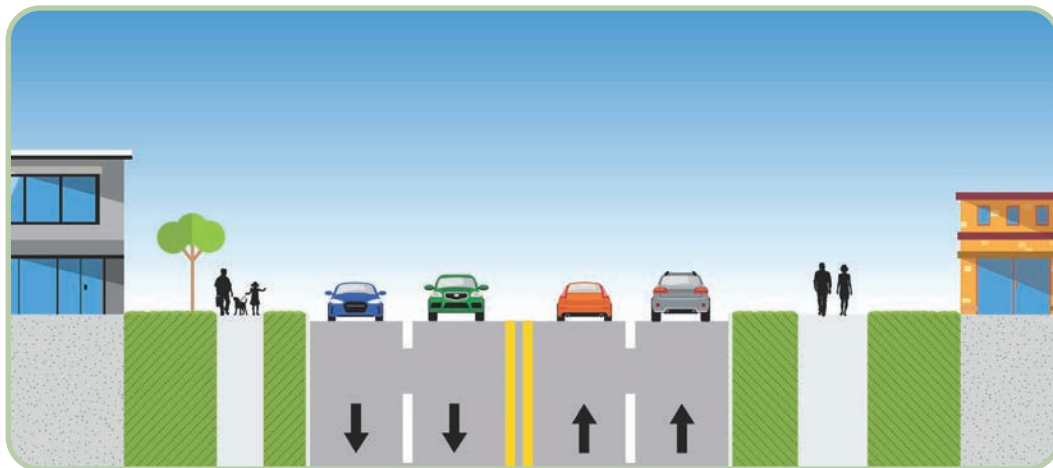


Policies and Regulations

In addition to federal regulations that guide state and federal construction of roads, some cities in the borough have design standards for bike and pedestrian facilities (see figure below). The MSB does not have requirements for developers to construct bike and pedestrian facilities.

City of Wasilla downtown overlay district sidewalk requirements				
Corridor	Min. distance from back of curb (BOC)	Paved furniture zone at BOC	Sidewalk req. both sides	Min. width of sidewalk
Avenues	2'	NA	Y	6'
Parkways	2'	NA	Y	5'
Boulevards	4.5'	NA	Y	5'
Main Streets	4'	4'	Y	8'
Urban Streets	4'	4'	Y	6'
Local Streets	3'	NA	Y	5'
Alleys	NA	NA	NA	NA

On a local level, Complete Streets policies are enacted for the safety of all users. The Federal Highway Administration (FHWA) and National Complete Streets Coalition support community development of the Complete Streets policy. The conceptual street cross-section below is an example of an urban arterial road provided in an FHWA guidance document; Complete Streets designs are tailored to a specific area and are not one-size-fits-all.



For a full list and summaries of all policies and regulations please see *Section 3.3 Policies and Regulations* of the 2022 *Existing Conditions and Issues Report* (Appendix C).

Issues and Needs

Crash Data

DOT&PF collects data regarding crashes involving bicycle and pedestrian injuries or fatalities. From 2010 to 2019, there were 103 incidents within the MSB, 33% of which resulted in a serious injury or fatality (DOT&PF, 2019). The incidents were distributed nearly evenly between cyclists and pedestrians. Due to higher populations and traffic volume in the areas, more incidents were reported in Wasilla and Palmer than in surrounding areas. Crashes tended to occur during daylight hours, with July having the highest frequency of incidents. The figure below shows locations of crashes throughout the borough. See Appendix D for a full-page view of the map.



Connectivity

Throughout the bike and pedestrian network, gaps were identified through analysis, a comment map, stakeholder interviews, and planned projects.

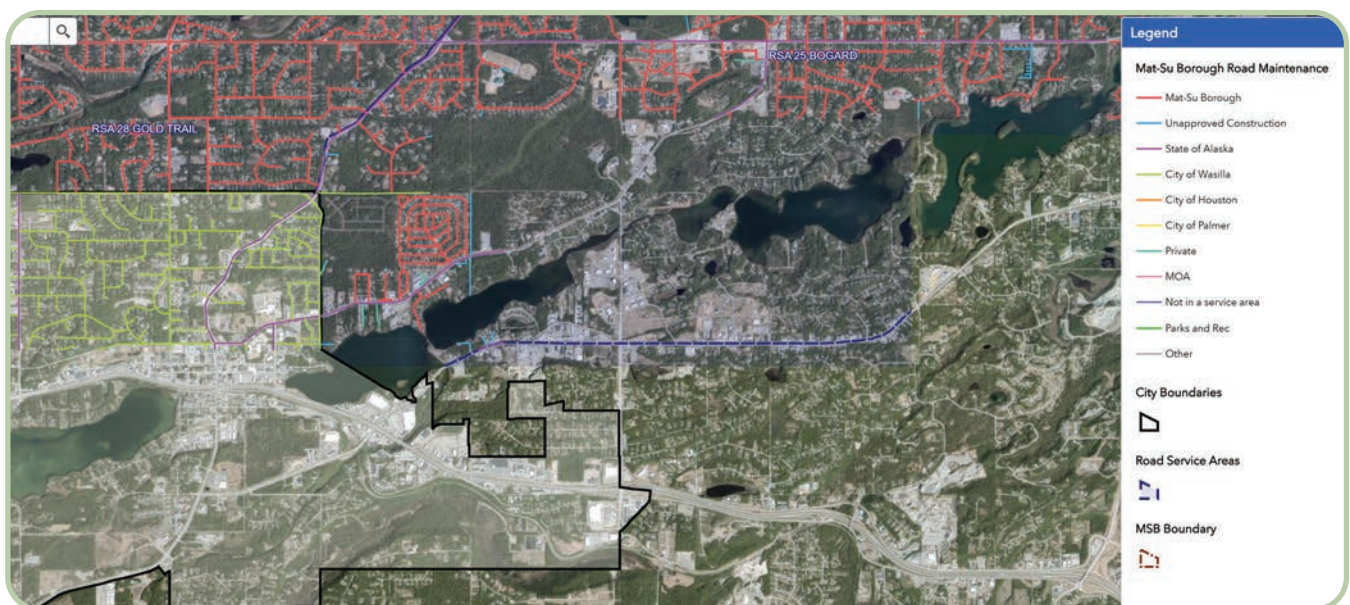
Maintenance

Throughout the development of the BPP, concerns about maintenance were raised repeatedly. Maintenance is an issue no matter the season,

whether it be snow-burdened pathways and narrowed roadways from snow plowing, gravel from winter on pathways during the summer, or year-round cracks and frost heaves.

DOT&PF prioritizes road maintenance based on the functional class of the roadway—the higher the classification, the higher the priority. Pathways along the roadway are given the same priority as the roadway itself, but the rate of maintenance is not always equal.

The MSB contracts its road maintenance to private parties through the Road Service Area (RSA) system. The borough oversees the maintenance of roughly 1,100 miles of roadways and prioritizes based on roadway classification (see map below), with subdivision roads having the lowest priority. Plowing is required after 4 or more inches of snowfall and must occur within 12 hours on school bus routes and main roadways. The projects recommended later in this plan would add a total of 134 miles of pathways for winter maintenance.



MSB Road Maintenance Map via MSB Interactive Maps & Apps

Wasilla and Houston prioritize their main city roads, and Palmer prioritizes its arterial roads and school routes. Neither Wasilla nor Houston have written regulations for snow removal from sidewalks. Palmer requires building owners or occupants to remove snow from their sidewalks, while the city removes snow from sidewalks surrounding city buildings.

User Conflicts

Comments from the public indicate that the most common user conflicts on bike and pedestrian paths in the Mat-Su are between vehicles (including ATVs) and bicycles. This is due to the absence or poor maintenance of pathways for cyclists, which causes cyclists to ride on roads, as well as the use of ATVs on paths intended for nonmotorized use. Paths and trails throughout the MSB are also used by horseback riders and sled dog teams.

All users should verify what activities are allowed on paths and trails by checking local ordinances and trail information. Generally, *paths* are for bicycle and pedestrian use and *trails* may have additional permitted uses.

For more information, see:

- » Recreation trails resources, page 21
- » Wasilla Municipal Code 10.12.020
- » Palmer Municipal Code 10.05.020
- » MSB Code 24.07.050

Photo credit: MSB Public Affairs Office



Funding

Funding for the BPP can come from several sources and be distributed to multiple entities. The BPP implementation plan may be able to receive funding for standalone projects or may be included in larger projects to be eligible for a wider range of funding opportunities. Examples of funding sources for which BPP projects are potentially eligible are listed below; these are not guaranteed sources of funding. See Appendix F for a list of grant opportunities for which BPP projects may be eligible.

Federal

There are many funding opportunities for bike and pedestrian infrastructure from federal sources. The Bipartisan Infrastructure Law (BIL, also known as the Infrastructure Investment and Jobs Act or IIJA) renewed funding for several federal programs. The following federal sources are most relevant to the BPP: the **Surface Transportation Block Grant (STBG)**, **Safe Streets and Roads for All**, and the **Carbon Reduction Program**.

Tribal

As federally recognized tribes, the Chickaloon Native Village and Knik Tribe are eligible to apply for funds from the **Tribal Transportation Program** and the **Tribal Transportation Program Safety Fund**, among other programs dedicated to Tribal transportation. Federally recognized tribes are also eligible to apply for other state and federal transportation programs.

State

DOT&PF provides state funding for transportation projects and administers and manages federal funds for individual projects. The state-funded **Community Transportation Program** is a competitive surface transportation program that runs in 3-year cycles. DOT&PF also provides funding for selected projects through the federal **STBG Transportation**

Alternatives Set-Aside, using funds that are not otherwise suballocated to Metropolitan Planning Organizations (MPOs).

Borough

Funding opportunities for projects throughout the MSB are available through Alaska **Statewide Transportation Improvement Program** (STIP), RSAs, **voter-approved transportation packages** (including bond packages), and **grants**. The MSB does not currently have road powers; therefore, funding for road projects is largely facilitated through community-driven bond requests. Bond packages must be approved by voters as the funding will be provided by the taxpayers.

Cities

Although the cities of Palmer, Wasilla, and Houston can seek **state, bond, and grant** funding similarly to the MSB, each city has its own respective CIP to use for improvements within city limits. The CIP nomination is simple—the city opens the nomination to the public and members of the public can provide their reason that the nomination is beneficial to the city.

Metropolitan Planning Organization

Mat-Su Valley Planning for Transportation (MVP) is in the process of forming an MPO that will be responsible for short- and long-term transportation planning for a defined area within the MSB. The MPO boundary will be finalized in Summer 2023 and the organization is scheduled to be fully formed in Fall 2023. Once it is fully formed, the MPO will be eligible to receive funds and engage in transportation planning (note: the MPO will not have ownership of any infrastructure, but MPO funding can be used on bike and pedestrian facilities as well as equipment to maintain new infrastructure). Learn more about the MPO and its formation at www.mvpmpo.com.



Implementation

The core of the BPP is the implementation plan for the nonmotorized transportation network of the MSB. The implementation plan was created by evaluating the existing conditions and issues in the MSB, public input, and existing plans and projects. The plan is divided into the categories of Policy, Infrastructure, and Programs and Planning; details about each project can be found on pages 36 through 48. Pages 32-35 show the locations of the infrastructure projects.

Who implements the BPP?

Many projects will likely have multiple partners responsible for implementing them, including the MSB, cities, the MVP MPO, and DOT&PF. The Projects to Implement tables on pages 36-48 identify the parties that will likely be responsible for each project.

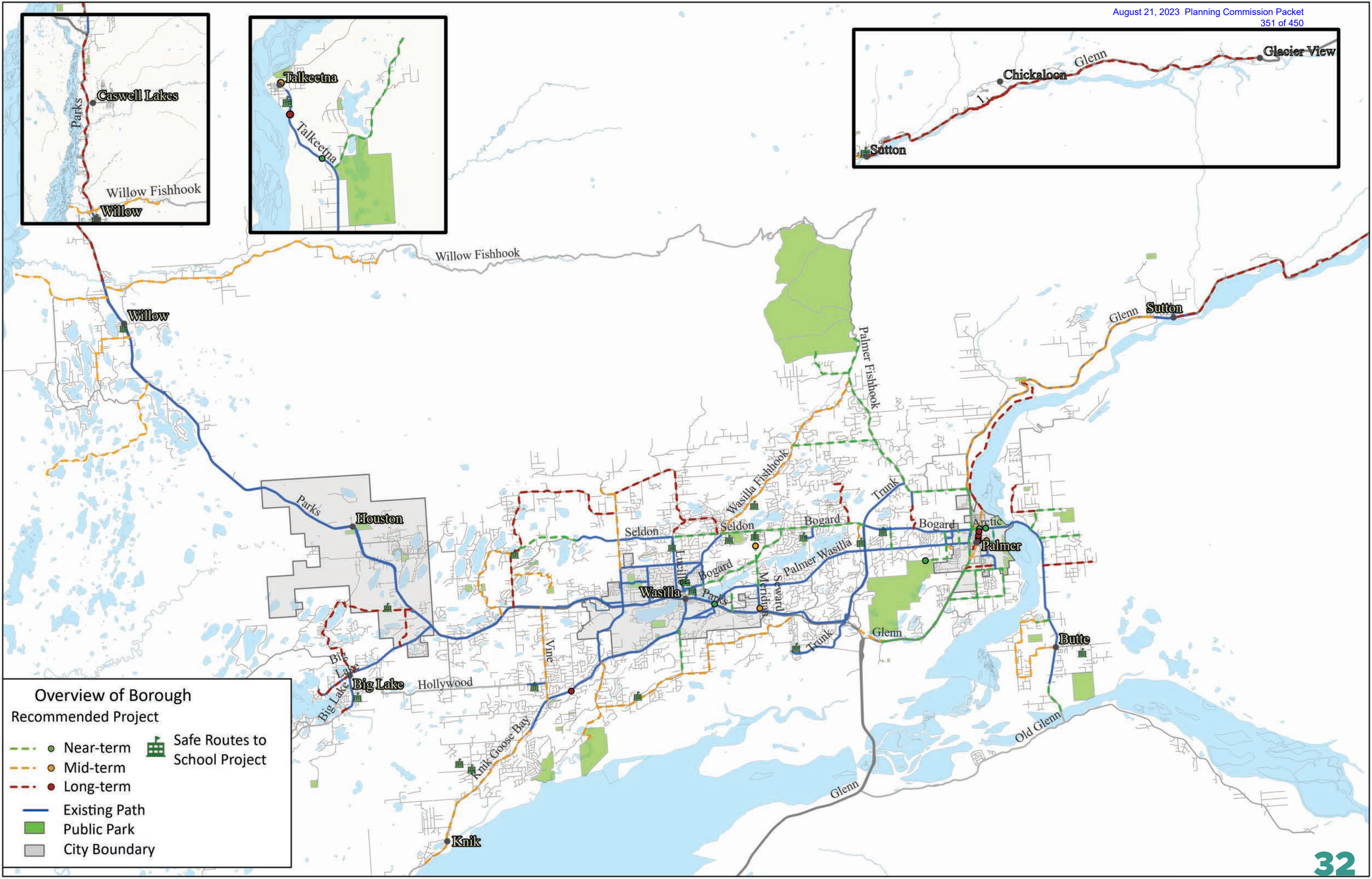
When are the recommendations implemented?

Each recommendation has a suggested timeframe for implementation based on safety considerations such as crash data and proximity to schools, review of existing plans and funding strategies, direction from the Steering Committee, and public comments. Projects that were already designed, under construction, or included in a funding plan were included as “near-term” to show ongoing support for their prioritization. Some projects may require additional studies or analyses prior to implementation. “Mid-term” projects will need additional support or study before being added to an infrastructure list. “Long-term” are typically larger and will require environmental engineering studies or involve difficult right-of-way acquisition challenges. The recommendations in the BPP assume that right-of-way has been or is easily obtained for facilities projects; if right-of-way is difficult to obtain, project timelines may shift. The recommendations are not a guarantee that the project will be completed within that timeframe.

Near-term within 0-5 years
47 projects

Mid-term within 6-10 years
27 projects

Long-term more than 10 years
21 projects



Overview of Borough
Recommended Project

Near-term

Mid-term

Long-term

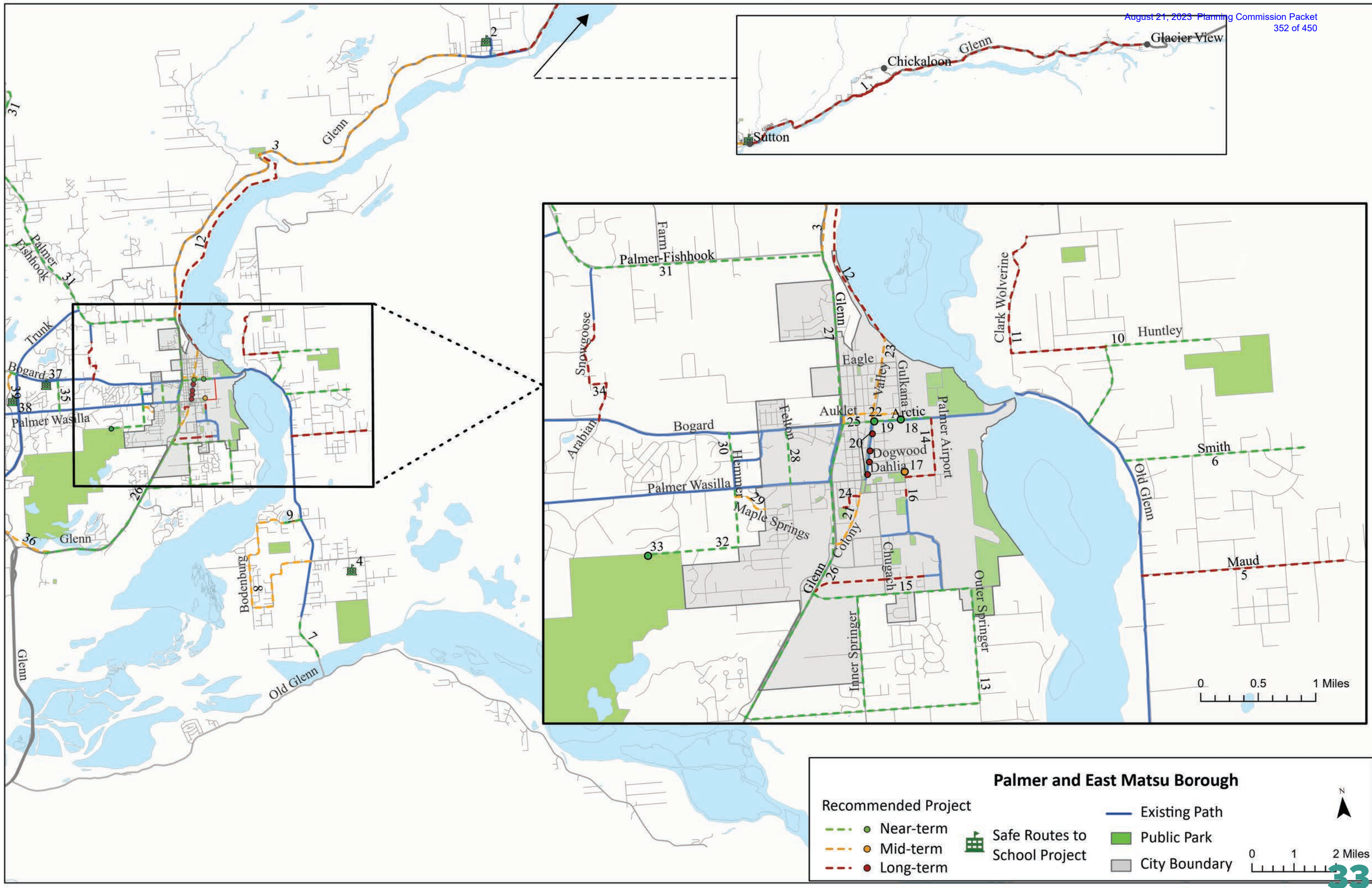
Existing Path

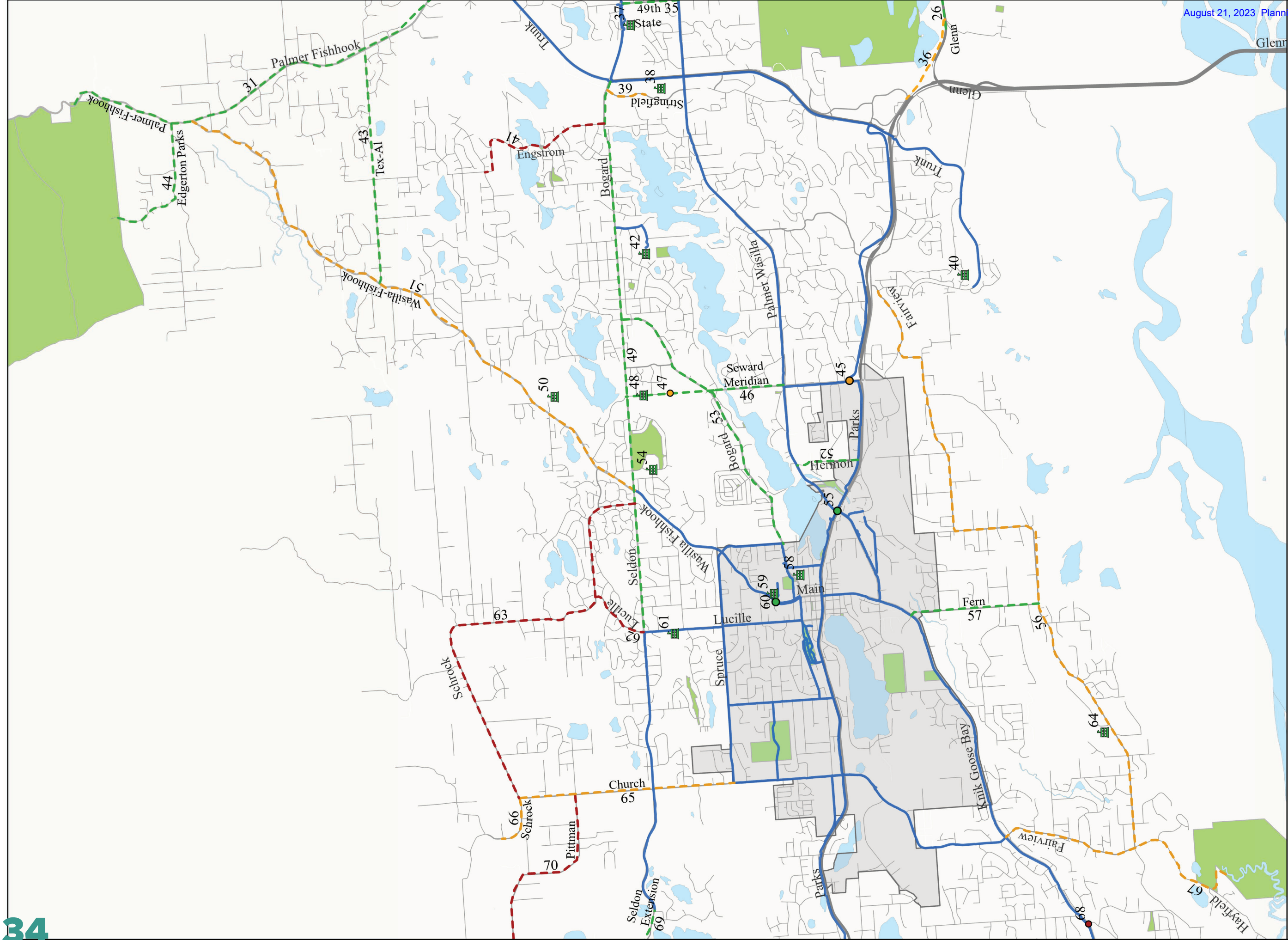
Public Park

City Boundary

Safe Routes to
School Project

32



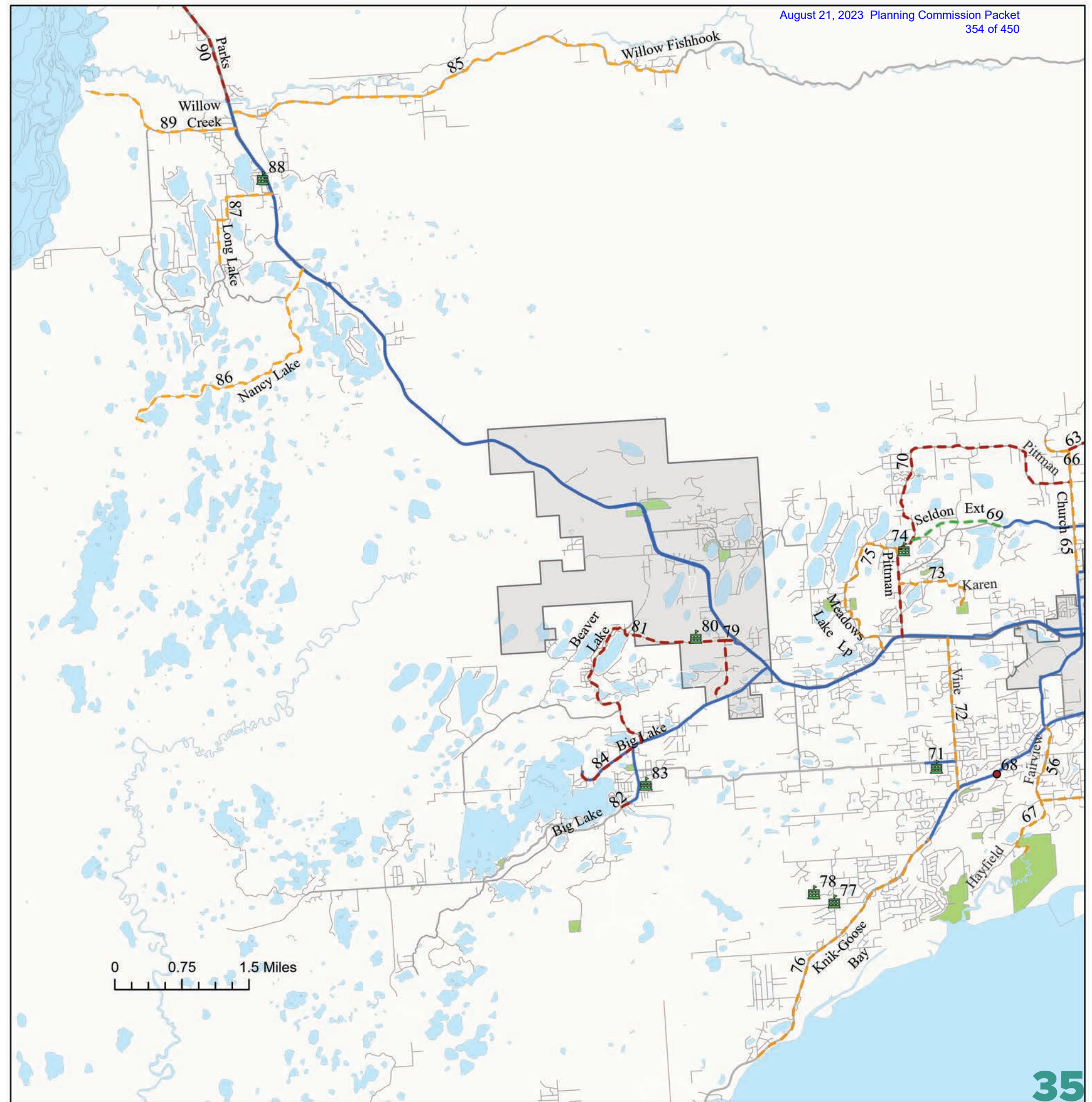
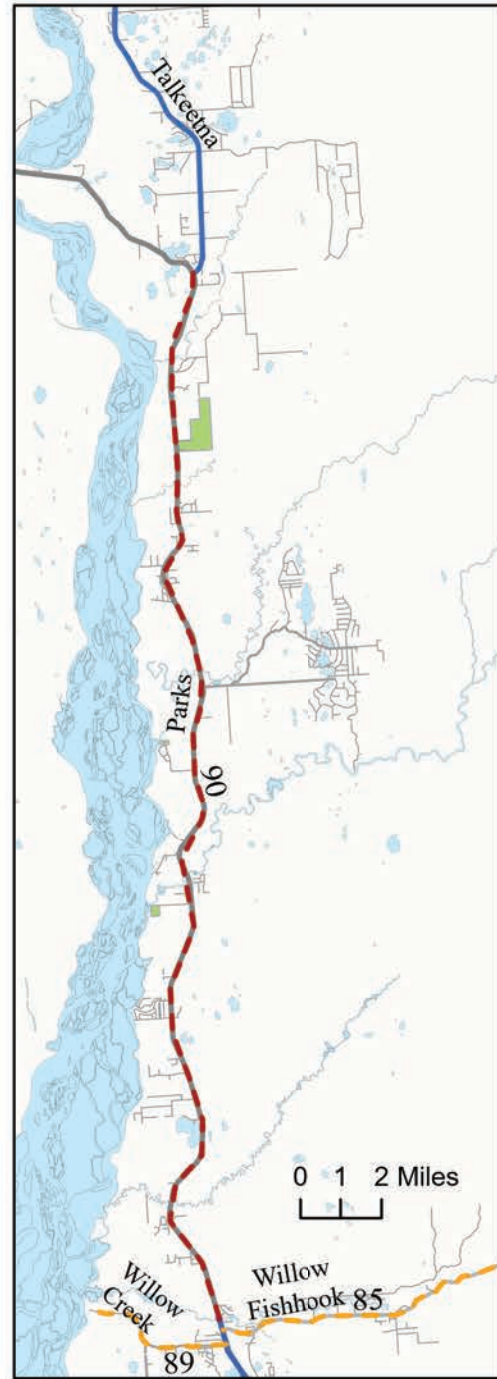


Recommended Project

- Near-term
- Mid-term
- Long-term
- Safe Routes to School Project

Wasilla and Central Matsu Borough

- Existing Path
- Public Park
- City Boundary



Projects to Implement

Policy

Policies to facilitate bicycle or pedestrian infrastructure and maintenance provide accountability and ensure that all user groups are accommodated in the transportation system. Policies can also improve the planning process by prescribing preferred facilities through design standards.

Recommendation	Description	Rationale	Authority
Near-term			
Facility design standards	Develop standard drawings and specifications for bicycle and pedestrian facilities. Write legislation for adoption by Assembly defining guidelines for pedestrian facilities based on road classification.	A standard design gives engineers and platting staff a basis for nonmotorized facility design. A standard design also helps with cost estimating and ensuring consistent infrastructure across the borough, which provides users a consistent experience that translates to fewer crashes. These standards should take into consideration environmental factors and surrounding infrastructure, such as culverts or wetlands.	MSB
Complete Streets policy	Craft a Complete Streets policy that includes all users.	A Complete Streets policy establishes an approach to planning, platting, designing, operating, and maintaining streets that enable safe access for all people who need to use them.	MSB/MPO/Cities
Snow-clearing policy	Adopt a snow-removal policy for nonmotorized infrastructure across the borough and across jurisdictions.	Removing snow from paths, sidewalks, and road shoulders provides year-round use for all users.	MSB/MPO/Cities
Maintenance policy	Adopt a general maintenance policy for nonmotorized infrastructure across the borough and across jurisdictions for debris removal, sweeping, and pavement patching.	Gravel and other debris on sidewalks and pathways can impede many nonmotorized users, including wheelchairs or other assistive devices, narrow-tired bicycles, and rollerblades and skateboards.	MSB/Cities/ DOT&PF
Subdivision regulations	Revise MSB Code to include pedestrian infrastructure when subdivisions are created; require safe route to school when building or subdividing within 1/2 mile from a school; distinguish between urban and rural contexts.	Current MSB subdivision code (Title 43) requires consideration of 'walkways' but does not require their dedication or construction. Code language that is more stringent would require pedestrian facilities be included in the subdivision and thus help eliminate the creation of new gaps in the nonmotorized network every time a new subdivision is created.	MSB
Vision Zero program	Coordinate the adoption of the international Vision Zero program.	Vision Zero seeks to reduce fatalities and serious injuries while ensuring mobility for all.	All Organizations

Recommendation	Description	Rationale	Authority
Include bike and pedestrian plans in the TIP	At least 20% of the MSB TIP and MVP TIP should be bike and pedestrian projects.	Both the Anchorage and Fairbanks MPOs require 20% of their TIP projects to be bike and pedestrian projects.	MPO/MSB

Infrastructure

All recommendations assume that right-of-way is available or easily obtainable for the proposed facility. Authorities have varying responsibilities for construction, maintenance, and/or funding. The project numbers correspond to the implementation map and do not indicate priority.

Recommendation	Description	Rationale	Authority	Timeframe
1. Widen Shoulder of Glenn Highway from Sutton to Glacier View	Create a path along Glenn Highway from Fish Lake Road to Chickaloon Branch Road	Proposed by public comment and approved by Steering Committee	DOT	Long-term
2. Sutton Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term
3. Glenn Highway path	Develop a separated path along Glenn Highway between Palmer Fishhook Road and Jonesville Road	Connecting Palmer-Fishhook Road to the separated path at Callison Road in Sutton would provide connectivity between Sutton and Palmer. This facility is also proposed in the Palmer Comprehensive Plan	DOT	Mid-term
4. Butte Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term
5. Maud Road path	Construct a separated pathway along Maud Road from Old Glenn Highway to the end of the paved portion of Maud Road.	Development in the area has increased traffic on Maud Road. The road is narrow with narrow shoulders. A separated non-motorized path would connect residential areas to the Old Glenn Highway path.	DOT	Mid-term
6. Smith Road Extension pathway	Provide a pathway connection between the Old Glenn Highway and the recreational trailhead access	This project is part of the 2021 Transportation System Package	DOT/MSB	Near-term
7. Fill the gap in the Old Glenn Highway path between Sodak Circle and Knik River	Connect the separated pathway on Old Glenn Highway between Sodak Circle and Knik River	This project is part of the 2021 Transportation System Package	DOT	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
8. Bodenburg Loop shoulder widening - Entire Loop	Widen the shoulder on the entire Bodenburg Loop Rd	The Butte Trail is a signification recreational destination. Connecting the trailhead to the Old Glenn Highway path would fill the gap along Bodenburg Loop.	DOT	Mid-term
9. Bodenburg Loop shoulder widening - To trailhead	Widen the shoulder on the north side of Bodenburg Loop Road between Old Glenn Highway and the Butte trailhead to six feet to accommodate cyclists and pedestrians	The Butte Trail is a signification recreational destination. Connecting the trailhead to the Old Glenn Highway path would fill the gap along Bodenburg Loop.	DOT	Near-term
10. Lazy Mountain trail connection	Connect E Huntley Road to Lazy Mountain Trail Head.	Provide connection from pedestrian pathway to recreational trail.	MSB	Long-term
11. Clark Wolverine Pathway	Create a path along Clark-Wolverine Rd from the Old Glenn Hwy	Proposed by public comment and approved by Steering Committee	DOT	Long-term
12. Palmer to Sutton pathway along the Matanuska River	Reconstruct the existing railroad bed into a pathway. Stabilize slopes that are subject to erosion.	This pathway follows an old railway and is very popular but is often muddy and difficult to use.	MSB/City of Palmer/ARRC	Long-term
13. Inner and Outer Springer Loops shoulder widening	Widen the shoulders on both sides of these roads to 5'.	This area is projected to grow significantly over the next 20 years. There are no dedicated bicycle or pedestrian facilities along these roads. A dedicated facility would provide connections between residential areas and the core Palmer commercial area (via Chugach Street sidewalks).	City of Palmer/ DOT	Near-term
14. Evergreen Ave/ Airport Road pathway	Construct a pathway along Evergreen Avenue and Palmer Airport Road, creating a connection from Gulkana Street to Arctic Avenue	Already designed and planned to be constructed	City of Palmer	Near-Term
15. Palmer East/West Abandoned rail line pathway	Turn the abandoned railroad between Thuma St and Inner Springer Loop into a bike/ pedestrian trail	Proposed by public comment and approved by Steering Committee	City of Palmer/ ARRC	Long-term
16. S Gulkana Pathway extension north	Create a path along S Gulkana St from E Fireweed Ave to the existing path near the Dr. Myron F. Babb Arboretum	Proposed by public comment and approved by Steering Committee	City of Palmer	Long-term

Recommendation	Description	Rationale	Authority	Timeframe
17. South Gulkana Street crossings	Provide pedestrian crossing facilities at South Gulkana Street intersections with East Dahlia Avenue and East Elmwood Avenue.	The large recreational fields between Elmwood and Dahlia are a destination for pedestrians and cyclists coming from neighborhoods east of South Gulkana Ave. Gulkana Ave traffic does not have to stop at these intersections and bushes/shrubbery make it difficult for motorists to see pedestrians.	City of Palmer	Mid-term
18. Arctic Avenue at Gulkana Street crossing	Provide a pedestrian crossing at this unsignalized intersection	Pedestrians and cyclists heading south on Gulkana Street must cross Arctic Avenue to reach the separated path on the south side of Arctic. Traffic on Arctic does not have to stop at the intersection with Gulkana Street.	City of Palmer/ DOT	Near-term
19. Arctic Avenue bike lanes	Provide bike lanes on both sides of Arctic Avenue between Glenn Highway and South Airport Road.	There are no dedicated bike facilities on the west end of East Arctic Avenue. Existing sidewalks should be used by pedestrians only. The shoulder is very narrow and doesn't not accommodate safe bicycle travel.	City of Palmer/ DOT	Mid-term
20. Valley Way Intersection Crosswalks	Create crosswalks/connections between the pathway along S. Valley Way and the other side of the road at Cottonwood Ave, Dogwood Ave, Blueberry Ave, and Dahlia Ave	Proposed by public comment and approved by Steering Committee	City of Palmer	Long-term
21. S Colony Way Pathway	Add a path along S Colony Way connecting the path at the intersection of the Glenn Highway/ East Harold Street (where E Harold St turns into S Colony Way) to the Palmer Visitor Information Center	Proposed by public comment and approved by Steering Committee	City of Palmer	Mid-term
22. Arctic Avenue at Valley Way crossing	Provide a pedestrian crossing of Arctic Avenue at the intersection with Valley Way.	There are no crossing facilities at this busy intersection that is not signalized. South Valley Way is a destination for walkers, bikers, and skateboarders, as it has a separated path and skate park.	City of Palmer/ DOT	Near-term
23. North Valley Way pathway	Pave and maintain the pathway within the Alaska Railroad right-of-way along North Valley Way to the Matanuska River.	This pathway follows an old railway and is very popular but is often muddy and difficult to use.	City of Palmer/ ARRC	Mid-term

Recommendation	Description	Rationale	Authority	Timeframe
24. Colony Way to Amoosement Park Pathway	Create a sidewalk along W Fireweed Ave, Cobb St, and W Fern Ave to connect S Colony Way to the Amoosement Park	Proposed by public comment and approved by Steering Committee	City of Palmer	Long-term
25. Arctic Avenue to Auklet pathway connection	Create a Path connection from Arctic Avenue to Auklet along the Glenn Highway Construct a new sidewalk on the south side of Auklet Avenue that crosses the railroad corridor and Valley Way, and continues to Gulkana Street	Proposed by public comment and approved by Steering Committee	City of Palmer/ARRC	Mid-term
26. Glenn Highway Pathway South of Palmer	Add a separated path along the Glenn Highway from South Inner Springer Loop to the Matanuska Lake State Rec Area	Project is already funded and designed	DOT	Near-term
27. Glenn Highway path	Provide a separated path along Glenn Highway from the northwest corner of Bogard & Glenn Highway to Palmer-Fishhook Road.	Extending the separated path north from Scott Road to Palmer-Fishhook Road would connect neighborhoods to schools and the commercial center of Palmer. This would complete a loop if a Palmer-Fishhook Road path is constructed. This need is also identified in the Palmer Comprehensive Plan	DOT	Mid-term
28. Felton Street pathway	Develop a pathway along Felton Street north of the Palmer-Wasilla Highway to Bogard Road.	This project is nearly completed.	City of Palmer	Near-term
29. Maple Springs Pathway	Create a sidewalk on Maple Springs Way	Proposed by public comment and approved by Steering Committee	MSB	Mid-term
30. North Hemmer Road pathway	Develop a pathway along Hemmer Road north of the Palmer-Wasilla Highway to Bogard Road.	This project is already in the design phase as part of the extension/repaving of North Hemmer Road.	MSB/DOT	Near-term
31. Palmer-Fishhook Road Separated Path	Provide a separated pathway from the Glenn Highway to Little Susitna River Bridge linking to the heavily used Trunk Road pathway.	This fast-growing area of the borough needs a connection between the Trunk Road pathway, the Glenn Highway, and the Little Susitna Bridge. This is a project in the 2021 Transportation System Package. The Fishhook Comprehensive Plan also identifies this need.	DOT/MSB	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
32. South Hemmer Road pathway	Develop a pathway along Hemmer Road south of the Palmer-Wasilla Highway to the Valley Pathways School.	This project is already in the design phase as part of the extension of South Hemmer Road.	MSB	Near-term
33. South Hemmer Road extension	Construct a parking lot on the borough-owned parcel west of the Valley Pathways School and extend South Hemmer Road to provide access to the Crevasse Moraine trail system.	Additional trail access and parking in a non-residential area will alleviate parking challenges at the Loma Prieta Drive parking lot.	MSB	Near-term
34. Snowgoose pathway extension South	Create a pathway from E Bogard Rd to N Palmer-Fishhook Rd following N Arabian Ln, E Scott Rd, and N Snowgoose Rd	Proposed by public comment and approved by Steering Committee	MSB	Long-term
35. 49th State Street separated path	Construct a separated pathway along 49th State Street between Bogard Road and Palmer-Wasilla Highway to connect with separated paths on both of those roadways	No dedicated bicycle/pedestrian facility between two major corridors.	DOT	Near-term
36. Future Hospital Access Road Pathway	Construct a path along future road that will provide alternative access to Matsu Regional Hospital, near the Glenn Highway Park Highway Interchange	Proposed by public comment and supported by the OSHP	DOT/MSB	Mid-term
37. Colony Middle School	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2014 Walkzone Inventory and Recommendations SRTS Plan	MSB	Near-term
38. Pioneer Peak Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term
39. Stringfield Road pathway	Convert Old Trunk Road/Stringfield Road to a pathway along Wasilla Creek from Katherine Road to Bogard Road	This connection provides a north-south pathway between residential areas and existing pathways.	MSB	Mid-term
40. Machetanz Elementary	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2017 Walk zone Inventory and Recommendations SRTS Addendum, and DOT SRTS Audit	MSB	Near-term
41. Engstrom Road Pathway	Widen shoulders on Engstrom from Bogard to Hart Lake Loop	Proposed by public comment and approved by Steering Committee	MSB	Long-term

Recommendation	Description	Rationale	Authority	Timeframe
42. Finger Lake Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term
43. Tex-Al Drive path	Add a separated pathway the length of Tex-Al Drive, including the new connecting segment proposed in the 2021 Transportation System Package	The connection between the east and west segments of Tex-Al Drive would provide a major east-west corridor in this area of the borough. This is project is part of the 2021 Transportation System Package	MSB	Near-term
44. Edgerton Parks Road and Mountain Trails Drive path	Extend a separated pathway from Palmer-Fishhook Road to the Government Peak Recreation Area trailhead along Edgerton Parks Road and Mountain Trails Drive	A pedestrian/bicycle facility would connect the Palmer-Fishhook Road to the Government Peak Recreational Area. This is project is part of the 2021 Transportation System Package	MSB	Near-term
45. Seward Meridian Parkway crossing treatment	Provide a pedestrian crossing at the intersection with East Blue Lupine Drive to connect the separated path on Blue Lupine to the Seward Meridian pathway.	Seward Meridian is a high-volume roadway with no traffic control at the East Blue Lupine intersection. Pedestrians and cyclists using the separated path on Blue Lupine must cross 6 lanes of traffic to reach the separated path on Seward Meridian.	MSB/DOT	Mid-term
46. Seward Meridian Road path	Provide a separated path along Seward Meridian between Palmer-Wasilla Highway and Seldon Road.	The already funded and designed Seward Meridian Parkway Road Improvement Project will upgrade Seward Meridian to a four-lane roadway, extend the Seward Meridian Parkway one mile from Bogard Road to Seldon Road, and construct a multi-use separated pathway	MSB/DOT	Near-term
47. Crosswalk for access to Fronteras Spanish Immersion Charter School	Provide bike and pedestrian access to the Fronteras Spanish Immersion Charter School on the east side of Seward Meridian as part of the planned Seward Meridian upgrade	Proposed by public comment and approved by Steering Committee	DOT	Mid-term
48. Teeland Middle School	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan and DOT SRTS Audit	MSB	Near-term
49. Bogard Road separated path	Provide a dedicated pathway on this busy road between Trunk Road and Lucille	2017 MSB Long-range Transportation Plan	DOT	Near-term
50. Shaw Elementary	See audit report	DOT SRTS Audit	DOT/MSB	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
51. Wasilla-Fishhook Road	Provide a separated pathway along Wasilla-Fishhook Road from Seldon Road to Palmer-Fishhook Road	There is no constructed bicycle/pedestrian facility north of Seldon Road. A dedicated non-motorized facility extending to Palmer-Fishhook Road would create a loop that connects dozens of subdivisions throughout this fast-growing area. Additionally, this connection was identified in the Fishhook Comprehensive Plan.	DOT	Mid-term
52. Hermon Road Upgrade	Pathway along Herman Road	Planned and in design with DOT - includes pedestrian/bicycle amenities	City of Wasilla/ DOT	Near-Term
53. Bogard Road separated path	Provide a separated pathway on this busy road between the Bogard Road roundabout to Peck Street.	This is a high-traffic road and supports several densely populated areas. No pedestrian connections exist between the residential areas to the Wasilla High/Middle Schools.	DOT&PF	Near-term
54. Larson Elementary	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2014 Walk zone Inventory and Recommendations SRTS Plan, and DOT SRTS Audit	DOT/MSB	Near-term
55. Crossing improvements at Parks Highway and Palmer-Wasilla Highway	Improve the pedestrian crossing facilities at this intersection.	This intersection is one of the busiest in the MSB. Currently, pedestrians must cross seven lanes of traffic in either direction to move between the busy retail/commercial areas. Additional study is needed, but potential solutions may include pedestrian refuge islands or leading signal intervals.	DOT	Near-term
56. Fairview Loop path	Extend the separated pathway along Fairview Loop from S Knik-Goose Bay Road to South Abby Boulevard.	This fast-growing region of the borough would benefit from a non-motorized facility that connects the separated path at Top of the World Circle to South Abby Boulevard. The facility would service residential areas, an elementary school, and a recreational area.	DOT	Mid-term
57. Fern Street path	Provide a connection between KGB Road and Fairview Loop in this fast-growing portion of the borough	This is a project in the 2021 Transportation System Package	MSB	Near-term
58. Wasilla Middle School	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/City of Wasilla	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
59. Iditarod Elementary	See SRTS Plan	2017 Walk zone Inventory and Recommendations SRTS Plan Addendum	DOT/City of Wasilla	Near-term
60. Crossing treatment at Wasilla-Fishhook, Kalli Circle, and Carpenter Circle	Provide an enhanced pedestrian crossing treatment at this busy intersection.	Iditarod Elementary school is on the east side of Wasilla-Fishhook Road and there are no pedestrian crossing facilities between the school and neighborhoods and after-school programs on the west side. Wasilla-Fishhook Road traffic does not have to stop at this intersection although there are school zone flashers and reduced speed limits.	City of Wasilla/ DOT	Near-term
61. Tanaina Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	MSB	Near-term
62. Lucille Street north extension	Add a path to along Lucille Road to connect Schrock Road and Seldon Road	Proposed by public comment and approved by Steering Committee	MSB	Long-term
63. Shock Road pathway	Create a path on Shrock Rd from Seldon Rd to Church Rd	Proposed by public comment and approved by Steering Committee	DOT	Long-term
64. Snow Shoe Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term
65. Church Road separated path	Provide a connection north from the Church Road separated path that ends at Spruce Avenue to Schrock Road	This project is part of the 2021-22 MSB Capital Improvement Plan	DOT	Mid-term
66. Schrock Road path	Provide a connection from Church Road to the Little Susitna River to connect the Pittman Road, Church Road, and Parks Highway Master Circle trail system	This project was identified in the 2021-22 MSB Capital Improvement Plan	DOT	Mid-term
67. Settlers Bay area trail connections	Connect Hayfield Road Scout Ridge Loop Trail and Cottonwood Creek Wetlands Trail.	Provide connection from pedestrian pathway to recreational trail.	MSB	Long-term
68. S Knik-Goose Bay Rd Crossing	Create a bike and pedestrian crossing on S Knik-Goose Bay Rd at Pinnacle Peak Dr to connect the south side of the street to the path on the north side	Proposed by public comment and approved by Steering Committee	DOT	Long-term

Recommendation	Description	Rationale	Authority	Timeframe
69. Seldon Road Extension Separated Path	Extend Sheldon Road from Windy Bottom Road to Pittman Road and add a separated pathway along the new roadway.	Project is already funded and designed	DOT	Near-term
70. Pittman Road path	Provide a dedicated space for bikes and pedestrians between the separated path on Parks Highway all the way to Church Road	This busy road does not have any dedicated pedestrian facilities.	DOT	Long-term
71. Knik Elementary	See audit report	DOT SRTS Audit	DOT	Near-term
72. Vine Road separated path	Construct a separated path along the full length of Vine Road from KGB Road to Parks Highway	This project is part of the 2021-22 MSB Capital Improvement Plan	DOT	Mid-term
73. West Karen Street separated path	Provide a separated pathway along this busy corridor to connect the Meadow Lakes Community Center with Pittman Road	The Meadow Lakes area is a fast-growing area of the borough. This project is in the 2021-22 MSB Capital Improvement Plan	MSB	Mid-term
74. Meadow Lakes Elementary	See audit report	DOT SRTS Audit	DOT/MSB	Near-term
75. Meadow Lakes Loop Road path	Connect Parks Highway separated path to the intersection of Pittman Road to accommodate bicycles and pedestrians along this significant corridor in a fast-growing area of the borough	This fast-growing area of the borough does not have a bicycle/pedestrian connection between Parks Highway, Hollywood Road, and KGB Road. This would also connect to the separated path on West Hollywood Road that extends to Goose Bay Elementary School. There were three serious bike/pedestrian crashes on this segment between 2015 and 2019. This project is in the 2021-22 MSB Capital Improvement Plan.	MSB	Mid-term
76. Knik-Goose Bay Road separated path	Construct a separated path along KGB Road from South Settlers Bay Road to Malemute Run.	Extending the separated path from South Settlers Bay Road to Malemute Run would provide a safe facility for pedestrians and cyclists along this high-volume, high-speed road in a fast-growing area of the borough.	DOT	Mid-term
77. Dena'ina Elementary	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2017 Walk zone Inventory and Recommendations SRTS Addendum, and DOT SRTS Audit	MSB	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
78. Reddington Jr/Sr High School	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2014 Walk zone Inventory and Recommendations SRTS Plan, and DOT SRTS Audit	MSB	Near-term
79. Hawk Lane east pathway extension to Parks Hwy	Create a path along Hawk Ln from Kenlar Rd to the Parks Hwy	Proposed by public comment and approved by Steering Committee	MSB	Long-term
80. Houston Middle School	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	MSB	Near-term
81. Beaver Lake Road/Hawk Lane/Kenlar Road Pathway loop	Create a path along Beaver Lake Rd starting at Big Lake Rd, continuing along Hawk Ln and Kenlar Rd and returning to Big Lake Rd	Proposed by public comment and approved by Steering Committee	MSB	Long-term
82. South Big Lake bike trail extension	Extend the South Big Lake bike trail across Fish Creek to the Big Lake South State Recreation Site	Proposed by public comment and approved by Steering Committee	DOT	Long-term
83. Big Lake Elementary	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2014 Walk zone Inventory and Recommendations SRTS Plan, and DOT SRTS Audit	DOT	Near-term
84. Northshore pathway west rehabilitation	Reconstruct the bike path on Northshore Dr to the North Shore Campground	Proposed by public comment and approved by Steering Committee	DOT	Long-term
85. Willow Fishhook Road Pathway	Create bike path along bike path along the paved portion of Willow Fishhook Rd, from the Parks Highway to intersection with Locke Lane	recommended in the Willow Summer Trails Plan. This path would improve bike access to Hatchers Pass.	DOT	Mid-term
86. Nancy Lake Parkway Path	Create an approximately 6.5 mile pathway that would connect Parks Highway Trail to the Nancy Lakes Rec Area/Red Shirt Trailhead.	Recommended in the Willow Summer Trails Plan.	DOT	Mid-term
87. Long Lake Road Path	Create pathway along Long Lake Road	Provides access from Parks Highway/Willow Core Area to residential areas	DOT	Mid-term
88. Willow Elementary	See SRTS Plan	2014 Walk zone Inventory and Recommendations SRTS Plan	DOT/MSB	Near-term

Recommendation	Description	Rationale	Authority	Timeframe
89. Willow Creek Road Path	Create an approximately 4 mile long pathway that connects Parks Highway pathway to campground/ river access at end of Willow Creek Road.	This pathway is recommended in the Willow Summer Trails Plan	DOT	Mid-term
90. Parks Highway separated path	Continue the separated pathway from Willow Creek Road to Talkeetna Spur	There is no dedicated facility north of Willow that connects to the separated path along Talkeetna Spur Road.	DOT	Long-term
91. Comsat Road Path	Add a separated path along Comsat Road	Talkeetna Community Council sent letter to ADOT&PF highlighting the dangerous bike and pedestrian conditions on this road	DOT	Near-term
92. Crossing Near Talkeetna Library	Add a crosswalk or crossing signal between separated path and Talkeetna Library	Proposed by public comment and approved by Steering Committee	DOT	Near-term
93. Wild Woods Park Pathway	Create a sidewalk at Wild Woods Park	Proposed by public comment and approved by Steering Committee	DOT/MSB	Long-term
94. Talkeetna Elementary	See SRTS Plan, MSB Transportation Bond, and DOT audit report	2021 MSB Transportation Bond, and 2014 Walkzone Inventory and Recommendations SRTS Plan, and DOT SRTS Audit	DOT/MSB	Near-term
95. Bear Trail Railroad Crossing	Pedestrian underpass from "Bear Trail" to downtown Talkeetna	Proposed by public comment and approved by ARRC	ARRC/MSB	Mid-term

Programs

Recommendation	Description	Rationale	Authority	Cost
Convene a nonmotorized task force.	Form a task force of mobility advocates that represent walking, biking, transit, the mobility impaired, seniors, and other groups to advise local agencies on mobility issues.	A task force of volunteers will give additional public opportunities for input on the nonmotorized system and bring issues to the forefront.	MSB	N/A
Conduct annual bicycle/ pedestrian counts at key locations across the MSB.	Using a network of volunteers, conduct annual counts at major intersections and along significant transportation corridors.	Baseline data are required to monitor use of the network.	MSB/DOT&PF	N/A

Recommendation	Description	Rationale	Authority	Cost
Conduct a level of service assessment for bicyclists and pedestrians.	Using the Highway Capacity Manual's methods, determine the level of service for major transportation corridors for both pedestrians and cyclists.	A level of service assessment will help identify issues along the bicycle and pedestrian network that can be addressed in future plans.	All Organizations	\$25K
Publish a bicycle and pedestrian map.	Develop and publish a simple, fold-out map that depicts bicycle and pedestrian facilities.	A regularly updated map can promote cycling and walking. Additionally, it can be an educational tool for informing the public on rules of the road.	All Organizations	\$7.5K
Conduct an ADA assessment in core areas.	Perform a reconnaissance study of curb ramps, curb slopes, detectable warnings, clear spaces, and other operable parts to determine compliance with ADA requirements in the core areas of the MSB.	An ADA assessment will help identify issues that need to be addressed.	All Organizations	\$25K
Conduct a user conflict study	Evaluate user conflicts on bike and pedestrian paths and develop a guide for minimizing user conflict on bike and pedestrian paths.	Conflicts between users were mentioned repeatedly throughout this project, but addressing these conflicts is not within the scope of the current plan.	MSB	N/A
Develop a wayfinding plan.	Prepare a comprehensive wayfinding plan for core areas of the MSB that includes look and feel standards, pedestrian- and vehicle-scale signage, standard specifications, and locations for wayfinding elements.	A strong wayfinding plan will help promote walking and cycling for both residents and visitors, as well as help walkers and cyclists find their destinations.	All Organizations	\$75K
Conduct a greenbelt pathway reconnaissance and feasibility study.	Assess the feasibility of creating a greenbelt pathway in the core urban areas of the borough to identify locations, costs, environmental issues, and property ownership.	A greenbelt pathway network would provide opportunities for locals and visitors to travel across the core urban area in a separated, natural setting.	MSB	\$100K
Develop an interpretive bicycle and pedestrian path.	Create a pathway connecting historic transportation routes. Include interpretive and wayfinding signs.	Tribal partners have expressed interest in developing an interpretive trail.	MSB, Knik Tribe, Chickaloon Native Village	N/A

TIP and CIP Nominations

Projects from the BPP can be nominated to the MSB TIP or the CIP to become eligible for funding.

TIP Scoring Criteria

The MSB is currently testing a newly-developed, borough-wide TIP. The TIP will be a tool that is used by the MSB Planning and Public Works Departments for evaluating and prioritizing publicly-nominated and MSB-nominated transportation projects. Once fully developed, MSB residents will be able to submit nominations throughout the year that will be evaluated when the MSB is developing lists for transportation packages for Assembly and voter approval. A nomination for the TIP will not be a guarantee for project funding. The TIP will also be used for MSB nominations to the MVP Transportation TIP, and the Statewide TIP. MSB Planning and Public Works will use the TIP as an implementation tool for the BPP and the LRTP. This section will be updated as the Program is developing.

CIP Scoring Criteria

The purpose of the MSB's CIP is to give residents a way to nominate community-supported projects for the MSB Capital Budget. Every year, the MSB CIP Scoring Committee reviews public nominations for capital projects, scores and prioritizes them, and brings them to the Assembly for adoption. Once the CIP is adopted, staff nominate the projects to the capital budget. *A nomination for the CIP is not a guarantee for project funding.* Many BPP projects meet the criteria of a valid CIP project; staff and residents may use the BPP to nominate projects to be considered in the annual budget discussions. This is just one way the BPP can be implemented; Assembly approval is required before any funds could be allocated for a project. All BPP projects will be scored alongside community nominations and other MSB priorities for possible inclusion in the CIP.

To be accepted as a valid nomination, the project must:

- » Support a goal or objective of a state- or Assembly-adopted plan
- » Fall within borough powers (MSB 1.10) to execute
- » Cost more than \$20,000
- » Have a useful life of more than 5 years
- » Not be considered routine maintenance or equipment replacement.

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Appendix A: Economic Impact Report

Economic Impact of Bicycle and Pedestrian Paths

Active transportation, such as bicycle and pedestrian paths, offer communities many positive impacts in areas such as health, connectivity, and economic benefits. This section provides a brief overview of these benefits and the potential impacts of improved and new access to bicycle and pedestrian paths in the Mat-Su Borough.

Health Benefits

Active transportation networks are critical to public health as they facilitate physical activity and positive connections between people of all ages and their community. Additionally, communal infrastructure, such as bike and pedestrian paths, helps to foster a community spirit, cultivate a sense of place and feeling of unity, and creates a network connectivity to everyday destinations. The closeness of ties to neighbors, the strength of our personal relationships and the resources present in our communities are also all related to health and wellbeing.

Active transportation investments are a cost-effective method to reduce the prevalence of highly preventable risk factors (i.e., obesity, diabetes, limited physical activity) and health concerns and help to meet the Healthy Alaskans 2035 goals.¹

The *2016 Matanuska-Susitna Borough Recreational Trails Plan* emphasized Borough residents' desire for healthy recreation and its expected benefits of increased alertness, decreased levels of heart disease, and other ailments.²

Additionally, one of the purposes in the *Alaska Statewide Active Transportation Master Plan (ASATP) 2019*, is to promote healthy lifestyles. Health was identified as one of the goal areas stating "Active transportation opportunities are an important factor in maintaining a healthy population. They also support DOT&PF's mission of *keeping Alaska moving through service and infrastructure*, while providing a transportation system that supports Alaska's ability to thrive." The ASATP set a walking and bicycling commute mode goal for the Mat-Su Borough. An

¹ Healthy Alaskans, Alaska's Health Improvement Plan, <https://www.healthyalaskans.org/data/data-sources/> (Accessed April 22, 2022).

² MSB Recreational Trails Plan 2016 Update, https://matsugov.us/docs/general/14086/trailplanupdate2016_final.pdf (Accessed April 22, 2022).

estimated 1.9% of Mat-Su's population commuted by walking; the plan's goal for the Mat-Su Borough is 3.8%. For bicycling, 0.2% of Mat-Su's population commuted by biking; the plan's goal for Mat-Su Borough is 0.4%. The ASATP also estimated \$1.89 million annually in economic benefits of complete walking networks in the Mat-Su Borough, including \$164,000 in health benefits, \$1.7 million in transportation benefits, and \$43,000 in environmental benefits. For complete biking networks in the Mat-Su, the economic benefits were an estimated \$309,000, including \$21,000 in health benefits, \$280,000 in transportation benefits, and \$8,000 in environmental benefits.³

Opportunities for Improved Health

The connection between physical activity and health is well-established. According to the Centers for Disease Control and Prevention (CDC), studies show that physical activity reduces the risk of major health concerns, including strokes, type 2 diabetes, and some forms of cancer.⁴ Physical activity is also known to promote positive mental health and can offer meaningful social connections. Studies have also shown the health benefits from active transportation result in savings on health care costs, showing that trails can serve as a cost-effective health intervention tool.⁵ These cost savings accrue to health insurers, providers, and participants.

PHYSICAL ACTIVITY

According to the American Journal of Public Health, people who live near safe, high-quality active transportation infrastructure tend to get more exercise than people who don't.⁶

Measures of physical activity vary slightly by population. The following presents the most recently available estimates of physical activity for adults, seniors, high school students, and middle school students in the Mat-Su Borough.

Adults and Seniors

In 2017, just over half of Mat-Su adults (55%) and seniors (54%) met the national recommendations for weekly physical activity of 2.5 hours of moderate exercise or 1.25 hours of vigorous exercise.

³ Alaska Statewide Active Transportation Master Plan 2019
https://dot.alaska.gov/stwdplng/areaplans/modal_system/docs/AK-Statewide-Active-Transportation-Plan.pdf
(Accessed April 22, 2022).

⁴ <https://www.cdc.gov/physicalactivity/index.html> (Accessed April 18, 2022).

⁵ Wang, G., Macera, C. A., Scudder-Soucie, B., Schmid, T., Pratt, M., Buchner, D. (2005). A cost-benefit analysis of physical activity using bike/pedestrian trails. *Health Promotion Practice* 6, 2, 174-79., <https://pubmed.ncbi.nlm.nih.gov/15855287/> (Accessed July 8, 2022).

⁶ Anna Goodman, Shannon Sahlqvist, David Ogilvie, and on behalf of the iConnect Consortium, 2014: *New Walking and Cycling Routes and Increased Physical Activity: One- and 2-Year Findings From the UK iConnect Study*, *American Journal of Public Health* 104, e38_e46, <https://doi.org/10.2105/AJPH.2014.302059> (Accessed April 18, 2022).

High School Students

In 2019, 12% of traditional high school students and 11% of alternative high school students reported they walked or rode their bike either to school or home from school three or more days in an average week, when the weather allows them to do so.

Middle School Students

In 2019, almost all middle school students (94%) reported being physically active for at least 60 minutes during one or more days of the last seven days.

Table 1. Physical Activity Level – Mat-Su Borough

Population Group and Year	Mat-Su Borough % (95% Confidence Interval)	Alaska % (95% Confidence Interval)
Adults – Engaged in 2.5 hours of moderate exercise or 1.25 hours of vigorous exercise weekly		
18+ Years (2017)	54.9% (48.8-60.9)	57.5% (51.2-63.6)
65+ Years (2017)	54.2% (42.9-65.2)	56.2% (51.1-61.3)
High School Students – Physically active at least 60 minutes per day on all seven days of the week		
Traditional High School (2019)	19.8% (17.0-23.0)	17.9% (15.3-20.9)
Alternative High School (2019)	9.9 (6.1-15.6)	11.7% (9.2-14.8)
Middle School Students – Physically active for at least 60 minutes during one or more days of the last seven days		
Middle School (2019)	93.6% (93.0-94.2)	NA

Sources: Mat-Su Health Foundation, 2019. *Hearing Every Voice: 2019 Mat-Su Community Health Needs Assessment*; Alaska Department of Health and Social Services, 2019. *Alaska Youth Risk Behavior Surveillance System*.
Note: Data are compiled from multiple sources and surveys. Indicators are defined in table subheadings.

WALK OR BIKE TO SCHOOL

In 2019, 12% of traditional high school students and 11% of alternative high school students reported they walked or rode their bike either to school or home from school three or more days in an average week, when the weather allows them to do so. Mat-Su Borough high school students attending both traditional and alternative schools report walking or biking to or from school at a lower rate than their peers statewide.

Table 2. Walked or Biked to or From School – High School Students, 2019

	Mat-Su Borough % (95% Confidence Interval)	Alaska % (95% Confidence Interval)
Traditional High School	12.0% (10.2-14.1)	20.9% (17.9-24.2)

Alternative High School	11.1% (7.0-17.3)	23.7% (20.1-27.7)
-------------------------	---------------------	----------------------

Source: Alaska Department of Health and Social Services, 2019. *Alaska Youth Risk Behavior Surveillance System*.

WEIGHT STATUS

Overweight and obese people are at increased risk for certain chronic diseases and health conditions.⁷ In 2018, an estimated 73% of adults and 68% of seniors in the Mat-Su were overweight or obese. Self-reported data reveals that 28% of traditional high school students and 34% of alternative high school students were overweight or obese in 2019. The CDC recommends community efforts to address overweight and obesity should focus on supporting accessible, and affordable active living opportunities.⁸

Table 3. Overweight and Obesity

Overweight and Obesity	Mat-Su Borough % (95% Confidence Interval)	Alaska % (95% Confidence Interval)
Adults		
18+ Years (2018)	73.2% (68.1-77.7)	65.9% (63.7-68.0)
65+ Years (2018)	67.6% (58.3-75.7)	N/A
High School Students		
Traditional High School (2019)	28.3% (25.8-31.0)	29.8% (26.5-33.3)
Alternative High School (2019)	33.8% (26.5-41.9)	35.8% (31.6-40.3)

Sources: Mat-Su Health Foundation, 2019. *Hearing Every Voice: 2019 Mat-Su Community Health Needs Assessment*; Alaska Department of Health and Social Services, 2019. *Alaska Youth Risk Behavior Surveillance System*.

CHRONIC DISEASE

Chronic diseases, such as diabetes, hypertension, and cancer, are associated with substantial health and economic costs.⁹ The table below highlights the rates of incidence for a number of chronic diseases for residents in the Mat-Su Borough and Alaska.

Table 4. Chronic Disease

Chronic Disease	Mat-Su Borough	Alaska
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⁷ Centers for Disease Control and Prevention, 2022. Health Effects of Overweight and Obesity. <https://www.cdc.gov/healthyweight/effects/index.html> (Accessed April 19, 2022).

⁸ Center for Disease Control and Prevention, 2022. Strategies to Prevent and Manage Obesity. <https://www.cdc.gov/obesity/strategies/index.html> (Accessed April 19, 2022).

⁹ Centers for Disease Control and Prevention, 2022. Health and Economic Costs of Chronic Diseases. <https://www.cdc.gov/chronicdisease/about/costs/index.htm> (Accessed April 19, 2022).

Heart Disease and Stroke		
Coronary heart disease death rate per 100,000 people	112.0 (2017)	133.4 (2017)
Stroke disease death rate per 100,000	25.6 (2017)*	34.8 (2017)
Diabetes		
Diabetes – Adults (%)	7.9 (2018)	8.4 (2018)
Diabetes – Adults 65+ Years (%)	19.5 (2018)	17.6 (2018)
Cancer		
Cancer death rate per 100,000 people	173.5 (2016)	157.8 (2016)
Colorectal cancer death rate per 100,000	15.4 (2016)	14.7 (2016)
Lung cancer death rate per 100,000 people	69.0 (2016)	38.9 (2016)

Source: Mat-Su Health Foundation, 2019. *Hearing Every Voice: 2019 Mat-Su Community Health Needs Assessment*.
*Data may be statistically unreliable and should be interpreted with caution.

Economic Benefits

Increased Property Values

Greenways and multi-use trails are seen by property owners and real estate professionals to create an amenity that commands higher prices from surrounding homes. When trails increase property value, local governments receive more property tax revenue.

According to research, homes in rural communities experienced between a 3% to 5% increase in value within half a mile of a trail.¹⁰ Individual homes within half a mile of trails sometimes experienced differences in appreciation, with the highest appreciation related to views of the green space or proximity to trail access points.¹¹ The largest value increases were associated with well-known and high-profile trails.^{12,13} Using this research as a basis, a house valued at \$285,000 in the Mat-Su Borough could have a realized value increase between \$8,550 to \$14,250 if located near a trail.¹⁴

¹⁰ Crompton, John L. 2020. The Impact of Trails and Greenways on Property Values. Parks and Recreation Magazine, <https://www.nrpa.org/parks-recreation-magazine/2020/may/the-impact-of-trails-and-greenways-on-property-values/> (Accessed April 18, 2022).

¹¹ Crompton, J., and S. Nicholls. 2006. "An Assessment of Tax Revenues Generated by Homes Proximate to a Greenway." *Journal of Park and Recreation Administration* 24(3): 103-108.

¹² Resource Dimensions. 2005. Economic Impacts of MVSTA Trails and Land Resources in the Methow Valley. Methow Valley Sport Trails Association.

¹³ Lindsey, G., Man, J., Payton, S., and K. Dickson. 2004. "Property values, recreation values, and urban greenways." *Journal of Park and Recreation Administration*, 22 (3): 69-90.

¹⁴ U. S. Census Bureau. *American Community Survey*. B25075; Value, Owner Occupied Housing Units. 2020.

Enhanced Visitor Experience

A 2020 study conducted for the Mat-Su Convention and Visitors Bureau found that hiking was the tenth-most desired experience among prospective visitors to the region, at 36%.¹⁵ The top desired Mat-Su activity was experiencing the outdoors (46.2%).

In the summer of 2016 (most current survey data available), the number two activity among out-of-state visitors to the Mat-Su was hiking/nature walk, with 17% of visitors participating; 1% of visitors participated in biking. Hiking/nature walk was much more popular among visitors who had traveled to/from Alaska by air (23%) when compared to those who traveled by cruise ship (8%) or highway/ferry (4%).¹⁶ Visitors to Mat-Su were slightly more likely than visitors to Anchorage to report hiking/nature walk participation at 17% versus 12%.¹⁷

Table 5. Hiking and Biking Activities in Mat-Su, Summer 2016 (%)

	ALL VISITORS	TRANSPORTATION MODE			DESTINATION	
		Air	Cruise	Highway/ Ferry	Talkeetna	Palmer/ Wasilla
Hiking/nature walk	17%	23%	8%	4%	17%	19%
Biking	1%	1%	-	1%	1%	1

Source: *Mat-Su Visitor Profile, Summer 2016*, conducted by McDowell Group for Mat-Su Convention and Visitors Bureau.

EXTENDING VISITOR STAY AND SPENDING

Trails can generate business impacts and create new jobs by attracting visitors, especially overnight visitors. Destination trails attract visitors from outside the local area who travel specifically for recreational opportunities. The benefits from destination trails include economic development, business revenue, employment, and employee earnings. In addition to its direct effect on businesses, visitor spending also has a ripple effect in the community as employees and business owners spend their earnings, and local and state governments receive more tax revenue. The economic impact of trails is highest when a trail is connected to local businesses that cater to trail user needs such as restaurants, grocery stores, camping, hotels, guiding services, and gear stores. This connection can occur directly through trail spurs that link to commercial centers, as well as through signs at trailheads or shuttles between a town and the trailhead. Because lodging often accounts for the biggest proportion of trip expenses, a trail's economic impact is greatly increased when it attracts more overnight users.¹⁸

¹⁵ 2020 Mat-Su Valley Visitor Research, conducted by Destination Analysts for Mat-Su Convention and Visitors Bureau.

¹⁶ *Mat-Su Visitor Profile, Summer 2016*, conducted by McDowell Group for Mat-Su Convention and Visitors Bureau.

¹⁷ *Alaska Visitors Statistics Program 7*, prepared by McDowell Group for Alaska Travel Industry Association.

¹⁸ <https://headwaterseconomics.org/wp-content/uploads/trails-library-business-impacts-overview.pdf> (Accessed April 13, 2022).

According to recent research that measured the economic impacts of trails:

- Overnight stays are the biggest contributor to total spending. At a mountain bike race in North Carolina, each additional night adds \$101 to a visitor's total spending.¹⁹ Along the Great Allegheny Passage, overnight users spend seven times more than day users.²⁰ Bikers along the Columbia Gorge had day spending averaging \$43 per party and \$600 for overnight costs.²¹
- The quality of trails and amenities that support trail users have the largest effect on the total number of visitors.^{22,23}
- Although biking/walking events are short-lived, participants often return to the community after the event.²⁴
- After visiting an area, some tourists become residents or second homeowners, bringing their businesses, supporting the local economy, and paying taxes.^{25,26}

BUSINESS DEVELOPMENT

Businesses that benefit from trail recreation and trail development include outdoor sporting goods stores (i.e., bicycle shops, outdoor clothing stores, and footwear shops), guiding services (hiking and bicycling tour guides), transportation services (i.e., backpacking shuttle services), accommodations, and eating establishments.²⁷

¹⁹ Schiller, A., and J. Whitehead. 2013. Economic Impact of the 2012 '6 Hours of Warrior Creek' Mountain Bike Race. Boone, NC: Center for Economic Research and Policy Analysis at Appalachian State University.

²⁰ Campos, Inc. 2009. The Great Allegheny Passage (GAP) Economic Impact Study (2007-08). The Progress Fund.

²¹ Dean Runyan Associates. 2014. Columbia River Gorge Bicycle Recreation: Economic Impact Forecast for the Communities Along the Historic Columbia River Highway. Prepared for the Friends of the Historic Columbia River Highway, Oregon Tourism Commission, Port of Cascade Locks, Port of Hood River, Port of The Dalles.

²² Berard, D., S. Chapin, A. Hoogasian, T. Kane, D. Marcouiller, and T. Wojciechowski. 2014. The Economic Impacts of Active Silent Sports Enthusiasts. Madison, WI: University of Wisconsin Department of Urban and Regional Planning, Extension Report 14.1.

²³ Tourism British Columbia. 2013. Rossland Mountain Bike Visitor Study 2011 Results. Research, Planning & Evaluation, Tourism British Columbia Ministry of Jobs, Tourism, and Skills Training.

²⁴ Western Canada Mountain Bike Tourism Association. 2007. Sea to Sky Mountain Biking Economic Impact Study.

²⁵ Meltzer, N. 2014. "Adapting To the New Economy: The Impacts of Mountain Bike Tourism in Oakridge, Oregon" [Master's Thesis]. Eugene, OR: University of Oregon Department of Planning, Public Policy and Management.

²⁶ Resource Dimensions. 2005. Economic Impacts of MVSTA Trails and Land Resources in the Methow Valley. Methow Valley Sport Trails Association.

²⁷ https://matsugov.us/docs/general/14086/trailplanupdate2016_final.pdf (Accessed April 22, 2022).

Appendix B: Full Stakeholder Contact List

MSB Departments:

- » MSB Land Management
- » MSB Public Works
- » MSB Planning

Advisory Boards and Committees:

- » Safe Routes to School Committee
- » Health and Human Services Board
- » Transportation Advisory Board
- » Parks, Recreation & Trails Advisory Board
- » Mat-Su Coalition on Housing and Homelessness
- » Alaska Climate Action Network

State of Alaska:

- » Alaska Department of Transportation & Public Facilities
- » Alaska Veterans & Pioneer Home

Non Profit/Community Organizations and Partners:

- » Mat-Su Health Foundation
- » Mat-Su Trails and Parks Foundation
- » Valley Mountain Bikers and Hikers
- » American Lung Association
- » American Association of Retired Persons
- » My House
- » Identity Inc
- » United Way
- » Mat-Su Ski Club
- » Valley Interfaith Action Group
- » Palmer Chamber of Commerce
- » Greater Wasilla Chamber of Commerce
- » Mat-Su Valley Board of Realtors
- » Mat-Su Convention and Visitor Bureau

Tribal Councils:

- » Chickaloon Native Village
- » Knik Tribe

Transit:

- » Valley Transit
- » Sunshine Transit
- » Chickaloon Area Transit Systems

Cities:

- » City of Palmer
- » City of Wasilla
- » City of Houston

Community Councils:

- » Big Lake Community Council
- » Buffalo Mine/Soapstone Community Council
- » Butte Community Council
- » Chase Community Council
- » Chickaloon Community Council
- » Fishhook Community Council
- » Gateway Community Council
- » Glacier View Community Council
- » Greater Farm Loop Community Council
- » Knik-Fairview Community Council
- » Lazy Mountain Community Council
- » Louise, Susitna, Tyone Community Association
- » Meadow Lakes Community Council
- » North Lakes Community Council
- » Petersville Community Council
- » Point MacKenzie Community Council
- » Swentna Community Council
- » South Knik River Community Council
- » South Lakes Community Council
- » Susitna Community Council
- » Sutton Community Council
- » Talkeetna Community Council
- » Tanaina Community Council
- » Trapper Creek Community Council
- » Willow Area Community Organization

Local Businesses:

- » All About Herbs
- » Backcountry Bike and Ski
- » AKtive Body
- » Alaska Bicycle Center

Appendix C: Existing Conditions & Issues Report

MAT-SU BOROUGH BICYCLE AND PEDESTRIAN PLAN

DRAFT EXISTING CONDITIONS & ISSUES

PREPARED FOR

Mat-Su Borough
350 E. Dahlia Ave.
Palmer, AK 99645

SEPTEMBER 2022



MAT-SU BOROUGH BICYCLE AND PEDESTRIAN PLAN

DRAFT EXISTING CONDITIONS & ISSUES

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Mat-Su Borough
350 E. Dahlia Ave.
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SEPTEMBER 2022

Project Number I0946.22001



EXECUTIVE SUMMARY

This report documents the state of the bicycle and pedestrian network across the Mat-Su Borough (MSB) by identifying issues, evaluating connectivity, and analyzing pedestrian and bicycle crash data. The inventory is based on data collected from the MSB, State of Alaska, and Mat-Su Trails and Parks Foundation. Data are supplemented with interviews with stakeholders and user groups to determine where the issues are and what are the greatest needs. The information in this document will be used to help identify potential recommendations and strategies for improving the bicycle and pedestrian network across the Borough.

The MSB covers a large geographic area. Lower population densities and dispersed population centers make it challenging to provide bicycle and pedestrian connectivity across the entire borough. Likewise, different jurisdictions (borough, state, cities) have different standards and funds for constructing and maintaining non-motorized facilities. These challenges will require coordination and thoughtful planning to overcome.

However, there is a robust bicycle and pedestrian network and considerable enthusiasm for cycling, running, hiking, and walking in the borough. Advocates for non-motorized recreation and commuting can help communities identify new connections and facilities, as well as connect resources between partner organizations.

There are many opportunities to connect neighborhoods, parks, trails, and businesses with bicycle and pedestrian infrastructure such as sidewalks, separated paths, and wide shoulders. In addition to providing more opportunities for walking and biking, these connections may improve property values, promote healthy lifestyles, provide mobility options for residents without vehicles, and open opportunities for tourism. See the attached memo *Economic Impact of Bicycle and Pedestrian Paths* for more information.

Continued population growth of the region means the time to plan for the bicycle and pedestrian network is now. Conflicts between motor vehicles and bikers/walkers may increase if adequate facilities are not present. An equitable and safe network of bicycle and pedestrian infrastructure will ensure residents and visitors of all abilities and ages benefit from an improved quality of life through healthier, better-connected communities.

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1.0 STUDY AREA

The Matanuska-Susitna Borough geographical area includes approximately 25,000 square miles of land and approximately 600 square miles of water (ADOLWD, 2020). The Matanuska-Susitna Valley (Mat-Su) is traditionally land of the Dena'ina and Ahtna Athabaskans and encompasses rivers, wetlands, hills, mountains, lakes, farmland, and forest. The MSB is in a transitional climate zone, which is characterized by long, cold winters and mild summers.

There are 25 communities within the MSB, including three cities – Houston (second class city), Palmer (home rule city), and Wasilla (first class city). The remaining 22 communities are classified as Census Designated Places. The borough is a second-class borough with a 7-member Assembly and Mayor.

1.1 POPULATION

According to the Alaska Department of Labor and Workforce Development, the 2020 MSB population is 107,081. The region continues to be the fastest growing part of the state due to its proximity to Anchorage, availability of land for development, and lower housing costs than Anchorage. Easy access to outdoor recreation is also a draw for new residents.

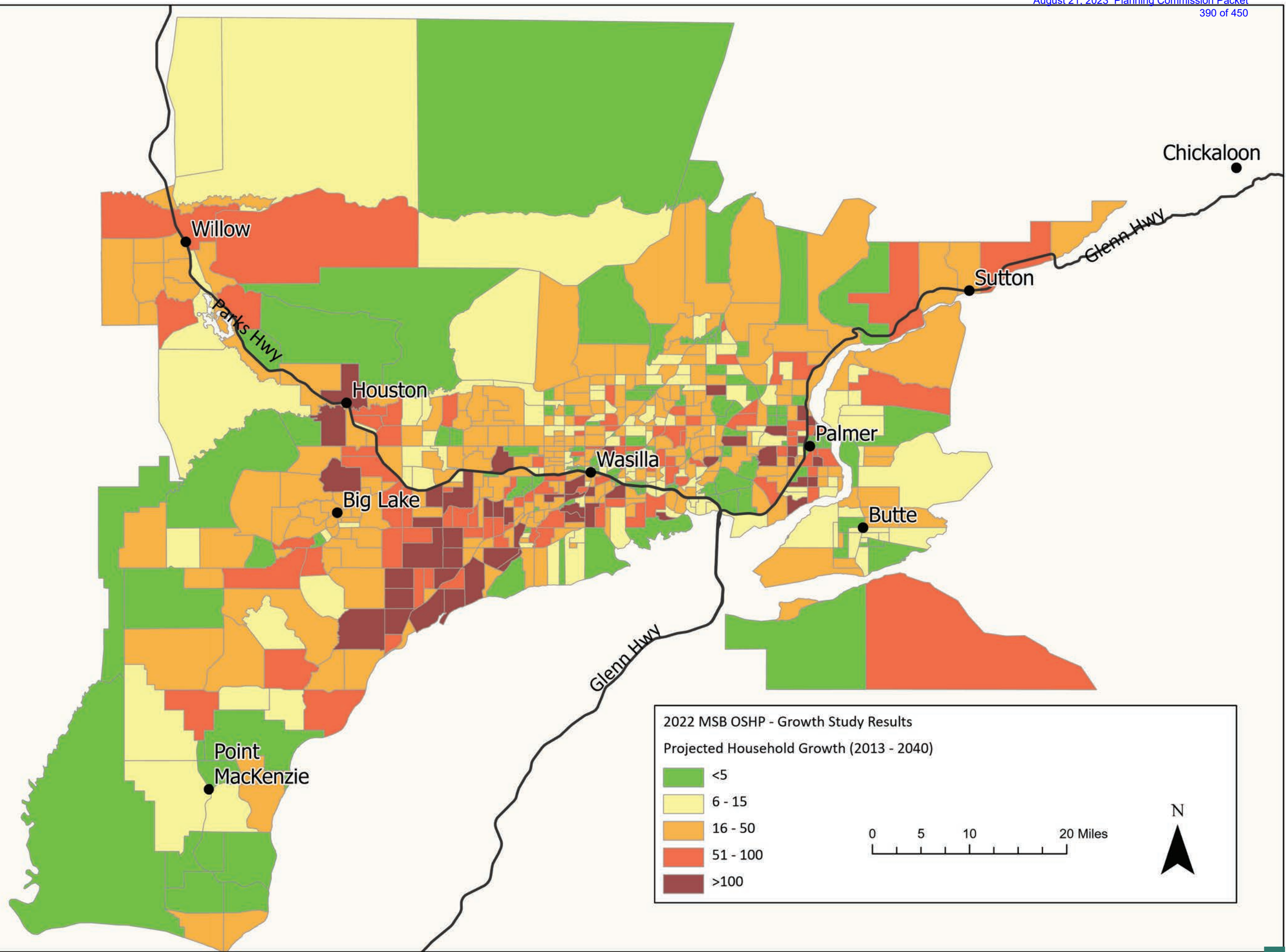
1.1.1 GROWTH

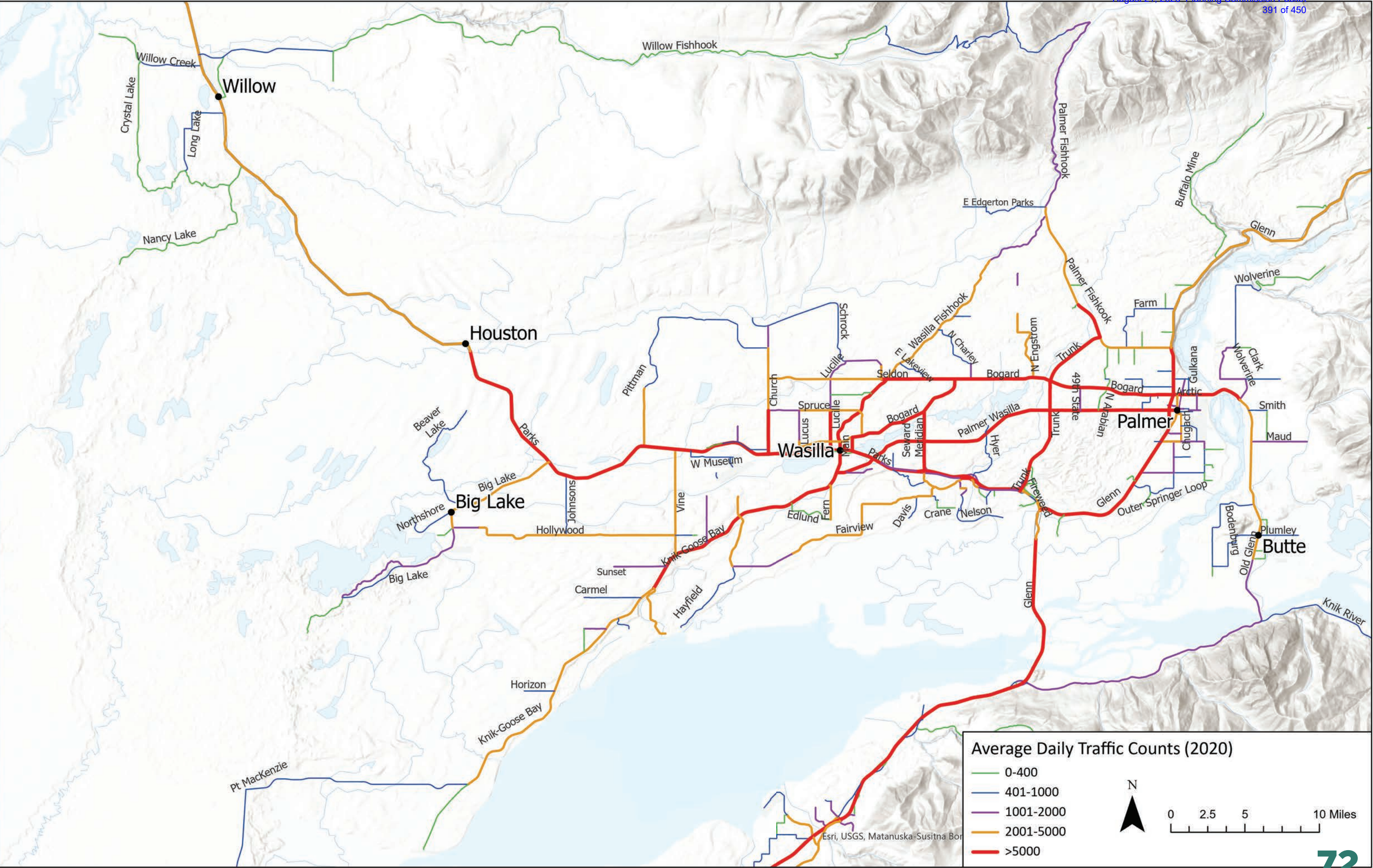
Figure 1 shows projected growth in the borough. The areas with the highest growth potential are southwest of Wasilla along Knik-Goose Bay (KGB) Road to Knik, and south of Meadow Lakes. Additional pockets of high growth projections include Houston, areas around Palmer, and the Willow area.

1.1.2 TRAFFIC VOLUMES

Traffic volumes play a significant role in determining the type of bicycle or pedestrian facility to include along a roadway. Higher volume roads benefit from separated facilities while low-volume roads may sufficiently accommodate bikes and pedestrians with a wide shoulder.

In the MSB, arterial roads carry more than 5,000 vehicles per day on average (Figure 2). These routes include the Parks and Glenn Highways, KGB Road, Seldon Road, Bogard Road, the Palmer-Wasilla Highway, Trunk Road, Seward-Meridian Parkway, and portions of Lucille, Arctic, and Wasilla-Fishhook Road. As the borough's population grows, traffic will continue to increase.









2.0 BICYCLE AND PEDESTRIAN FACILITIES

The bicycle and pedestrian network consists of separated paths, paved shoulders, sidewalks, bike lanes, and shared roadways. Descriptions of each of these facility types are in Table 1.

Table 1 - Bicycle and pedestrian facility types found in the MSB

Infrastructure type	Intended use	Description
<p>Separated Path</p> 	Mixed	The separated path network in the MSB connects Houston, Big Lake, Wasilla, Palmer, and Butte. Paths are generally paved with asphalt and run parallel to high-volume roadways.
<p>Paved Shoulder</p> 	Mixed	<p>Paved shoulders can function the same as bike lanes. A major factor in the safety of shoulders for bicyclists is the presence and design of rumble strips that alert drivers when they leave the travel lane.</p> <p>In rural areas where sidewalks are not feasible, a paved shoulder can also accommodate pedestrians.</p>
<p>Sidewalk</p> 	Pedestrian	Sidewalks are commonly used for pedestrian travel in urban areas. The MSB subdivision code does not have a sidewalk requirement, making sidewalks sporadic. Sidewalks are typically found in the original Palmer townsite area and the historic, commercial part of downtown Wasilla (MSB Long Range Transportation Plan).
<p>Bike lane</p> 	Bicycle	<p>Bike lanes are a designated, exclusive space for bicyclists to operate one-way on the roadway using pavement markings and signs.</p> <p>In the MSB, there are several miles of bike lane located in Palmer.</p>

Infrastructure type	Intended use	Description
<p>Shared Roadway</p> 	Bicycle	<p>In shared lanes, bicyclists ride in mixed traffic, therefore their comfort and safety varies widely based on traffic operating speeds and volumes.</p> <p>Roads with low traffic volumes can provide bike friendly routes in the MSB.</p>

2.1 PEDESTRIAN

Walkers use a variety of facilities to get between destinations in the borough. In urban areas, sidewalks provide walkways between homes, businesses, and schools. Several major roadways have separated paths that run greater distances than the sidewalk network. Pedestrians also use the paved shoulder of many roadways when no other facility exists.

2.2 BICYCLE

Cyclists have several options for riding around the borough. Using roadways shared with motor vehicles is the most common facility. Likewise, paved shoulders are found on most paved roads and provide some level of separation from motor vehicle traffic, although that is dependent on the width of the shoulder. There are a few designated bike lanes in the borough, all within the City of Palmer. Multi-use separated paths are common on busier roads. Riding bicycles on sidewalks is generally permitted in Alaska except in business districts or where a regulatory traffic control device prohibits it. Sidewalk riding is discouraged by most best practices due to potential conflicts with pedestrians.

2.3 CONNECTIONS TO TRANSIT

Before hopping on a bus, the rider must walk or bike to the bus stop. Public transit services allow residents to make longer trips than would be possible solely on foot or bicycle.

The MSB has limited scheduled, route-based transit services. Additionally, local transit is provided on-demand. Therefore, there are very few designated bus stops or shelters across the borough. Without widespread, scheduled transit service, the connectivity of the bicycle and pedestrian network is more important for ensuring that residents without a motor vehicle can get safely to their destination. If route-based transit is developed in the future, it will be important to ensure that there are connections to non-motorized facilities.

3.0 RELATED PLANS, PROJECTS, AND POLICIES

3.1 PLANS

3.1.1 2019 ALASKA STATEWIDE ACTIVE TRANSPORTATION PLAN

The Alaska Statewide Active Transportation Plan (ASATP) seeks to improve safety, increase accountability, and promote healthy lifestyles in Alaskan communities and to develop a safer and more efficient active transportation network and infrastructure to encourage walking and bicycling. The Plan sets a framework for how to plan for and measure progress toward a vision for increased and safer active transportation opportunities across the state. The performance measures identified in the Plan may be useful for formulating local performance measures in the BPP.

3.1.2 2016 MSB TRAILS PLAN

The MSB Trails Plan was initially adopted in 2000 and updated several times over the years. The most recent update was completed in 2016 and then adopted in June 2017. This plan focuses on the backcountry and underfunded trails through the MSB, unlike the separated trails that run parallel to the road system. The BPP will evaluate links between the recreational trail network and the paved non-motorized system to ensure connections between the two.

3.1.3 MSB OFFICIAL STREETS AND HIGHWAYS MAP (IN PROGRESS)

The purpose of the Official Streets and Highways Plan is to have a logical road network with relevant classifications based on use and volume that also addresses safety concerns. The map depicts corridors for future road projects based on development and build-out estimates that gauge where the MSB population is going to work and live in the next two decades. The estimated completion date for the plan is the summer of 2022. The BPP will utilize the OSHP map to complement the forecasted development and road construction in the MSB.

3.1.4 2035 MSB LONG RANGE TRANSPORTATION PLAN

The Bike and Pedestrian Plan is seen as a supplemental chapter to the 2035 MSB Long Range Transportation Plan (LRTP) that was adopted in 2017. The LRTP assessed growth within the MSB over the next 20 years and provides overarching transportation goals for the MSB.

As the MSB is the fastest growing region in Alaska and has been for the last 20 years, key elements from the 2035 LRTP can be incorporated into the Bike and Pedestrian Plan. This will ensure that the Bike and Pedestrian Plan will not interfere with the current transportation system goals of the MSB and will complement the LRTP with longevity and resiliency in mind.

Policy strategies and actions suggested in the LRTP that are relevant to Bike and Pedestrian Planning:

- Develop an Active Transportation Master Plan
- Improve connectivity
- Adopt a Policy Requiring Bike/Pedestrian Improvements near/along Transit Corridors
- Improve Awareness of Transportation Choices
- Continue Coordination with MSB School District Regarding Safe Routes to Schools (SRTS)
- Proactively Support Active Transportation Provisions with Highway Facility Improvements
- Prepare a Regional Trail Map Reflecting Trail Systems

The LRTP developed a prioritized, fiscally constrained list of roadway improvements to be completed through 2035, which includes several projects that involve pedestrian or bicycle infrastructure. Several of these projects are already adapted into the Statewide Transportation Improvement Program, while others are suggested at a later timeframe (see Figure 3).

- Big Lake Road Reconstruction with appropriate pedestrian amenities
- Seldon Rd Upgrade and pedestrian facilities
- Seward Meridian Parkway with pedestrian path
- Vine Road Improvements with included pedestrian facilities
- Knik Goose Bay Road improvements with appropriate pedestrian amenities
- Bogard Road Improvements with appropriate pedestrian facilities

3.1.5 SAFE ROUTES TO SCHOOL PLAN

The 2014 Safe Routes to School (SRTS) plan assessed the walking/biking infrastructure around 17 schools throughout the MSB, provided general bike and pedestrian recommendations, and school-specific recommendations for improving the walking/biking conditions. The plan's study and recommendations focused on a half-mile radius around schools. Goals in the bike and ped plan will align with the SRTS plan, while covering a much broader geographic extent and demographic of users. The bike and ped plan can potentially build on the SRTS plan with the potential for connections to pedestrian and bike infrastructure near schools outside of the immediate half-mile radius.

3.1.6 2011 MSB ECONOMIC DEVELOPMENT STRATEGIC PLAN

Although developed in 2010 and adopted in 2011, there are still several strategies to be utilized from the MSB Economic Development Strategic Plan. As part of the MSB BPP, an updated economic analysis will be developed. A prioritized list of projects will be incorporated into the BPP meant to increase tourism, recreation, and hospitality spending. This will be done using the 2011 MSB Economic Development Strategic Plan, a current economic analysis, and input from stakeholders. The updated bike and pedestrian network will promote connectivity to areas that will benefit from better infrastructure and facilities.

3.1.7 COMPREHENSIVE PLANS

Borough Comprehensive Development Plan (2005; update in progress)

The Matsu Borough Comprehensive Plan was developed in 2005 with an update currently in progress. It's intended use is to develop general goals and policy recommendations to help guide future development and identify infrastructure needs at the borough-wide level. In addition to the borough-wide plan, community based comprehensive plans provide residents with the opportunity to guide development within their specific community. The BPP will be guided by the borough and community comprehensive plans and can be used to guide future comprehensive plan updates.

The borough-wide comprehensive plan includes a parks and open space goal to acquire, develop, and redevelop a system of parks, recreation facilities, community centers, and open spaces that are accessible to the entire community. The plan promotes the development of pedestrian and bicycle linkages between schools, public facilities, neighborhoods, parks and open spaces, and population centers where feasible.

A borough-wide transportation goal is to develop an integrated surface transportation network that facilitates the efficient movement of people, goods, and services. This constitutes policy recommendations that encourage a multi-modal system, street and trail connectivity, and the delegation to local community plans to address specific community level needs.

Borough Core Area Comprehensive Plan (2007 update)

The core area plan reiterates much of the needed transportation infrastructure identified in the old Long Range Transportation Plan. The plan does identify a goal of providing safe and efficient vehicular and non-motorized travel within the core area and between the core area and other destinations. Additionally, the plan supports a policy of increasing local transit services.

Wasilla (2011)

The City of Wasilla Comprehensive Plan includes a goal to maintain and improve City sidewalks and non-motorized pathways to increase walkability. It identified concerns of limited connectivity, maintenance costs, and multi-modal sharing issues that the city needs to address. The plan specifically looked at the core downtown area and strategies to revitalize it, and improving the pedestrian environment in this area with sidewalks and crosswalks was identified as an important aspect of the communities' future downtown vision.

The plan suggested improving pedestrian access around parks and schools, enhancing connectivity between commercial establishments, improving safety using signs and designated road crossings, and improving walkways using strategies such as a year-round maintenance plan, enhancing ADA accessibility, and encouraging low-impact lighting.

Houston (2016)

An overall transportation objective in the Houston Comprehensive plan is to improve and expand non-motorized transportation facilities where possible. Specific strategies include expanding multi-use pathways and lighting improvements, designing safe crossings of the Parks Highway to connect residential and commercial areas, and to support the development of a Hawk Lane bike path.

Palmer (2006)

The Palmer Comprehensive Plan also recognized the need for improved pedestrian and bike infrastructure. Two priorities identified in the plan are to upgrade and better maintain downtown sidewalks, and to develop a trail along the railroad right-of-way from the State Fairgrounds through the City and north to Sutton. The plan identifies the area bounded by the Glenn Highway to the west, Eagle Avenue to the north, South Airport to the east and E. Commercial Drive to the south – as an area that should have sidewalks extended as the area grows.

The plan recommends policy that all subdivisions make adequate provisions for safe, functional pedestrian circulation. And it identifies the need for better winter maintenance and snow removal, suggesting a partnership with the planned Business Improvement District to achieve this.

Big Lake (2009)

The development of a pedestrian and bicyclist friendly street network is identified as a strategy aligned with the community's transportation goals. Desired land use patterns specify a "town center" use area.

Improving the pedestrian environment of the central town area is important for the town center vision. The plan recommends better access to Jordan Lake Park and to Big Lake itself, including adding a walking trail to the lake. To meet recreational goals, the plan recognizes that conflicts between motorized and non-motorized users on bike paths is a concern.

Chase (2017)

The only formally developed surface access into Chase is via railroad, boat, or fly-in. Current access into and through the Chase area relies on a system of trails. A primary conflict is pedestrian use of railroad right-of-way and even the tracks themselves for transportation, which is an illegal and dangerous practice. The plan also identifies the need to improve the trail system to expand legal access to parcels within the planning area.

Chickaloon (2008)

A transportation recommendation in this plan is to build a separated path on the Glenn Highway, between Fish Land Road (MP 73) and Chickaloon River Road (MP 78). The plan also recommends supporting development of trails that connect open areas and parks to residential and commercial areas in the community.

Lazy Mountain (2008)

This plan advocates for several non-motorized policies and goals, including encouraging street and trail connectivity, improving roadside trails, and constructing roadside trails with future road projects. The plan also identifies the Clark-Wolverine Road corridor as a priority location for a pathway, as well as a connection between the Old Glenn Highway and the George W. Palmer Bridge.

Fishhook Comprehensive Plan (2017)

A strategy to meet recreational goals in this plan is to develop additional pedestrian and bike trails and linkages between parks, open spaces, water bodies, and neighborhoods. Acquiring additional public greenbelts to enhance these links through collaboration with foundations, non-profits, and government sources will help to meet this objective.

The plan advocates for the design and construction of recognized bike lanes and off-road vehicle (ORV) access along the Palmer and Wasilla Fishhook Roads by maintaining a working relationship with the State of Alaska Department of Transportation and Public Facilities (DOT&PF), and state and local elected officials to ensure awareness of the need for these infrastructure upgrades.

Meadow Lakes (2005)

The Meadow Lakes comp plan suggested creating a pedestrian-oriented, mixed-use town center along the south side of the Parks Highway near the Pittman Road intersection. This town center concept includes development standards to promote walking, such as requiring sidewalks, planting vegetation between streets and buildings, screening parking areas, and encouraging denser development.

Willow (2013)

This plan includes a community goal of creating a walkable community and a pedestrian-oriented town center. Goal 3 in the plan is to "Establish, improve, and maintain appropriate roadside trails and pedestrian paths."

3.2 CURRENT AND PLANNED PROJECTS

Across the borough, infrastructure projects are developed by several entities. Figure 3 depicts bicycle and pedestrian infrastructure projects that are either funded or needed based on other planning efforts.

State of Alaska Department of Transportation & Public Facilities – The DOT&PF is responsible for the state highway network, which includes separated pathways, sidewalks, and bike lanes. State transportation infrastructure projects are funded by a combination of federal, state, tribal, and local money.

Mat-Su Borough – The borough develops roads through the subdivision process. Developers construct roads at the time of subdivision before selling any subdivided parcels. Additionally, the borough can design and construct infrastructure through voter-approved bond packages. The Capital Improvement Plan (CIP) can be used to prioritize bike and pedestrian facilities, but it does not have fiscal backing. Projects can be nominated by MSB staff or other stakeholders to be put on the CIP.

City of Wasilla – The City of Wasilla maintains a CIP that identifies new infrastructure such as roads, paths, and sidewalks. The CIP is a 5-year plan for implementation and is funded through tax revenues and grants.

City of Palmer – The City of Palmer maintains a Capital Improvement Plan (CIP) that identifies new infrastructure such as roads, paths, and sidewalks. The CIP is a 5-year plan for implementation and is funded through tax revenues and grants.

City of Houston – The City of Houston maintains a Capital Improvement Plan (CIP) that identifies new infrastructure such as roads, paths, and sidewalks. The CIP is a 5-year plan for implementation and is funded through tax revenues and grants.

The Mat-Su region is also in the process of establishing a **Metropolitan Planning Organization (MPO)**. The newly formed MPO will coordinate transportation projects within the designated urbanized area, including non-motorized infrastructure projects. The MPO is expected to be in place in late 2023, pending designation as an urbanized area per the 2020 US census results.

3.2.1 PLANNED PROJECTS

MSB Long Range Transportation Plan (LRTP) – The LRTP makes specific transportation improvement recommendations that will guide transportation investment through 2035. These projects can be adapted into the Alaska Statewide Transportation Improvement Program or funded with an MSB bond package. Projects listed in the LRTP are not necessarily funded, but they do represent the transportation priorities of the MSB. The LRTP is considered fiscally constrained because each project within the plan will be constructed within the planning horizon.

MSB Capital Improvement Program (CIP): The CIP is a nomination-based program that provides a list of projects with community support for possible funding. Once the Bike and Pedestrian Plan is adopted, many of the projected additions or changes to the network will qualify as valid CIP projects.

3.2.2 FUNDED PROJECTS

Alaska Statewide Transportation Improvement Program (STIP): The STIP covers all surface (non-aviation) transportation improvement projects for which partial or full federal funding is approved and that are expected to take place within a four-year period.

2021 MSB Transportation System Package: A list of transportation projects, potentially funded with general obligation bonds that was approved by voters in 2021. The MSB has not sought bonds for these projects yet.

2018 MSB School District Pedestrian Improvements:

Projects identified through the borough's Safe Routes to School program. This on-going effort is based on the 2014 Safe Routes to Schools assessment.

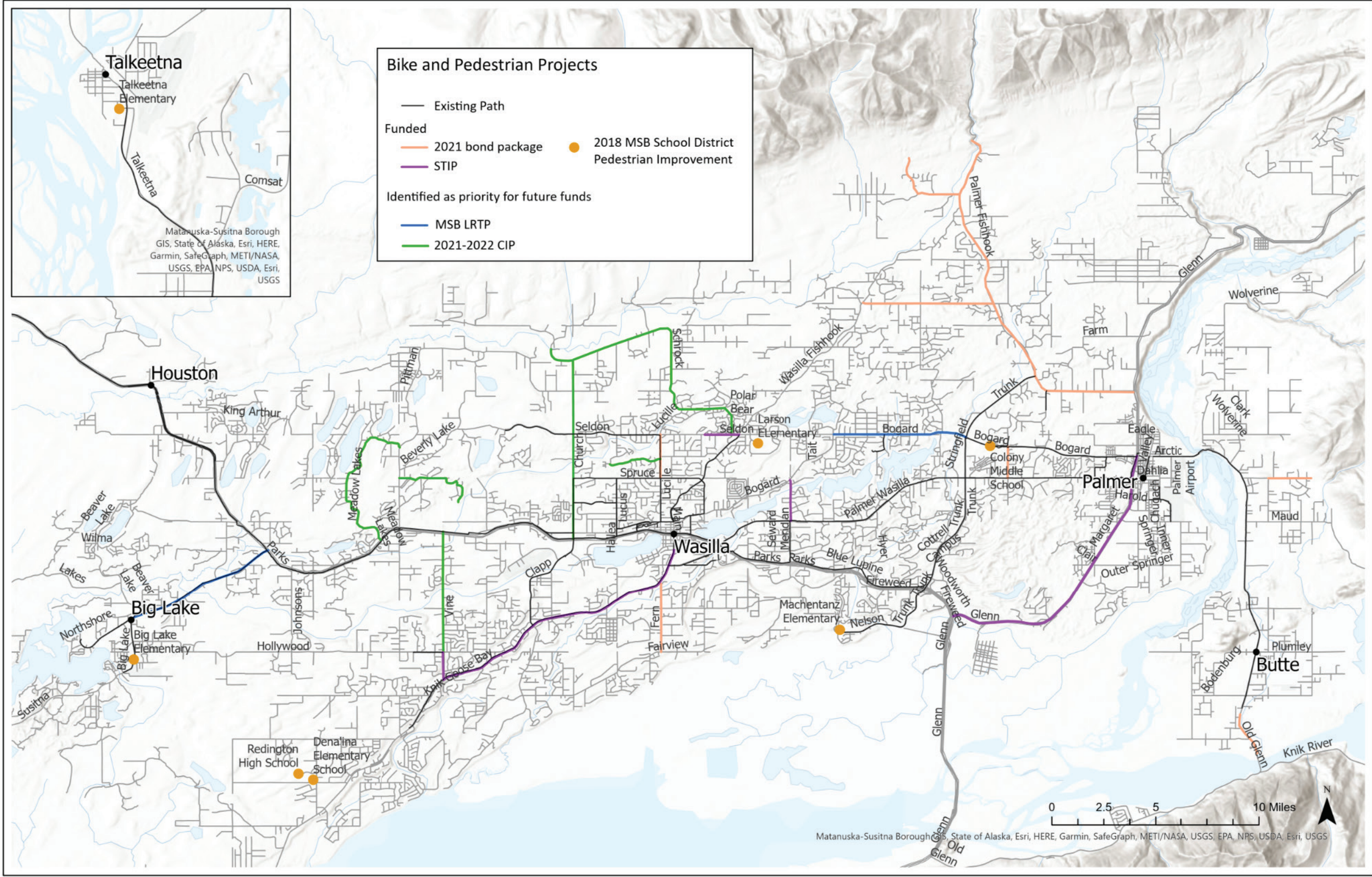


Figure 3 - Bicycle and pedestrian projects identified in other plans

3.3 POLICIES AND REGULATIONS

Implementation of planned bicycle and pedestrian infrastructure is guided by local, state, and federal policies and regulations. These include design standards, engineering best practices, and local code.

3.3.1 NATIONAL

3.3.1.1 2010 AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN

The “2010 Standards” set minimum requirements, both scoping and technical, for newly designed and constructed or altered state and local government facilities, public accommodations, and commercial facilities to be readily accessible to and usable by individuals with disabilities.

3.3.1.2 PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG)

Beginning in 1992, specific guidelines were proposed to regulate accessibility on public streets, within public rights-of-way. These guidelines became known as PROWAG. These guidelines remain in development, but are the recommended best practices when planning, designing, and constructing pedestrian features within public rights-of-way.

3.3.1.3 GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES, 2012 FOURTH EDITION, AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO)

A comprehensive design guide on how to incorporate bicycle travel into roadway design. This guide is the primary reference used by engineering professionals in designing infrastructure for bicycles.

3.3.1.4 COMPLETE STREETS

Complete Streets policies are enacted at the local level to ensure all streets are safe and feel safe for all users. The Federal Highway Administration (FHWA) and the National Complete Streets Coalition support communities in developing a context sensitive Complete Streets policy.

3.3.2 DOT&PF

3.3.2.1 ALASKA TRAFFIC MANUAL

The Alaska Traffic Manual is Alaska’s standard traffic control device manual for public roads. It consists of the Manual on Uniform Traffic Control Devices (MUTCD) and the Alaska Traffic Manual Supplement. It references the Alaska Sign Design Specifications, which is the sign layout standard for Alaska public roads. For non-motorized transportation, the ATM includes sections on traffic control for school areas and traffic control for bicycle facilities.

3.3.2.2 ALASKA HIGHWAY PRECONSTRUCTION MANUAL 2020

The DOT&PF’s preconstruction manual covers all aspects of highway project development – from preliminary engineering and environmental through final Plans, Specifications, and Estimates (PS&E). The manual does not prescribe design standards directly. However, it does outline the steps in the process and potential studies and analyses that may be needed when developing new or improved roadway infrastructure. The Design Study Report is a key step of the development process and is the step in which pedestrian and bicycle accommodations are identified.

3.3.3 MAT-SU BOROUGH

3.3.3.1 MSB CODE

Borough code does not contain design standards for roadways, bicycle facilities, or pedestrian facilities. Title 11 directs the Public Works Department to establish standards for the design and construction of roads in the borough. Title 43 covers borough subdivisions. It does not prescribe the construction of sidewalks or pathways.

3.3.3.2 MSB SUBDIVISION CONSTRUCTION MANUAL 2020

This document provides established standards for the design of roads. Road standards are prescribed based on the roadway's functional classification. There are no provisions for mandatory bicycle or pedestrian facilities along subdivision roads. The manual indicates that bicycle and pedestrian pathway design shall follow the current edition of the Guide for the Development of Bicycle Facilities (AASHTO).

3.3.3.3 MSB DESIGN CRITERIA MANUAL

The MSB is in the process of creating a design criteria manual that will include sections on non-motorized facilities.

3.3.4 CITY OF WASILLA

The City's *Downtown overlay district design standards* sets requirements for sidewalks (Table 2) and identifies sidewalk clear zone standards. This document contains design standards that apply to the development of public and private areas and for the creation of systems and amenities that are beneficial to the public, in the Downtown Overlay District and all of its sub-districts. The design standards contained in this article are mandatory. The instructions for application, enforcement, and interpretation of these standards are further discussed in Title 16 of the City of Wasilla Municipal Code. Title 16 contains detailed definitions of as well as the description of the downtown overlay district and its boundaries. Within the downtown overlay district, the regulations set forth in this article shall be the minimum requirements and shall apply uniformly to each class or kind of building, structure, or land.

Table 2 - City of Wasilla downtown overlay district sidewalk requirements

Corridor	Min. distance from back of curb (BOC)	Paved furniture zone at BOC	Sidewalk req. both sides	Min. width of sidewalk
Avenues	2'	NA	Y	6'
Parkways	2'	NA	Y	5'
Boulevards	4.5'	NA	Y	5'
Main Streets	4'	4'	Y	8'
Urban Streets	4'	4'	Y	6'
Local Streets	3'	NA	Y	5'
Alleys	NA	NA	NA	NA

4.0 ISSUES AND NEEDS

4.1 CRASH ANALYSIS

Crash data for the period 2010-2019 were collected and analyzed to determine if there were any hotspots of crash activity and to get a general sense of the severity and quantity of crashes involving bicycles and pedestrians. DOT&PF provided raw tabular data that included the crash location, type and severity of the crash, time of day, day of week, year, and weather conditions. The locations were mapped using ArcGIS Pro software.

The number of bicycle and pedestrian crashes was relatively low with only 103 reported incidents over ten years (Figure 4). However, approximately 33% of all crashes resulted in either serious injury or a fatality. All seven fatalities were pedestrians. Crashes were nearly even between cyclists and pedestrians, with 52 cyclist crashes and 51 pedestrian crashes.

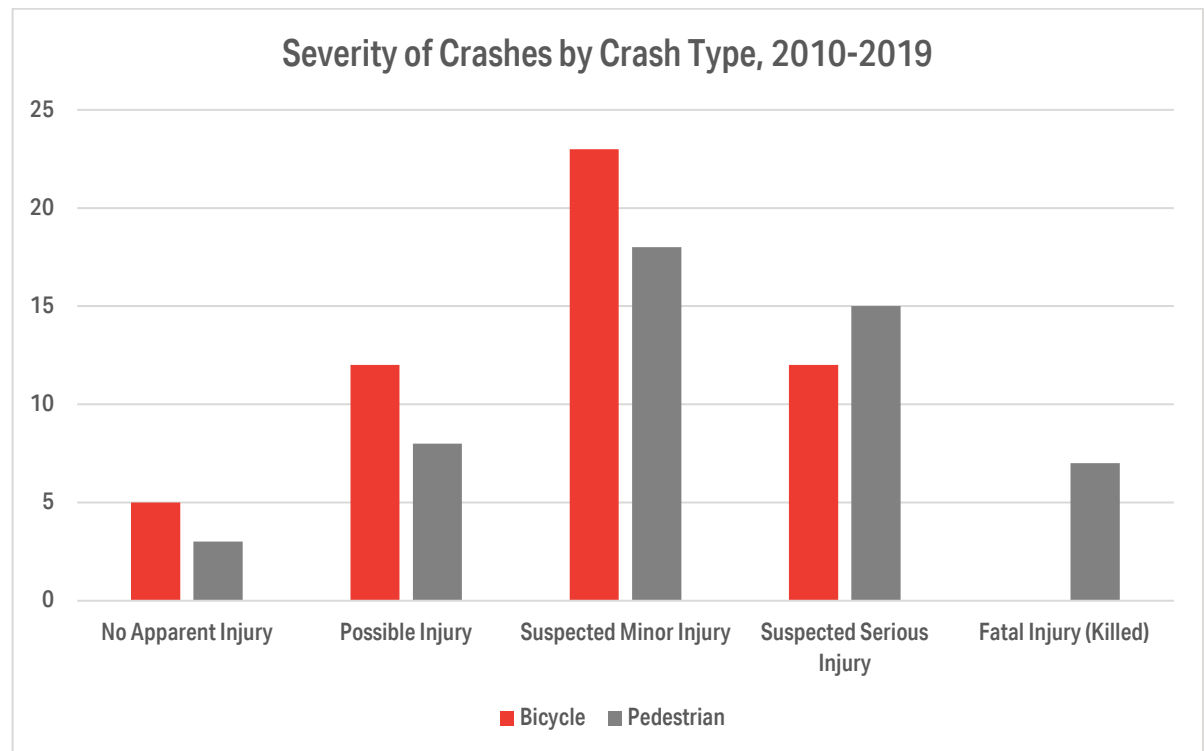


Figure 4 - Crash Severity by Crash Type, 2010-2019

Crashes occurred throughout the borough, with slightly more incidents in Wasilla and Palmer than surrounding areas (Figure 5 and Figure 6). This is likely due to the higher density of residents and higher traffic volumes in these areas.

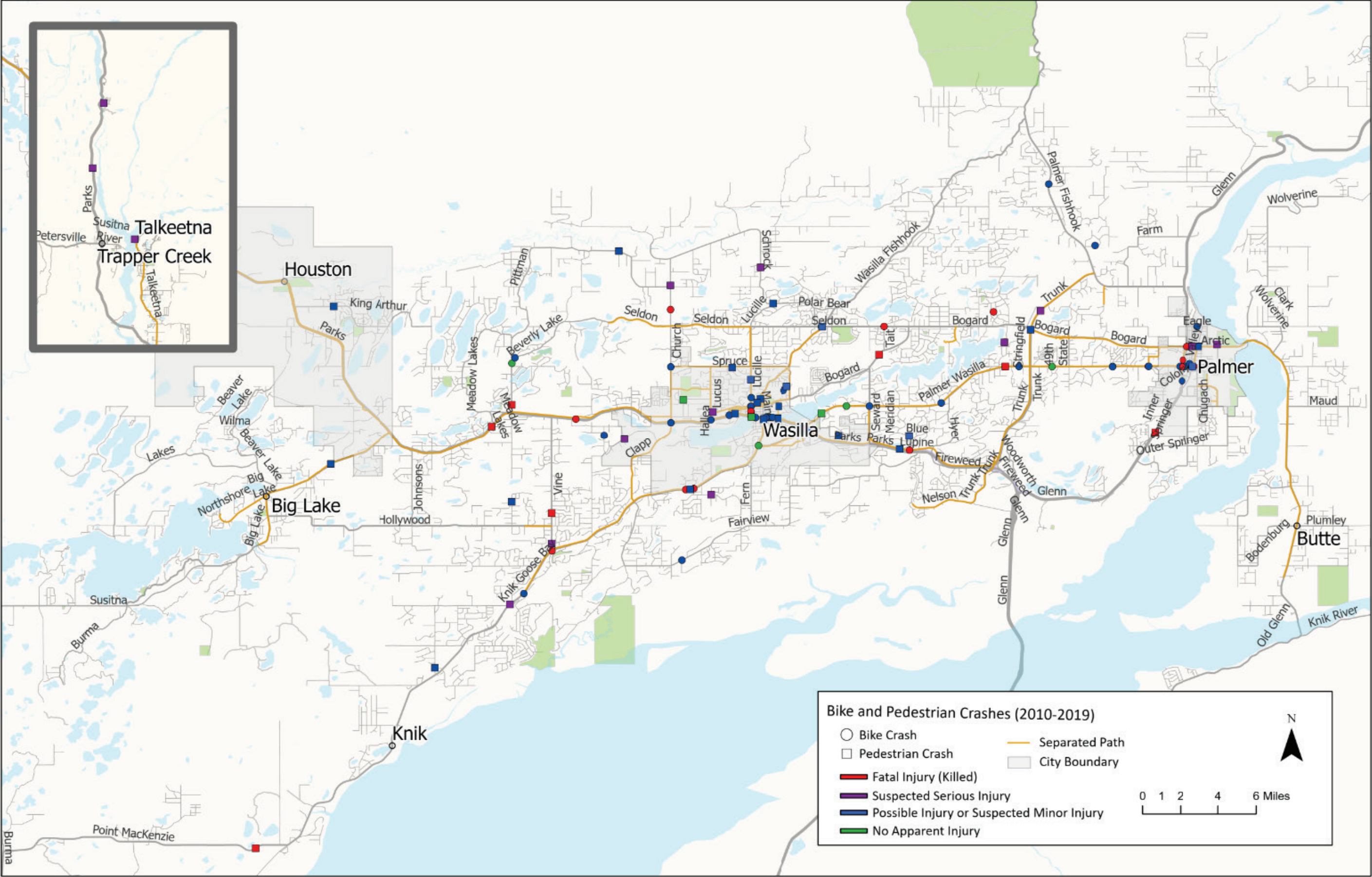


Figure 5 - Bicycle and pedestrian crash locations, 2010-2019

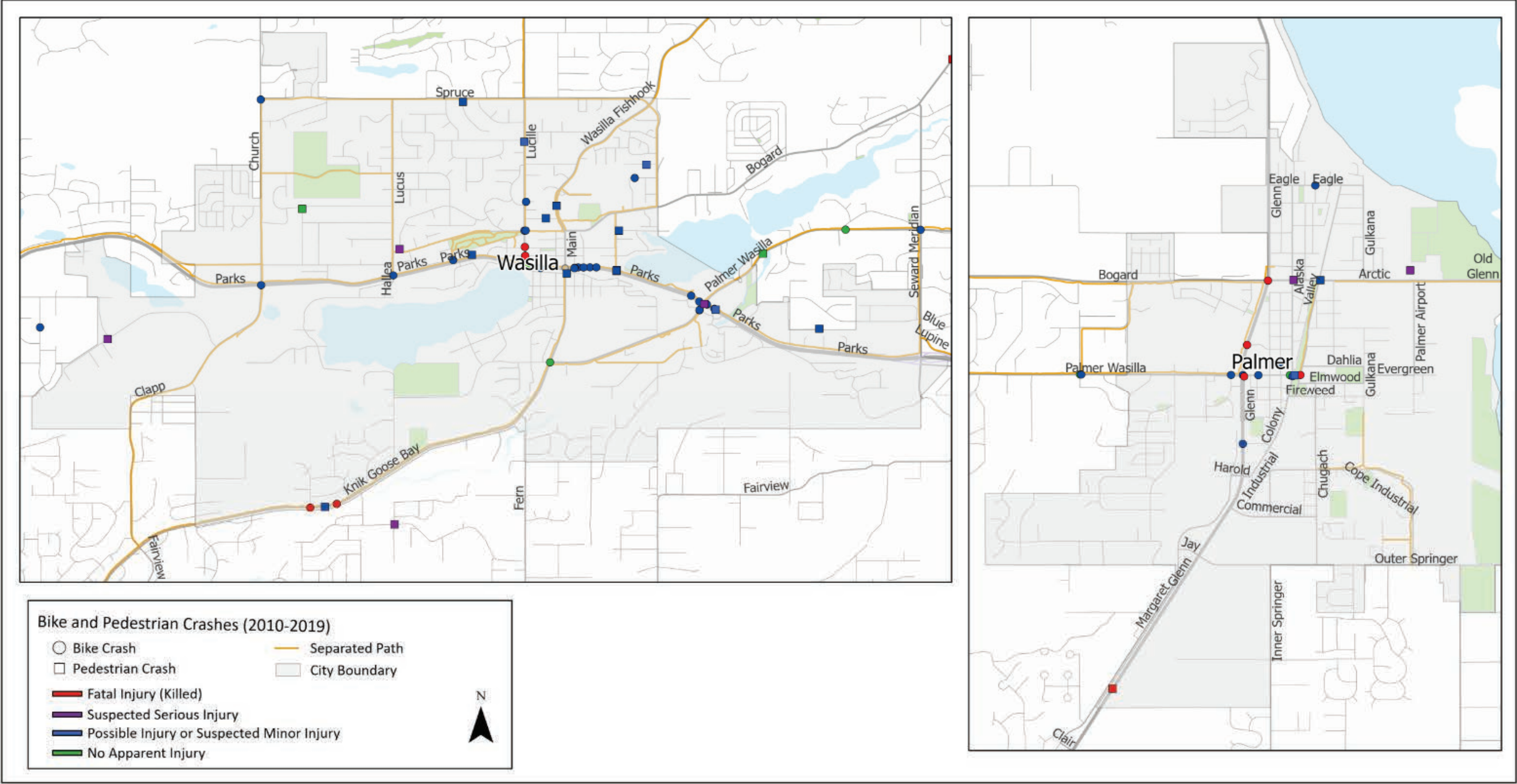


Figure 6 - Bicycle and pedestrian crash hotspots



For the period analyzed, the number of crashes has declined for both bicycles and pedestrians (see Figure 7). In 2013, there was a higher number of bicycle crashes (17) than in subsequent years. The cause of this spike in bicycle crashes is not clear from the data.

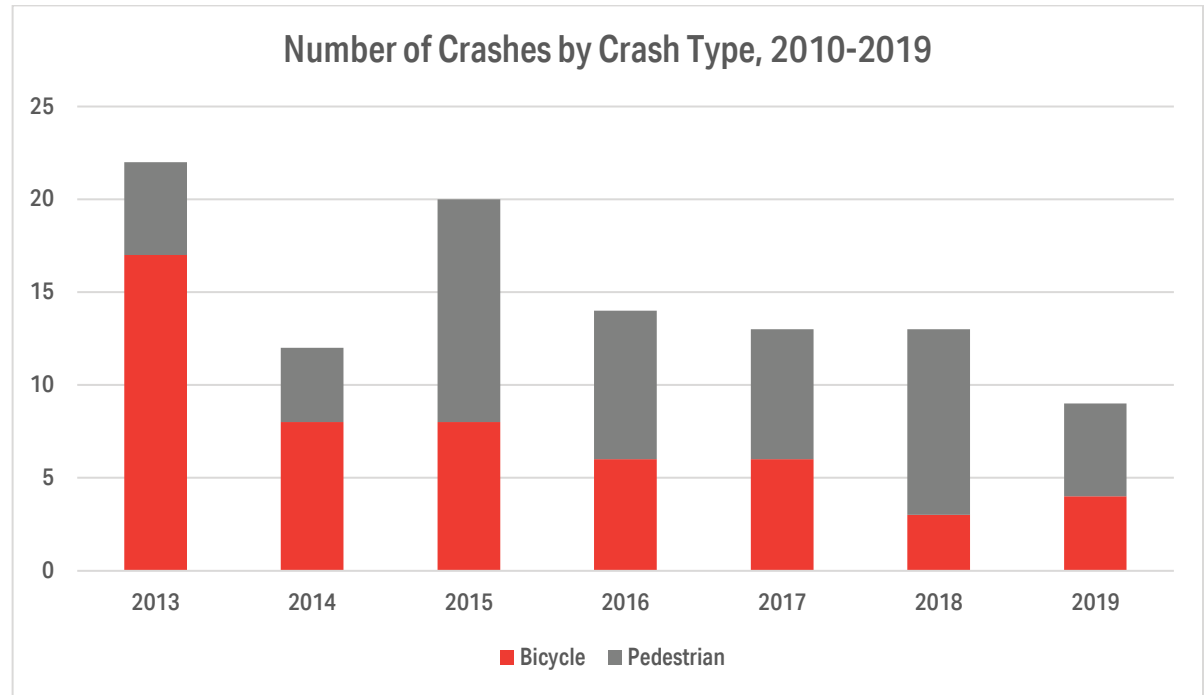


Figure 7 - Crashes by Crash Type, 2010-2019

Most crashes involving a pedestrian or cyclist happened at intersections (see Table 3). Intersections are locations where pedestrians or cyclists are crossing the path of motor vehicles, which introduces potential conflicts between modes.

Table 3 - Summary of crashes at intersections

Summary of Bike and Pedestrian Crashes		
Total Bike and Pedestrian Crashes		103
Percent Bike Crashes		50%
	At an Intersection	82%
	On a Dark, Not Lighted Street	<1%
Percent Pedestrian Crashes		50%
	At an Intersection	67%
	On a Dark, Not Lighted Street	18%



73% of crashes occurred during daylight (see Figure 8). This is likely because there is more bicycle and pedestrian activity during the summer, when available daylight is at its peak.

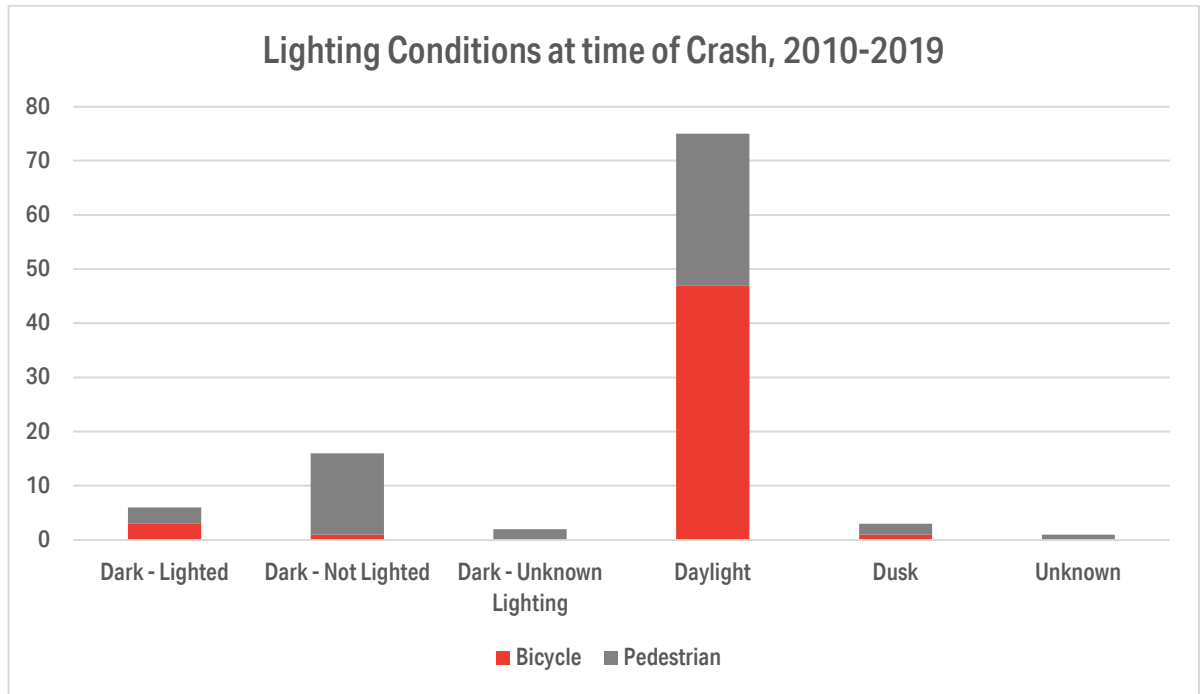


Figure 8 - Lighting conditions at the time of crash, 2010-2019



Crashes show a seasonal pattern with crashes peaking in July (Figure 9). September and October are the second highest months for crashes, likely due to the beginning of school when students are walking or biking to school before cold weather. January and November did not experience any bicycle crashes, and there was only one each for February, March, and December. This is presumably due to the reduction in the number of cyclists during the winter.

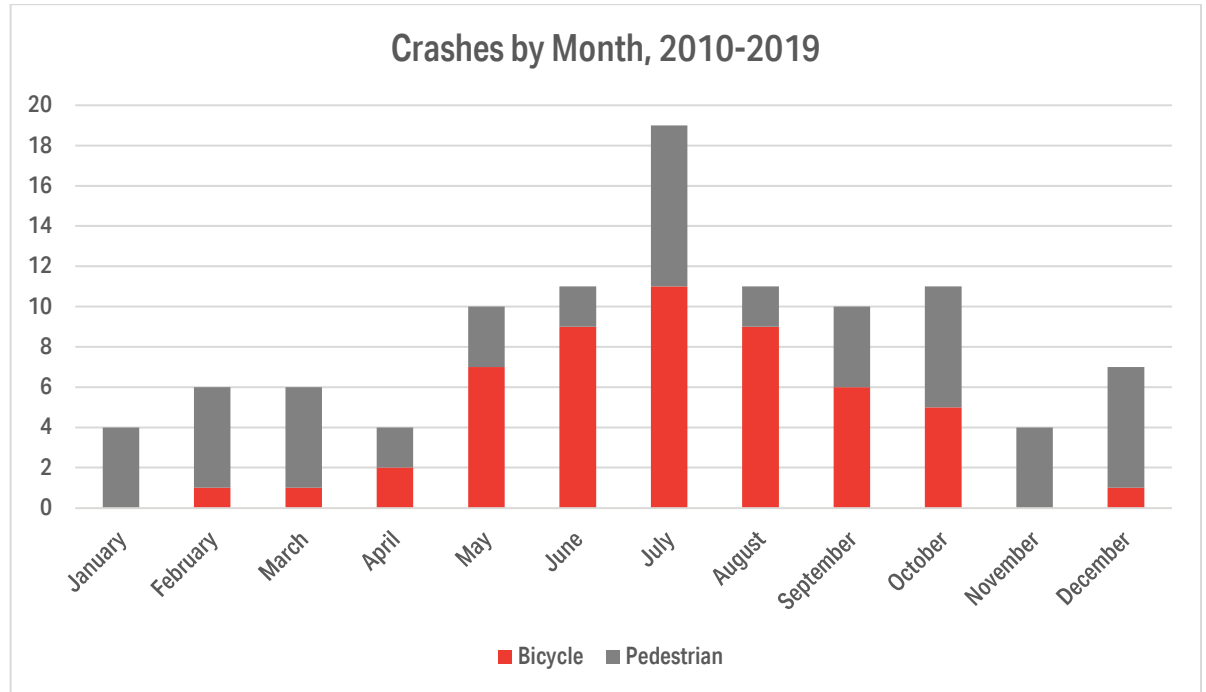


Figure 9 - Crashes by month



Figure 10 shows the number of crashes by time of day. Bicycle and pedestrian crashes peak in early afternoon and early evening. There is also a spike in crashes for both pedestrians and cyclists between midnight and 1am. Very few crashes occurred between 1am and 8am.

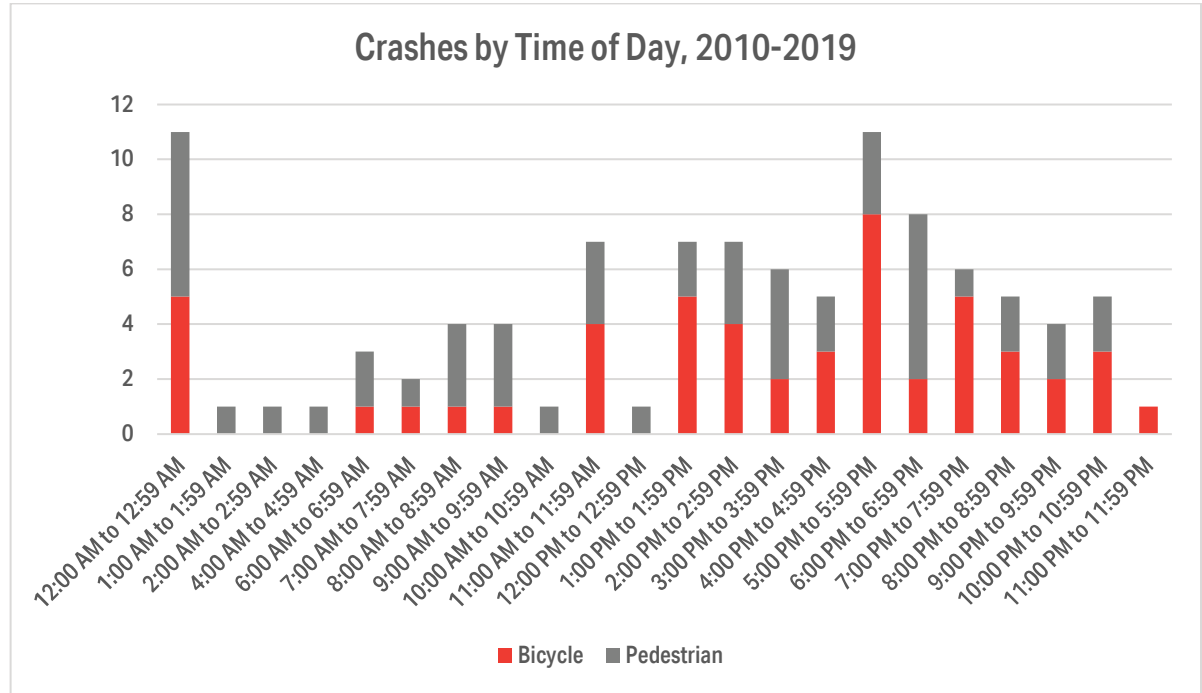


Figure 10 - Crashes by time of day

4.2 NETWORK GAPS & CONNECTIVITY

A combination of analysis, review of planned projects, one-on-one stakeholder interviews, and public comments have been used to develop a preliminary list of network gaps and connectivity issues. Public comments were obtained via an online comment map and individual emails to the project team or MSB staff.

Table 4 – Significant gaps in the bicycle and pedestrian network

What is the gap?	Description	What is the source of this need?
Smith Road Extension pathway	Provide a pathway connection between the Old Glenn Highway and the recreational trailhead access	2021 Transportation System Package
Old Glenn Highway pathway between Sodak Circle and Knik River	There is a gap in the pathway	2021 Transportation System Package
49 th State Street pathway between Bogard Road and Palmer-Wasilla Highway	No dedicated bicycle/pedestrian facility between two major corridors	2021 Transportation System Package
Lucille Street pathway from Spruce Street to Seldon Road	No dedicated bicycle/pedestrian facility along this busy corridor that includes access to an elementary school. The crash analysis showed several bike/pedestrian crashes on this segment during 2010-2019.	2021 Transportation System Package
Tex-Al Drive pathway	The connection between the east and west segments of Tex-Al Drive would provide a major east-west corridor in this area of the borough	2021 Transportation System Package
Edgerton Parks Road and Mountain Trails Drive pathway	A pedestrian/bicycle facility would connect the Palmer-Fishhook Road to the Government Peak Recreational Area	2021 Transportation System Package
Palmer-Fishhook Road pathway	This fast-growing area of the borough needs a connection between the Trunk Road pathway, the Glenn Highway, and the Little Susitna Bridge	2021 Transportation System Package; Fishhook Comprehensive Plan; public input
Fern Street pathway	Provide a connection between KGB Road and Fairview Loop in this fast-growing portion of the borough	2021 Transportation System Package; public input
Bogard Road pathway	Provide a dedicated pathway on this busy road between Trunk Road and Seldon Road	2017 MSB Long-range Transportation Plan
West Karen Street pathway	Provide a separated pathway along this busy corridor to connect the Meadow Lakes Community Center with Pittman Road	2021-22 MSB Capital Improvement Plan
Church Road pathway	Provide a connection north from the Church Road separated path that ends at Spruce Avenue to Schrock Road	2021-22 MSB Capital Improvement Plan
Schrock Road pathway or widened shoulders	Provide a connection from Church Road to the Little Susitna River to connect the Pittman Road, Church Road, and Parks Highway Master Circle trail system	2021-22 MSB Capital Improvement Plan

What is the gap?	Description	What is the source of this need?
Vine Road pathway	This fast-growing area of the borough does not have a bicycle/pedestrian connection between Parks Highway, Hollywood Road, and KGB Road. This would also connect to the separated path on West Hollywood Road that extends to Goose Bay Elementary School. There were three serious bike/pedestrian crashes on this segment between 2015 and 2019.	2021-22 MSB Capital Improvement Plan
Meadow Lakes Loop Road pathway	Connect Parks Highway separated path to the intersection of Pittman Road to accommodate bicycles and pedestrians along this significant corridor in a fast-growing area of the borough	2021-22 MSB Capital Improvement Plan
Pittman Road pathway or widened shoulders	Provide a dedicated space for bikes and pedestrians along this busy road between the separated path on Parks Highway all the way to Church Road	Analysis; public input
Inner and Outer Springer Loops sidewalks, pathway, or widened shoulders	This area is projected to grow significantly over the next 20 years. There are no dedicated bicycle or pedestrian facilities along these roads. A dedicated facility would provide connections between residential areas and the core Palmer commercial area (via Chugach Street sidewalks)	Analysis; public input
Knik-Goose Bay Road pathway	Extending the separated path from South Settlers Bay Road to Malemute Run would provide a safe facility for pedestrians and cyclists along this high-volume, high-speed road in a fast-growing area of the borough.	Analysis; public input
Fairview Loop pathway	This fast-growing region of the borough would benefit from a non-motorized facility that connects the separated path at Top of the World Circle to South Abby Boulevard. The facility would service residential areas, an elementary school, and a recreational area.	Analysis; public input
Parks Highway pathway	There is no dedicated facility north of Willow that connects to the separated path along Talkeetna Spur Road.	Public input
Glenn Highway pathway	Extending the separated path north from Scott Road to Palmer-Fishhook Road would connect neighbors to schools and the commercial center of Palmer. This would complete a loop if a Palmer-Fishhook Road path is constructed.	Analysis; public input; Palmer Comprehensive Plan
Glenn Highway pathway	Connecting Palmer-Fishhook Road to the separated path at Jonesville Road in Sutton would provide connectivity between the Sutton and Palmer.	Public input; Palmer Comprehensive Plan

What is the gap?	Description	What is the source of this need?
Wasilla-Fishhook Road pathway	There is no constructed bicycle/pedestrian facility north of Seldon Road. A dedicated non-motorized facility extending to Palmer-Fishhook Road would create a loop that connects dozens of subdivisions throughout this fast-growing area.	Fishhook Comprehensive Plan; analysis; public input
Bodenburg Loop pathway	The Butte Trail is a significant recreational destination. Connecting the trailhead to the Old Glenn Highway path would fill the gap along Bodenburg Loop.	Analysis; public input
Maud Road pathway	Development in the area has increased traffic on Maud Road. The road is narrow with narrow shoulders. A separated non-motorized path would connect residential areas to the Old Glenn Highway path.	Analysis; public input
Seward Meridian Road pathway	Healthcare facilities along Seward Meridian generate considerable vehicular traffic. There is no dedicated bike/pedestrian facility north of Palmer-Wasilla Highway. A separated path would connect to the path along Seward Meridian south of Palmer-Wasilla Highway and provide connections to an elementary school, businesses, and healthcare facilities.	Public input



4.3 MAINTENANCE

Maintaining bicycle and pedestrian infrastructure throughout the year is important to ensure year-round use by the residents that rely on walking or biking for their everyday needs. Winter maintenance includes removal of snow and ice from walkways and paths, as well as providing sand or gravel for traction. Summer maintenance entails sweeping up gravel and brushing back vegetation.

Snow removal from sidewalks and shared-use paths is a concern. Icy sidewalks or sidewalks full of snow make it difficult or dangerous to walk, particularly for the elderly and individuals with mobility impairments. Inconsistent snow removal is also a concern as it makes it difficult for a pedestrian or cyclist when they encounter differing levels of snow removal along their route.

Once the snow and ice melts, there can be a significant amount of gravel left behind on roadway shoulders. This is also a problem on shared-use paths where all-terrain vehicles (ATVs) ride adjacent to or cross the path and kick gravel onto the path. Gravel is particularly problematic for road bikes with narrow tires. Increased sweeping efforts would address this need.

Each facility owner employs different maintenance policies for maintaining their infrastructure.

4.3.1 DOT&PF

The DOT&PF removes snow from roadways and pathways according to their assigned priority. Priority is generally based on the functional classification of the roadway. Thus, higher functioning roads receive higher priority snow removal. Sidewalks and pathways share the same priority level as the adjacent roadway. However, in practice, snow is not removed from paths and sidewalks as quickly as it is removed from roads. Often this is due to the need for specialty equipment or manual labor to remove snow from pedestrian paths.

4.3.2 MSB

Most road maintenance on borough roads is contracted out to private parties. After a snowfall of four inches or more, plows must plow main roads and school bus routes within 12 hours. Snow removal priority, in descending order, is primary collectors, secondary roads, and then subdivision roads. In addition to snow plowing, the borough oversees sanding, grading of gravel roads, drainage control, minor road repairs, and pavement patching. The borough maintains more than 1,100 miles of road.

4.3.3 CITY OF WASILLA

The City of Wasilla snow removal plan includes continuous plowing of a primary 'snow route' on main city roads, with the next priority being downtown streets, and then subdivision streets. Sidewalk plowing and sanding is not specified in the snow removal plan, and there are no sidewalk maintenance provisions in the city municipal code.



4.3.4 CITY OF PALMER

Snow removal and sanding are performed on city streets and roads in the following order of priority:

1. Main arterial and business district streets, school routes, etc.
2. Main feeder or collector streets leading to arterial streets and highways, and Palmer Municipal Airport.
3. Neighborhood residential streets and subdivisions.
4. All other low-density streets

Municipal code specifies those owners or occupants of premises bordered by a paved sidewalk are responsible for removal of snow and ice from sidewalks. The City of Palmer Department of Public Works' *Snow Removal Information* states that the city will remove snow and ice from sidewalks around city buildings and will attempt to clear snow along school routes, bike paths, and a single lane along core downtown area sidewalks, as time allows.

4.3.5 CITY OF HOUSTON

The City of Houston prioritizes school routes for snow removal, followed by main roads, then side streets. Houston municipal code does not include provisions on sidewalk maintenance, and sidewalks are not explicitly included in the city snow removal plan

4.4 USER CONFLICTS

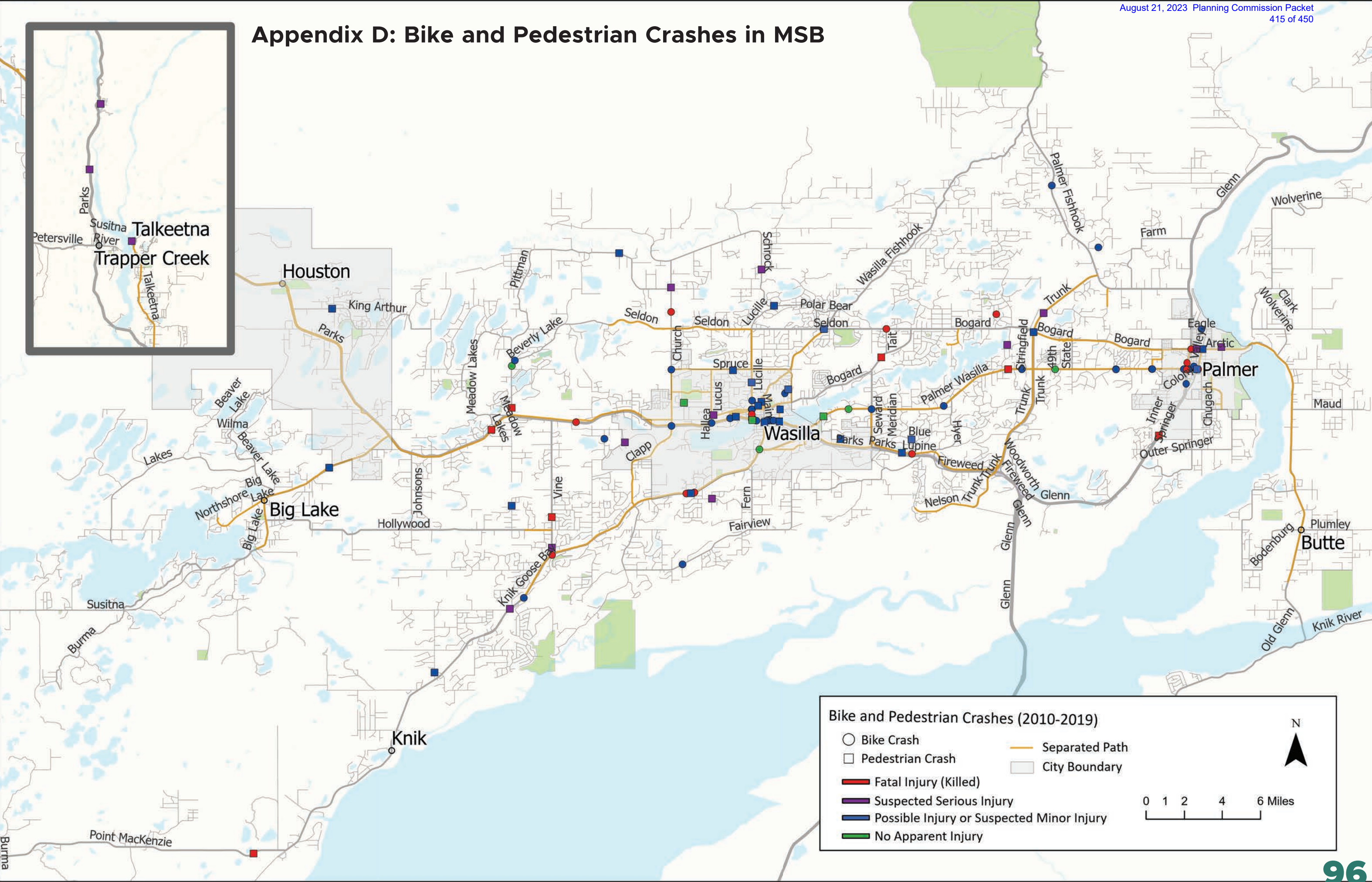
Conflicts between non-motorized users and motorized users are common across Alaska. Issues in the MSB include motorized use of non-motorized facilities, motorized users riding adjacent to non-motorized facilities and causing damage to pathways, and motorized users kicking gravel and debris onto non-motorized facilities.

Additionally, there have been reported conflicts between non-motorized users and motor vehicles in the borough. This is generally between motorists and cyclists that are riding on the roadway. Cyclists on road bikes with narrow tires often cannot ride on separated pathways due to gravel or poor-quality asphalt and thus ride in the road or on the shoulder.

5.0 SUMMARY

The bicycle and pedestrian network in the Mat-Su Borough provides key connections to destinations across the borough. The infrastructure projects currently in development will provide additional connections and ensure safe travel for walkers and cyclists. However, there are still several significant gaps in the non-motorized network. As the borough continues to grow and traffic volumes increase, it is important to fill these gaps and enable mobility for everyone in the borough.

Appendix D: Bike and Pedestrian Crashes in MSB





Appendix F: Potential Grant Opportunities

Grant	Provider	Link
Community Transportation Program	DOT&PF	https://dot.alaska.gov/stwdplng/cip/stip/projects/CTP.shtml
Transportation Alternative Program	DOT&PF	https://dot.alaska.gov/stwdplng/cip/stip/projects/TAP.shtml
Complete Streets	FHWA	https://highways.dot.gov/complete-streets/make-complete-streets-default-approach
Tribal Transportation Program Safety Fund	BIL	https://highways.dot.gov/federal-lands/programs-tribal/safety/funds
Railway Highway Crossing Program	BIL	https://highways.dot.gov/safety/hsip/xings/railway-highway-crossing-program-overview
Transportation Funding Opportunities for Tribal Nations		https://highways.dot.gov/sites/fhwa.dot.gov/files/docs/federal-lands/programs-tribal/36311/transportation_funding_opportunities_for_tribal_nations.pdf
Guide for Maintaining Pedestrian Facilities for Enhanced Safety Research Report	FHWA	https://safety.fhwa.dot.gov/ped_bike/tools_solve/fhwasa13037/research_report/chap2f.cfm
Congestion Mitigation and Air Quality Improvement Program	BIL	https://www.fhwa.dot.gov/bipartisan-infrastructure-law/cmaq.cfm
Carbon Reduction Program	BIL	https://www.fhwa.dot.gov/bipartisan-infrastructure-law/crp_fact_sheet.cfm
Metropolitan Planning Program	BIL	https://www.fhwa.dot.gov/bipartisan-infrastructure-law/metro_planning.cfm
STBG Transportation Alternatives set-aside	BIL	https://www.fhwa.dot.gov/bipartisan-infrastructure-law/ta.cfm
Pedestrian and Bicycle Funding Opportunities: U.S. Department of Transportation Transit, Safety, and Highway Funds	USDOT	https://www.fhwa.dot.gov/environment/bicycle_pedestrian/funding/funding_opportunities.pdf?u=092922
Bicycle and Pedestrian Funding, Design, and Environmental Review: Addressing Common Misconceptions	FHWA	https://www.fhwa.dot.gov/environment/bicycle_pedestrian/guidance/misconceptions.cfm
Surface Transportation Block Grant Program	BIL	https://www.fhwa.dot.gov/specialfunding/stp/bil_stbg_implementation_guidance-05_25_22.pdf
Community Placemaking Grants	Project for Public Spaces	https://www.pps.org/community-placemaking-grants
Urbanized Area Formula Grants - 5307	FTA	https://www.transit.dot.gov/funding/grants/urbanized-area-formula-grants-5307
Pilot Program for Transit-Oriented Development Planning - Section 20005(b)	FTA	https://www.transit.dot.gov/TODPilot
Safe Streets and Roads for All	BIL	https://www.transportation.gov/sites/dot.gov/files/2022-03/Safe-Streets-and-Roads-for-All-Fact-Sheet_March-2022.pdf

CODE ORDINANCE

Sponsored by:
Introduced:
Public Hearing:
Action:

**MATANUSKA-SUSITNA BOROUGH
ORDINANCE SERIAL NO. 23-068**

AN ORDINANCE OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY ADOPTING
THE MATANUSKA-SUSITNA BOROUGH BICYCLE AND PEDESTRIAN PLAN.

WHEREAS, all the information regarding this ordinance can be
found in Information Memorandum No. 23-XXX.

BE IT ENACTED:

Section 1. Classification. This ordinance is of a general and
permanent nature and shall become a part of the Borough Code.

Section 2. Amendment of section. MSB 15.24.030(B)(12) is
hereby amended to read as follows:

**(a) Matanuska-Susitna Borough Bicycle and
Pedestrian Plan, adopted 2023.**

Section 3. Effective date. This ordinance shall take effect
upon adoption.

ADOPTED by the Matanuska-Susitna Borough Assembly this - day
of -, 2023.

EDNA DeVRIES, Borough Mayor

ATTEST:

LONNIE R. McKECHNIE, CMC, Borough Clerk
(SEAL)

By:
Introduced:
Public Hearing:
Action:

MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION RESOLUTION NO. 23-30

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH PLANNING COMMISSION
RECOMMENDING THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY APPROVAL OF
THE MATANUSKA-SUSITNA BOROUGH'S BICYCLE AND PEDESTRIAN PLAN.

WHEREAS, the Assembly adopted the Long-Range Transportation
Plan (LRTP) in 2017; and

WHEREAS, the LRTP consists of goals and strategies to build
out a better-connected and safer transportation network, including
goals to increase mobility and transportation choices; and

WHEREAS, one of the strategies in the LRTP is to develop an
active transportation master plan that creates a vision for the
bicycle and pedestrian system and identifies recommendations to
implement that vision; and

WHEREAS, the Matanuska-Susitna Borough Planning Department
has developed the Borough's first Bike and Pedestrian Plan to
implement LRTP strategies and goals pertaining to nonmotorized
transportation; and

WHEREAS, the Bike and Pedestrian Plan (BPP) was developed by
understanding the existing system of sidewalks, pathways, roads,
and development, and incorporates an economic analysis that shows
increased transportation choices have positive impacts on the

social, physical, economic, and environmental qualities of life that MSB residents enjoy; and

WHEREAS, the BPP gathered feedback from community councils, MSB advisory boards, transportation planners and engineers, local governments, bike & pedestrian advocate groups, developers and real estate agents, and the general public through steering committee meetings, public meetings and events, and a 45-day public comment period; and

WHEREAS, the public process allowed for timely comment and discussion on possible routes, neighborhood concerns, increasing safe routes to school, development standards, and other pertinent information; and

WHEREAS, there were seventy-two public comments received and those comments were incorporated into the BPP where appropriate; and

WHEREAS, the Transportation Advisory Board, the Parks, Recreation, and Trails Advisory Board, and the Local Road Service Area Advisory Board adopted resolutions of support for the Bicycle and Pedestrian Plan; and

WHEREAS, the BPP provides a clear path forward for the Matanuska-Susitna Borough to begin implementing the recommended policies, infrastructure, and programs outlined in the BPP.

NOW, THEREFORE, BE IT RESOLVED, that the Matanuska-Susitna Borough Planning Commission hereby recommends approval of an ordinance adopting the Bicycle and Pedestrian Plan.

NOW, THEREFORE, BE IT FURTHER RESOLVED, that the Matanuska-Susitna Borough Planning Commission recommends the Matanuska-Susitna Borough Assembly dedicate funding and resources to support the implementation of the Bicycle and Pedestrian Plan.

ADOPTED by the Matanuska-Susitna Borough Planning Commission this -- day of --, 2023.

C.J. Koan, Chair

ATTEST

KAROL RIESE, Planning Clerk

(SEAL)

YES:

NO:

INTRODUCTION FOR PUBLIC HEARING LEGISLATIVE

Resolution No. 23-04

Amending MSB 17.05: Essential Services

(Pages _ - _)

INTRODUCTION FOR PUBLIC HEARING

for installation of high-voltage powerlines within the Borough. Approval of the conditional use permit is done through the Planning Commission who will review several standards related to trespass issues, scenic resources, and the MSB comprehensive plan. Utility companies will also be required to host a public process prior to submitting a conditional use permit for review by the Planning Commission.

DRAFT

CODE ORDINANCE

Sponsored by:
Introduced:
Public Hearing:
Action:

**MATANUSKA-SUSITNA BOROUGH
ORDINANCE SERIAL NO. 23-080**

AN ORDINANCE OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY AMENDING MSB 17.05 ESSENTIAL SERVICE UTILITIES BY PROVIDING ADDITIONAL PROCESS AND STANDARDS FOR APPROVAL.

BE IT ENACTED:

Section 1. Classification. This ordinance is of a general and permanent nature and shall become a part of the Borough Code.

Section 2. Amendment of section. MSB 17.05 is hereby amended as follows:

17.05.010 PURPOSE.

(A) The purpose of this chapter is to provide for public participation in [THE] decisionss affecting the installation of essential service utilities in such a manner that they enhance the health, safety and general welfare of borough residents and properties while minimizing negative impacts. This chapter recognizes the importance and benefits of essential services, while ensuring that all reasonable [FEASIBLE] mitigation measures are taken by the utilities to protect the public health, safety, welfare, and scenic qualities of the Matanuska Susitna Borough. [ESSENTIAL SERVICES SHOULD BE

INSTALLED IN COGNIZANCE OF EXISTING AND PROJECTED DEMANDS FOR SUCH SERVICES.]

(B) Essential service[S] **utilities** should be installed in cognizance [OF EXISTING AND PROJECTED DEMANDS FOR SUCH SERVICES] **of the goals and objectives of borough-adopted comprehensive plans.**

(C) This chapter establishes the process for borough review and approval for essential service utilities to ensure all reasonable mitigation measures are incorporated to minimize negative impacts to the community and shared public assets.

17.05.020 APPLICABILITY.

(A) This chapter applies to all areas of the borough except within the city boundaries of Houston, Palmer, Wasilla, and the Port MacKenzie special use district boundary. [THIS CHAPTER APPLIES TO MUNICIPAL UTILITIES THAT EXTEND BEYOND CITY BOUNDARIES AND THE PORT DISTRICT BOUNDARY INTO UNINCORPORATED BOROUGH LANDS.]

(B) A conditional use permit in accordance with this chapter shall be obtained prior to construction of Type II Essential Service Utilities.

(1) Minor relocation of Type II Essential Service Utilities that have been approved under this chapter is allowed without an additional permit to the extent that relocation:

(a) generally follows the approved route;

(b) is necessary due to reasons beyond control of the utility company.

17.05.030 TYPE I ESSENTIAL SERVICE UTILITY.

(A) All proposed Type I essential service utilities, when installed or extended in any public way or borough-owned land, shall require a permit issued by the borough in accordance with MSB 11.30.030.

[17.05.040 TYPE II ESSENTIAL SERVICE UTILITY.

(A) ALL PROPOSED TYPE II ESSENTIAL SERVICE UTILITIES SHALL REQUIRE A PUBLIC INVOLVEMENT PROGRAM IN ACCORDANCE WITH A PUBLIC PARTICIPATION PLAN AS SUBMITTED BY THE UTILITY IN ALL AREAS OF THE BOROUGH EXCLUDING THE CITIES OF HOUSTON, PALMER, WASILLA, AND THE PORT MACKENZIE SPECIAL USE DISTRICT BOUNDARY.

(B) WITHIN 20 CALENDAR DAYS OF RECEIPT OF THE PROPOSED PUBLIC PARTICIPATION PLAN, THE DIRECTOR SHALL PROVIDE THE APPLICANT WITH WRITTEN ACKNOWLEDGEMENT OF

RECEIPT OF THE PLAN, ALONG WITH ANY RECOMMENDATIONS CONCERNING THE PROPOSED PROCESS. THE PUBLIC INVOLVEMENT PROGRAM, AT A MINIMUM, MUST CONTAIN THE FOLLOWING:

(1) MINIMUM REQUIREMENTS.

(A) THE UTILITY'S PUBLIC INVOLVEMENT PROGRAM MUST COMPLY WITH ESTABLISHED STATE AND FEDERAL GUIDELINES GOVERNING THE UTILITY INCLUDING ADEQUATE PUBLIC NOTICE, PUBLIC PROCESS, PUBLIC MEETINGS, OR PUBLIC HEARINGS;

(B) IF NO ESTABLISHED STATE OR FEDERAL GUIDELINES APPLY TO THE PROPOSED ACTION, THE UTILITY SHALL FOLLOW ITS OWN UTILITY BOARD ADOPTED GUIDELINES FOR PUBLIC NOTIFICATION AND INVOLVEMENT;

(C) IF THERE ARE NO ESTABLISHED STATE, FEDERAL OR UTILITY BOARD ADOPTED GUIDELINES, THE PUBLIC INVOLVEMENT PROGRAM WILL CONSIST OF THE MINIMUM REQUIREMENTS OUTLINED IN SUBSECTION (B) (2) OF THIS SECTION, PUBLIC INVOLVEMENT; AND

(D) IF A STATE, FEDERAL OR UTILITY BOARD ADOPTED PUBLIC INVOLVEMENT PROGRAM IS USED, THEY MUST AT LEAST MEET OR EXCEED THE MINIMUM GUIDELINES IN SUBSECTION (B) (2) OF THIS SECTION, PUBLIC INVOLVEMENT.

(2) PUBLIC INVOLVEMENT.

(A) A MINIMUM OF ONE PUBLIC MEETING WILL BE HELD BY THE UTILITY AND SHALL BE HELD IN AN AREA CENTRAL TO THE AREA IMPACTED BY PROPOSED ACTION;

(B) A MINIMUM OF ONE FORMAL PUBLIC HEARING WILL BE HELD BY THE UTILITY LATER IN THE PROCESS TO ALLOW FOR FORMAL PUBLIC TESTIMONY. THE PUBLIC HEARING WILL BE HELD IN AN AREA CENTRAL TO THE AREA IMPACTED BY PROPOSED ACTION; AND

(C) NOTICE OF THE PUBLIC MEETING AND PUBLIC HEARING TO OCCUR A MINIMUM OF 15 DAYS IN ADVANCE OF THE PUBLIC MEETING OR PUBLIC HEARING. THE PUBLIC NOTICE WILL INCLUDE:

(I) THREE NOTICES IN A NEWSPAPER OF GENERAL CIRCULATION WITHIN THE BOROUGH;

(II) PUBLIC POSTINGS IN LOCAL AREAS SUCH AS LIBRARIES, PUBLIC BUILDINGS, SCHOOLS, STORES, LAUNDROMATS, LODGES, ON THE UTILITY'S WEBSITE, AND ON THE MATANUSKA-SUSITNA BOROUGH'S WEBSITE, ETC.;

(III) PUBLIC SERVICE ANNOUNCEMENTS ON LOCAL RADIO STATIONS STARTING 15 DAYS BEFORE THE PUBLIC MEETING; AND

(IV) MAILINGS, AS APPROPRIATE, INCLUDING NOTIFICATION OF ALL AFFECTED COMMUNITY COUNCILS.

(C) IMPLEMENTATION OF THE PUBLIC INVOLVEMENT PROGRAM SHALL COMMENCE WITHIN 120 DAYS FROM THE ISSUANCE DATE OF WRITTEN ACKNOWLEDGEMENT.

(D) UPON COMPLETION OF THE PUBLIC INVOLVEMENT PROGRAM ELEMENTS, THE APPLICANT SHALL CREATE AND SUBMIT TO THE DIRECTOR A DECISIONAL DOCUMENT THAT DESCRIBES HOW THE PUBLIC INVOLVEMENT PROGRAM WAS IMPLEMENTED, THE NATURE OF PUBLIC COMMENT, THE CHOSEN COURSE OF ACTION, TIMELINE FOR CONSTRUCTION, AND THE PUBLIC'S APPEAL PROCESS. COPIES OF ALL WRITTEN PUBLIC COMMENTS AND AN AUDIO RECORD, IF AVAILABLE, SHALL BE INCLUDED IN THE DECISIONAL DOCUMENT.]

17.05.042 PRE-APPLICATION REQUIREMENTS FOR NEW TYPE II ESSENTIAL SERVICE UTILITIES.

(A) Prior to applying for a conditional use permit, the potential applicant shall hold at least two community meetings:

(1) The meetings shall be held at a facility capable of seating a minimum of 20 people, and is central

to the area or areas impacted by the proposed development;

(2) The meetings shall not start prior to 5 p.m. and no later than 7 p.m.;

(3) Notification of the meetings shall, at a minimum, include the following:

(a) map of the route or routes of the proposed development;

(b) description of the proposed development including type of utility, height, and basic design;

(c) date, time, and location of informational meeting;

(d) contact name, telephone number, and address of applicant; and

(e) comment form created by the borough that has a comment submittal deadline and provides options for submitting comments.

(4) At a minimum, notification for the meeting shall include the following:

(a) property owners within 600 feet of the centerline of the proposed route or routes; and

(b) any community council whose boundary is within 600 feet of the proposed route or routes.

(5) The mailing shall be sent at least 15 calendar days prior to the pre-application meeting.

(B) A written report summarizing the results of the community meetings shall be prepared that includes the following information:

(1) dates and locations of all meetings where citizens were invited to discuss the potential applicant's proposal;

(2) content, dates mailed, and numbers of mailings, including letters, meeting notices, newsletters, and other publications;

(3) sign-in sheet(s) used at each meeting, that includes places for names, addresses, phone numbers, and other contact information such as email addresses;

(4) a list of residents, property owners, and interested parties who have requested in writing that

they be kept informed of the proposed development through notices, newsletters, or other written materials;

(5) the number of people who attended meetings;

(6) copies of written comments received at the meetings;

(7) a certificate of mailing identifying all who were notified of the meetings; and

(8) a written summary that addresses the following:

(a) the substance of the public's written concerns, issues, and problems;

(b) how the applicant has addressed, or intends to address, concerns, issues, and problems expressed during the process; and

(c) concerns, issues, and problems the applicant has not addressed or does not intend to address and why.

(C) The pre-application report shall be submitted to the director. Written approval or rejection of the

report shall be issued by the director. If rejected, director shall specify why the report was rejected and the applicant shall have the opportunity to rectify deficiencies and resubmit the report.

17.05.045 CONDITIONAL USE PERMIT PROCESS FOR TYPE II ESSENTIAL SERVICE UTILITIES.

(A) Incomplete Applications. For all permits under this chapter, the director may reject any application that fails to meet the requirements of this chapter. The rejection shall be issued, in writing, within 15 calendar days of receipt of an application under this chapter and shall state the deficient items.

(1) Notification. Upon determination of a complete application, the director shall notify surrounding property owners in accordance with MSB 17.03, except that:

(a) the notification area will be 600 feet from the centerline of the proposed route or routes; and

(b) the notification shall include all individuals who were notified of or submitted comments at the community meeting required by MSB 17.05.042.

(B) Determination. In granting or denying a permit for a Type II Essential Service Utility, the Planning Commission shall make written findings of fact and determinations of law and shall include conditions as deemed appropriate to protect the public health, safety or general welfare.

(C) Process Time Frame. For conditional use permits reviewed by the commission:

(1) A public hearing shall be held by the commission within 90 calendar days of receipt of a complete application;

(2) The commission shall render a decision within 30 calendar days from the close of public hearing.

17.05.048 STANDARDS FOR APPROVAL OF TYPE II ESSENTIAL SERVICE UTILITIES.

(A) A conditional use permit for a new Type II Essential Service Utility may only be approved if it meets the requirements of this chapter.

(B) In granting or denying a permit, the commission shall make findings on whether the applicant has demonstrated that:

(1) To the extent that is technically and reasonably feasible, and without substantially increasing installation or maintenance costs, the location of the Type II essential service utility is such that its impacts on the visual and scenic resources of surrounding properties have been minimized;

(2) Adequate measures have been taken to minimize trespassing resulting from installation of the new Type II essential service utility.

(3) Utilities shall make reasonable adjustments to new Type II essential services to minimize the impact to public parks, trails recognized within adopted borough plans, and water bodies have been minimized;

(4) The Type II essential service utility shall not interfere with the approaches to any existing airport or airfield that are identified in the borough's regional aviation system plan or by the Alaska State Aviation System Plan; and

17.05.050 DEFINITIONS.

- "Type I essential service utility" means any above or below ground structures or facilities used for utility distribution including:

- (1) "Electricity distribution" means medium voltage (less than 50KV) power lines, low voltage electrical substations and pole-mounted transformers; and low voltage (less than 1,000V) distribution wiring to provide service to individual customers; and

- (2) "Service pipeline" means a distribution line that transports gas, oil, water, or sewage from a common source of supply to the meter set assembly or distribution endpoint to provide service to individual customers.

- "Type II essential service utility" means any aboveground [OR BELOW GROUND] **high-voltage (50KV or higher) power lines.** [STRUCTURES OR FACILITIES USED FOR UTILITY TRANSMISSION INCLUDING:

- (1) "ELECTRICITY TRANSMISSION" MEANS] HIGH-VOLTAGE (50KV OR HIGHER) POWER LINES., HIGH-VOLTAGE ELECTRICAL SUBSTATIONS AND POLE-MOUNTED TRANSFORMERS, AND HIGH-VOLTAGE DISTRIBUTION OR TRANSMISSION WIRING; AND]

[(2) "TRANSMISSION PIPELINE" MEANS PIPELINES INSTALLED FOR THE PURPOSE OF TRANSMITTING GAS, OIL, WATER, OR SEWAGE FROM A SOURCE OR SOURCES OF SUPPLY TO ONE OR MORE DISTRIBUTION CENTERS, TO ONE OR MORE LARGE VOLUME CUSTOMERS, OR A PIPELINE INSTALLED TO INTERCONNECT SOURCES OF SUPPLY. IN TYPICAL CASES, TRANSMISSION LINES DIFFER FROM DISTRIBUTION LINES IN THAT THEY OPERATE AT HIGHER PRESSURES, ARE LONGER, AND THE DISTANCE BETWEEN CONNECTIONS IS GREATER.]

17.05.100 APPEAL PROCEDURE.

(A) Appeals from a decision of the commission granting or denying a permit under this chapter shall be filed and conducted in accordance with MSB 15.39.

17.05.200 NONCONFORMING USES.

(A) Within the borough there may exist non-conforming uses as of the date of adoption of the ordinance codified in this chapter, or amendments thereto which were lawful before the effective date of applicable regulations, but which would otherwise be prohibited, regulated, or restricted under this chapter. Such existing nonconforming uses are permitted to continue subject to the provisions of this section, but shall not be expanded except as specifically provided in

this chapter.

(B) Except as specifically provided for by code, this chapter does not require the relocation or removal of a nonconforming use existing or under construction at the time of adoption of the ordinance codified in this chapter if such use was lawful at the time of its construction. No nonconforming use shall be constructed or operated except in accordance with these regulations, except to the extent it was in existence or under actual construction as of the effective date of the ordinance codified herein or amendment thereto. "Actual construction" is defined as the substantial placement of construction materials and performance of labor for construction of facilities which cannot reasonably be used except in a manner which does not conform with these regulations.

(C) Type II essential service utilities that have not been constructed but have successfully completed the minimum requirements and public involvement process outlined in Assembly Ordinance 07-076 are granted nonconforming status under this section.

17.05.300 VIOLATIONS, ENFORCEMENT, AND PENALTIES.

(A) Except as otherwise specified in this chapter

violations of this chapter are infractions.

(B) Remedies, enforcement actions, and penalties shall be consistent with the terms and provisions of MSB 1.45.

(C) Failure to correct a violation of any permit condition is a violation of borough code.

(D) In addition to other applicable penalties, failure to correct the violation of code, after reasonable notice, may result in revocation of the permit.

Section 3. Effective date. This ordinance shall take effect upon adoption.

ADOPTED by the Matanuska-Susitna Borough Assembly this - day of -, 2023.

EDNA DeVRIES, Borough Mayor

ATTEST:

LONNIE R. McKECHNIE, CMC, Borough Clerk

(SEAL)

CHAPTER 17.05: ESSENTIAL SERVICE UTILITIES

Section

17.05.010 Purpose

17.05.020 Applicability

17.05.030 Type I essential service utility

17.05.040 Type II essential service utility

17.05.050 Definitions

17.05.010 PURPOSE.

(A) The purpose of this chapter is to provide for public participation in the decision affecting the installation of essential service utilities in such a manner that they enhance the health, safety and general welfare of borough residents and properties while minimizing negative impacts. This chapter recognizes the importance and benefits of essential services, while ensuring that all feasible mitigation measures are taken to protect the scenic qualities of the Matanuska Susitna Borough. Essential services should be installed in cognizance of existing and projected demands for such services.

(Ord. 07-076, § 2 (part), 2007)

17.05.020 APPLICABILITY.

(A) This chapter applies to all areas of the borough except within the city boundaries of Houston, Palmer, Wasilla, and the Port MacKenzie special use district boundary. This chapter applies to municipal utilities that extend beyond city boundaries and the port district boundary into unincorporated borough lands.

(Ord. 22-062, § 2, 2022; Ord. 07-076, § 2 (part), 2007)

17.05.030 TYPE I ESSENTIAL SERVICE UTILITY.

(A) All proposed Type I essential service utilities, when installed or extended in any public way or borough-owned land, shall require a permit issued by the borough in accordance with MSB 11.30.030.

(Ord. 07-076, § 2 (part), 2007)

17.05.040 TYPE II ESSENTIAL SERVICE UTILITY.

(A) All proposed Type II essential service utilities shall require a public involvement program in accordance with a public participation plan as submitted by the utility in all areas of the borough excluding the cities of Houston, Palmer, Wasilla, and the Port MacKenzie special use district boundary.

(B) Within 20 calendar days of receipt of the proposed public participation plan, the director shall provide the applicant with written acknowledgement of receipt of the plan, along with any recommendations concerning the proposed process. The public involvement program, at a minimum, must contain the following:

(1) *Minimum requirements.*

- (a) the utility's public involvement program must comply with established state and federal guidelines governing the utility including adequate public notice, public process, public meetings, or public hearings;
- (b) if no established state or federal guidelines apply to the proposed action, the utility shall follow its own utility board adopted guidelines for public notification and involvement;
- (c) if there are no established state, federal or utility board adopted guidelines, the public involvement program will consist of the minimum requirements outlined in subsection (B)(2) of this section, Public Involvement; and
- (d) if a state, federal or utility board adopted public involvement program is used, they must at least meet or exceed the minimum guidelines in subsection (B)(2) of this section, Public Involvement.

(2) *Public involvement.*

- (a) a minimum of one public meeting will be held by the utility and shall be held in an area central to the area impacted by proposed action;
- (b) a minimum of one formal public hearing will be held by the utility later in the process to allow for formal public testimony. The public hearing will be held in an area central to the area impacted by proposed action; and
- (c) notice of the public meeting and public hearing to occur a minimum of 15 days in advance of the public meeting or public hearing. The public notice will include:
 - (i) three notices in a newspaper of general circulation within the borough;
 - (ii) public postings in local areas such as libraries, public buildings, schools, stores, laundromats, lodges, on the utility's website, and on the Matanuska-Susitna Borough's website, etc.;
 - (iii) public service announcements on local radio stations starting 15 days before the public meeting; and

(iv) mailings, as appropriate, including notification of all affected community councils.

(C) Implementation of the public involvement program shall commence within 120 days from the issuance date of written acknowledgement.

(D) Upon completion of the public involvement program elements, the applicant shall create and submit to the director a decisional document that describes how the public involvement program was implemented, the nature of public comment, the chosen course of action, timeline for construction, and the public's appeal process. Copies of all written public comments and an audio record, if available, shall be included in the decisional document.

(Ord. 22-062, § 2, 2022; Ord. 07-076, § 2 (part), 2007)

17.05.050 DEFINITIONS.

- "Type I essential service utility" means any above or below ground structures or facilities used for utility distribution including:

- (1) "Electricity distribution" means medium voltage (less than 50KV) power lines, low voltage electrical substations and pole-mounted transformers; and low voltage (less than 1,000V) distribution wiring to provide service to individual customers; and

- (2) "Service pipeline" means a distribution line that transports gas, oil, water, or sewage from a common source of supply to the meter set assembly or distribution endpoint to provide service to individual customers.

- "Type II essential service utility" means any aboveground or below ground structures or facilities used for utility transmission including:

- (1) "Electricity transmission" means high-voltage (50KV or higher) power lines, high-voltage electrical substations and pole-mounted transformers, and high-voltage distribution or transmission wiring; and

- (2) "Transmission pipeline" means pipelines installed for the purpose of transmitting gas, oil, water, or sewage from a source or sources of supply to one or more distribution centers, to one or more large volume customers, or a pipeline installed to interconnect sources of supply. In typical cases, transmission lines differ from distribution lines in that they operate at higher pressures, are longer, and the distance between connections is greater.

(Ord. 07-076, § 2 (part), 2007)

By: A. Strawn
Introduced: August 7, 2023
Public Hearing: August 21, 2023
Action:

MATANUSKA-SUSITNA BOROUGH
PLANNING COMMISSION RESOLUTION NO. 23-04

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH PLANNING COMMISSION RECOMMENDING APPROVAL OF AN ORDINANCE AMENDING MSB 17.05 - ESSENTIAL SERVICE UTILITIES BY PROVIDING ADDITIONAL PROCESS AND STANDARDS FOR APPROVAL.

WHEREAS, MSB 17.05 - Essential Service Utilities regulates installation of new high voltage (>50kV) electrical transmission lines or transmission pipelines; and

WHEREAS, while the code does require a public process, there are no standards for approval other than the public process itself; and

WHEREAS, this ordinance reduces the applicability of MSB 17.05 to not include transmission pipelines, or any utility that is installed below ground; and

WHEREAS, this ordinance also creates a conditional use permit process for installation of high-voltage powerlines within the Borough; and

WHEREAS, installation of new high-voltage powerlines should require standards related to trespass issues, scenic resources, and the MSB comprehensive plan; and

WHEREAS, utility companies should be required to host a public process prior to submitting a conditional use permit for review by the Planning Commission; and

NOW, THEREFORE, BE IT RESOLVED, that the Matanuska-Susitna Borough Planning Commission hereby recommends approval of Assembly ordinance 23-080.

ADOPTED by the Matanuska-Susitna Borough Planning Commission this -- day of --, 2023.

C J KOAN, CHAIR

ATTEST

CORINNE LINDFORS
Planning Clerk

(SEAL)

YES:

NO:

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MATANUSKA-SUSITNA BOROUGH

Planning and Land Use Department

350 East Dahlia Avenue • Palmer, AK 99645

Phone (907) 861-7822

www.matsugov.us

MEMORANDUM

DATE: August 10, 2023

TO: Planning Commissioners

FROM: Alex Strawn, Planning and Land Use Director

SUBJECT: Tentative Future PC Items

Upcoming PC Actions

Quasi-Judicial

- Dime Bag – Marijuana Retail Facility; 6298B01L002 (Staff: Peggy Horton)
- Green Go, LLC – Marijuana Cultivation Facility; 17N01W11A020 (Staff: Rick Benedict)
- The Aardvark – Alcoholic Beverage Dispensary; 1454000L001 (Staff: Peggy Horton)
- Smoke Out Point – Marijuana Retail Facility; 2209B02L001A (Staff: Rick Benedict)
- Luiten on Big Lake – Variance; 6285000T001A (Staff: Peggy Horton)
- Craft Cannabis Cabin – Marijuana Retail Facility; 1842B01L007 (Staff: Rick Benedict)
- Northern Gravel & Trucking LLC – Earth Materials Extraction; 18N02E03B005 (Staff: Peggy Horton)
- Back Acres LLC – Earth Materials Extraction; 17N02E23A017 (Staff: Peggy Horton)
- Ficklin Gravel Products LLC – Earth Materials Extraction; 16N0403A009 (Staff: Rick Benedict)
- Mountain Gravel Investments – Earth Materials Extraction; 18N01E35006 (Staff: Peggy Horton)
- Meadow Lakes Holiday – Alcohol Package Store; 17N02W09A014 & 17N02W09A022 (Staff Peggy Horton, Planner II)

Legislative

- Historic Preservation Plan (HPP) (Staff: Gerrit Verbeek)
- Municipal Separate Storm Sewer System (MS4) (Staff: Kim Sollien)
- MSB Borough-Wide Comprehensive Plan (Staff: Kelsey Anderson)
- Glacier View Comprehensive Plan Update (Staff: Leda Borys)
- Corridor Studies (Staff: Kim Sollien)
- Public Transit Plan (Staff: Kim Sollien and Maija DiSalvo)
- Amending MSB 17.73 Multi-family (Staff: Jason Ortiz)
- Amending MSB 17.59 Standardize definitions for lake management regulations (Staff: Alex Strawn)
- Fiscal Year 2025 Capital Improvement Program (Staff: Rick Antonio)